

AGENDA
VILLAGE OF PLEASANT PRAIRIE
PLEASANT PRAIRIE VILLAGE BOARD
PLEASANT PRAIRIE WATER UTILITY
PLEASANT PRAIRIE SEWER UTILITY
Village Hall Auditorium
9915 – 39th Avenue
Pleasant Prairie, WI
June 15, 2015
6:00 p.m.

1. Call to Order
2. Pledge of Allegiance
3. Roll Call
4. Minutes of Meeting – May 18, 2015
5. Tabled Public Hearing
 - A. Consider the Liquor License Renewal Application for the BP Amoco gasoline station and convenience store located at 10477 120th Avenue.
6. Citizen Comments (Please be advised per State Statute Section 19.84(2), information will be received from the public and there may be limited discussion on the information received. However, no action will be taken under public comments.)
7. Administrator’s Report
8. New Business
 - A. Consider Resolution #15-20 in Memory and Celebration of Barbara C. Peterson – RecPlex Member #1.
 - B. Receive the 2014 Annual Report of the Kenosha Area Convention and Visitors Bureau.
 - C. Receive Plan Commission recommendation and consider Resolution #15-21 for a Floodplain Boundary Adjustment on the property located at 12575 Uline Drive for the purpose of constructing a conference center between the Uline Corporate Office building and the easternmost warehouse building on the property.
 - D. Consider Amendment #3 to the Settlement Agreement between the Village and VIDHYA Corp VIII, Inc. to modify the testing requirements and to follow the recommendation and approval of the Wisconsin Department of Natural Resource for testing at the BP Amoco gasoline station and convenience store located at 10477 120th Avenue.

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- E. Consider an award of contract for Municipal Solid Waste Disposal.
- F. Consider an award of contract for Single Stream Recycling.
- G. Consider the purchase of an ambulance chassis and module.
- H. Consider the disallowance of a claim filed as a result of a sewer backup at 4500 83rd Street.
- I. Consider the 2015-206 Towing License renewal applications.
- J. Consider a Letter of Credit Reduction for the Ashbury Creek Subdivision.

9. Village Board Comments

10. Adjournment

The Village Hall is handicapped accessible. If you have other special needs, please contact the Village Clerk, 9915 – 39th Avenue, Pleasant Prairie, WI (262) 694-1400

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9915 - 39th Avenue
Pleasant Prairie, WI
May 18, 2015
6:00 p.m.**

A regular meeting of the Pleasant Prairie Village Board was held on Monday, May 18, 2015. Meeting called to order at 6:00 p.m. Present were Village Board members John Steinbrink, Kris Keckler, Steve Kumorkiewicz, Dave Klimisch and Mike Serpe. Also present were Michael Pollocoff, Village Administrator; Tom Shircel, Assistant Administrator; Jean Werbie-Harris, Community Development Director; Kathy Goessl, Finance Director; Dave Smetana, Police Chief; Doug McElmury, Fire & Rescue Chief; Rocco Vita, Village Assessor; Matt Fineour, Village Engineer; John Steinbrink Jr., Public Works Director; Carol Willke, HR and Recreation Director; Dan Honore, IT Director; Sandro Perez, Inspection Superintendent and Jane M. Romanowski, Village Clerk. Four citizens attended the meeting.

1. CALL TO ORDER

2. PLEDGE OF ALLEGIANCE

John Steinbrink:

VFW Post 7308 will lead us in the Pledge.

3. ROLL CALL

4. VFW POST 7308 PRESENTATION OF LOCAL AND NATIONAL PUBLIC SERVANT AWARDS TO A VILLAGE EMERGENCY MEDICAL TECHNICIAN, FIREFIGHTER AND LAW ENFORCEMENT OFFICER.

Paul Fredericksen:

I'm Paul Fredericksen. I'm the Commander of the Post 7308, Pleasant Prairie Post. And I live at 8821 65th Street. Pleasant Prairie is right at my backyard even though I'm in the City. Very pleased to come and present these awards, well deserved awards. And with me to represent our Post, our District and the State, first of all I have the State Chief of Staff Phil Allen who is also a member of Post 7308. He's been past Commander about, what, nine times?

Phil Allen:

Eleven.

Paul Fredericksen:

Eleven, okay. He's a charter member of the Post, and we're going into our 25th anniversary, and we're going to celebrate that this next week. And then Randy Strickland, he's also a two times past Commander for 7308. He's also the President, or he's stepped down, the District

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Commander for District 1 which goes all the way out to Janesville. Dan McKee he's a Pleasant Prairie native, and he lives over on the other side of the tracks. And he's going to be our Quartermaster. So I brought the brass with me.

And we're very pleased to present - first I'm going to present the awards from the Post level. And then after we got these awards determined we send it onto the State, and the State send on these names to national, and we have some national awards. This is the second year in a row for this, and we're very proud to be able to do this. And it gives us a chance -- you know, we hear so many things about bad behavior being awarded. Well, now is our chance to give awards for good behavior so it's a wonderful thing.

And really having been a firefighter for 30 years and having worked with the police department over the length of that time, too, I know what these guys and gals go through, and they're not always that appreciated, although we try to do our best. We make split second decisions, and there's a lot of pressure on these people when they get out onto the street and they get into somebody else's homes. So with that in mind there are some criteria. We don't just give them out. We ask the nominees to come from the departments. They know their personnel better than we do.

For the National Emergency Medical Technician Public Servant Citation is any individual who actively gives medical treatment, provides rescue service or civil disaster assistance as a member of any public or volunteer company organized to give emergency medical care, and provide rescue and civil disaster assistance to our nation's citizens. And we know that law enforcement, any individual who serves on a municipal, county, state or federal unit passed with enforcement of the laws pertaining to their area of responsibility. This award does not apply to individuals employed by private companies or security services.

Then, of course, the National Firefighter Public Servant Citation is any individual who actively fights fires as a member of any public or volunteer company organized to fight fires and give assistance to our nation's citizens. The criteria is they have to have demonstrated recognition by their colleagues or those they serve. Consistent excellence in the performance of their duties, consistent dedication to their official responsibilities over a period of years, and continues growth and responsibilities and skills within their profession.

The documentation required for all candidates is a nomination letter containing the candidate's name, title, address, and telephone and identifying the award for which the individual should be considered. They have to present a one page resume of the candidate's overall background, a one page resume of the candidate's background in their field, a one page listing of the candidate's accomplishments and awards in their field. And then the nomination is made by the Post Safety Chairman, and it goes from there to the State and then from there the State Commander has to sign off on it.

And speaking of that the State Commander is John Giese. He was not able to make it tonight. He had planned to. He sends his regrets and his congratulations to all. So I think last year I started out with the Post awards with the firefighters and the EMTs. So this year I'm going to go in reverse, and I'm going to ask William Larson to come up.

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[Inaudible]

The Certificate reads the Veterans of Foreign Wars Certificate of Appreciation awarded to William Larson in grateful recognition of unyielding adherence to the highest ideals of law enforcement in maintaining, preserving and protecting the lawful rights of all citizens. To witness whereof we have hereunto set our hands in the official seal of the Veterans of Foreign Wars, United States, this day of 18 May 2015 and signed by me and my Quartermaster.

Phil Allen:

Okay, good evening. Phil Allen, past Commander of 7308, as he said. I'm the Chief of Staff of the State this year. The State Commander and I travel the State most of the time together. But this happened to be a night where we had three places to be. So me being in this area he asked me to come over here. He would have liked to have been here because he's a retired police chief from the City of Elkhorn. So he would have really liked to have been here to give this award to you.

So Officer William Larson, in recognition and sincere appreciation of his dedication and unselfish service to the Pleasant Prairie Police Department. During William Larson's outstanding career he has maintained a record of highly professional and exemplary service to the community and the nation. His extraordinary commitment clearly illustrates the proud care, core values, and traditions of the law enforcement profession further highlighted by the degree of admiration and respect of both his peers and the local public he serves. Officer William Larson is truly an exceptional example of all who wear the badge. And this honor reflects the proud ideals and high standards of Veterans of Foreign Wars in the United States. Signed John Stroud, he's our National Commander in Chief of the VFW.

Paul Fredericksen:

The next one is for EMT, a Certification of Accommodation Award to Thomas A. Overocker and special recognition of exemplary service to the humanity by administering emergency medical assistance. In witness hereof we hereunto set our hands in the official seal of the Veterans of Foreign Wars of the United States on this day 18 May 2015, signature by myself and my Quartermaster.

Phil Allen:

VFWFVW National Emergency Medical Technician EMT Award awarded to Frederick Thomas A. Overocker in recognition and utmost appreciation of his dedication and unselfish service to the Pleasant Prairie Fire Department. During Thomas Overocker's outstanding career he has demonstrated a record of extraordinary and courageous service to the community and the nation. His exceptional devotion is illustrative of his theme core values and traditions of the profession of emergency responders further evidenced by the high degree of admiration and respect from both his peers and the local public he serves. Firemedic Thomas Overocker is truly a premier example of all EMTs, and this honor reflects the proudest ideals and values of the Veterans of Wars of the

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United States of America. On behalf of John Stroud, our National Commander in Chief, I present you with this award.

Paul Fredericksen:

And the last one we have here is a Certification of Appreciation Award for Justin Beach in special recognition and highest praise for alertness and personal courage and every efficient guardianship of life and property. Once again, this is the VFW seal and signed by myself and my Quartermaster. Firefighters have a special place in my heart, obviously, so I'm glad to see that we're represented here. Thanks for your nominations to the Chief.

Phil Allen:

The VFW National Firefighter Award is awarded to Firemedic Justin H. J. Beach in recognition and utmost appreciation of his dedication and selfless service to the Pleasant Prairie Fire Department. During Justin Beach's outstanding career he has displayed an exceptional record of exemplary courageous service to the community and the nation. His extraordinary commitment in his keeping with the esteemed core values and traditions of the firefighters' profession further manifested by the high degree of admiration and respect of both his peers and the local public he serves. Firemedic Justin Beach is truly a role model to all firefighters, and this honor reflects the proudest ideals of the Veterans of Foreign Wars of the United States, signed John Stroud, National Commander in Chief.

Paul Fredericksen:

Okay, that concludes our awards, but I'd just like to add something if I may. I know you're business and you've got a lot of business to attend to. But we are the Pleasant Prairie VFW Post. We are the only veterans group that is affiliated with Pleasant Prairie at least by name, right? We have other groups that work in Pleasant Prairie. We work throughout the whole County, and we work with all groups. And we don't just work in Pleasant Prairie, but our main focus is Pleasant Prairie.

With that being said I'd like to have anyone who is interested who meets the criteria as a member of VFW, the Veterans of Foreign Wars, from Pleasant Prairie to join our organization, and we'd be glad to have you. We work with the schools, and we work a lot with Invest, and we work with a lot of different groups in Pleasant Prairie. And we'd be glad to have any of you that would be eligible and would want to work with us. That's all I have to say.

John Steinbrink:

Thank you, Paul. Before you leave the podium maybe you could give us a little heads up on some of the services coming up this Memorial Holiday.

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Paul Fredericksen:

Well, we have a whole bunch of things going on. Randy Strickland and I have been asked to go up to Union Grove and present the wreath for the Union Grove Veterans Cemetery Memorial Day presentation. We're putting flags in the ground out at Sunset Ridge on Thursday. There's about 3,500 flags at veterans' graves that have to have flags on them. If anybody can go at about eight o'clock in the morning on Thursday I'd be glad to have all the help we can get. We are having our Post meeting in the evening that same day, and it's our 25th anniversary dinner. So we are going to have a shortened meeting, and then we're going to get into celebrating 25 years in the Prairie and as a VFW Post.

There is also downtown in Kenosha the Historical Museum is going to have a salute to veterans. And, of course, we have our Memorial Day Ceremony at Old St. Mark's Cemetery. And we've been doing that ever since the beginning of time right from the first year in the Post. I don't know how many people are in there anymore, but there are veterans that are from 1812 and the Civil War. And there are relatives from this family, the Tafe [phonetic] family who come to our service every year. So we salute them. We get the Scouts out there. We get kids from Prairie Lane School that come out, the Girl Scouts. And we're actually going to have a fly over at the end of it. So if you can make it out there. I don't know the exact address, but it's at the southeast corner of 9th Court and 78th Street or something like that. John, you know where it is?

John Steinbrink:

It's a very well hidden place. It's at the back side of Southport School.

Paul Fredericksen:

Right behind Southport School. There's two ways in, but you might be able to get lost if you're not careful. And there are a whole bunch of things coming up. Pleasant Prairie working through Invest is holding a salute to veterans picnic on Flag Day, June 14th. So there are a whole bunch of things coming out. And I can actually send a calendar through Ruth out with all of the events that are coming. And hopefully you can come out and sidle up to the vets and we appreciate it. We really do. We appreciate what's happened in the last decade in recognition of veterans, especially Vietnam veterans. When we came home we felt that we were on our own. Now that's turned around, and we really appreciate that. And thank you for all working with us the way you do it. From RecPlex and everything else that goes on Pleasant Prairie gets it done and they know how to do things. And we're proud to be part of that system that we have working together. Thank you very much.

John Steinbrink:

Thank you, Paul. And, once again, I want to thank VFW Post 7308 for recognizing these outstanding individuals in the public service sector. The Village and its residents are very fortunate to be served by public safety personnel as we saw here tonight and our whole team of public safety personnel. And they're all of such a high caliber. And their dedication is to protect

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and serve the Village and its residents, and they do an outstanding job. And it's great to see them honored here this evening. Once again, thank you Post 7308. Other comments?

5. MINUTES OF MEETINGS - MAY 4, 2015

Steve Kumorkiewicz:

Make a motion to accept as written.

Dave Klimisch:

I'll second it.

John Steinbrink:

Motion by Steve, second by Dave. Any discussion on the minutes as presented?

KUMORKIEWICZ MOVED TO APPROVE THE MINUTES OF THE MAY 4, 2015 VILLAGE BOARD MEETING AS PRESENTED IN THEIR WRITTEN FORM; SECONDED BY KLIMISCH; MOTION CARRIED 5-0.

6. PUBLIC HEARING

A. Consider a "Class A" Intoxicating Liquor and Class "A" Fermented Malt Beverage license for the proposed Costco store, 7704 94th Avenue.

Jane Romanowski:

Mr. President and Board members, Costco Wholesale Corporation has submitted an application for the license you described for that location at 7707 94th Avenue which is currently under construction. Rodney Adrian Huft has been appointed as agent to hold that license. The Class A fermented malt beverage and intoxicating liquor license allows for the sale of intoxicating liquor and fermented malt beverages to consumers only in original packages or containers for off premise consumption only.

So our ordinance allows - since Costco is under construction right now, our ordinance allows the Board to grant a license, and that granting would be good for 90 days. And in that time they could finish their building, get their occupancy permits, make sure everything's all set, and they wouldn't have to come back to the Board. I could issue the license. If for some reason it would go beyond the 90 days they could come back and ask for an additional time frame to extend that granting so they can get their liquor license.

So the police department check has been completed. Ownership has been checked. Publication was met and the agent has met the residency and training requirements to hold a license. There are no delinquencies on record. And I don't know the exact date or what they're shooting for, but Costco proposes to open the establishment in June. If the Board approves the application and it's

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issued before June 30th we'll prorate fees from the day it was issued through June 30th. And then the new licensing year starts July 1st through June 30th of next year. So we'll just prorate those fees, add the costs of publications. And, of course, nothing will be issued out of my office until the certificates of occupancy have been issued from the Community Development Department, the Building Department and the Fire and Rescue Department. This mirrors what happened for Target. Same type of license and the same type of business. This is a public hearing.

John Steinbrink:

Thank you, Jane. This being a public hearing I will open it up to public comment or question. When you use the microphone please give us your name and address for the record. Do we have a signup sheet?

Jane Romanowski:

There wasn't anybody that signed up for this.

John Steinbrink:

Anyone wishing to speak on this item? Anyone wishing to speak on this item? Yes, sir. This is for the Costco item.

--:

Pardon?

John Steinbrink:

This is for the Costco item. Liquor license for the Costco store. And anyone wishing to speak on this item? Hearing none I'm going to close the public hearing and open it up to Board comment or question. Dave?

Dave Klimisch:

In our packet I see that there's at least nine other Costco's in Wisconsin that have liquor licenses. But it looks like hundreds across the country.

Jane Romanowski:

Yes, and they were very easy to work with. They had counsel that worked with me closely and called with any questions. I would recommend approval. It was very easy.

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Dave Klimisch:

So based on the track record that's obvious in front of us here with the nine in Wisconsin and all across the country I'll move approval.

Michael Serpe:

Second.

John Steinbrink:

Motion by Dave, second by Mike. And just for the record I believe the opening date is June 12th.

Jean Werbie-Harris:

The official opening and ribbon cutting is June 12th.

John Steinbrink:

June 12th. Alright, we have a motion and a second. Any comment or question?

KLIMISCH MOVED TO APPROVE THE "CLASS A" INTOXICATING LIQUOR AND CLASS "A" FERMENTED MALT BEVERAGE LICENSE FOR THE PROPOSED COSTCO STORE, 7704 94TH AVENUE FROM THE DATE OF ISSUANCE TO JUNE 30, 2016 SUBJECT TO THE CONDITIONS SET FOR BY THE VILLAGE CLERK; SECONDED BY SERPE; MOTION CARRIED 5-0.

7. CITIZEN COMMENTS

Jane Romanowski:

The first speaker tonight is Arthur Brennan.

John Steinbrink:

Once again, please everybody give us your name and address for the record.

Art Brennan:

Art Brennan, 10099 32nd Avenue. I'm here on a couple of counts. One is to save my mailbox. It's been sideswiped twice already. Not during the day, but traffic coming down 101st Street has a stop sign that usually works during the day. But at night after 9:30 one out of three cars don't even observe it. And the faster they go around that corner from 103rd onto 32nd the further they've got to go around. Just like that train was going too fast on the curve, but they don't have a track to stay on so they go over it.

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Now, where the sewer comes down 101st and meets the sewer on 32nd there's a manhole cover on my side of the road. Usually there's a steel spike painted red so the snowplows can see where not to plow. It's gone. I guess they knocked it down so many times with people making circle turns that they don't even bother with that anymore. And the gravel if you look at it the plows are pushing the gravel over the manhole cover.

Now, my problem is at night only one out of three cars stop at the stop sign. And they'll go at any speed, and the faster they go the wider their turn. Twice my mailbox has been sideswiped. They did not break the main post, but they did break the top of the cross bars and I've had to redo it. And this will continue for six months or so, whatever the construction is. So I want to ask the Board if they couldn't put a bumper at the stop sign or near it where it will slow the traffic down at night. I realize they can't have a policeman there all night long, but this will help at least slowing the traffic down. Even if they don't stop they'll at least be going slow enough where they could stay on the road. That's all.

John Steinbrink:

Alright, thank you, Mr. Brennan. And, once again, this is citizens' comments. The Board cannot take any action or discussion on these items brought up before us. But we hear what we're saying, and we're going to investigate it with public works and the police department. So thank you, sir.

Jodi Acheson:

Good afternoon, I'm Jodi Acheson with the Public Services. We own and operate the Kestrel Hawk Landfill on Oakes Road in Racine, Wisconsin. And we were one of four companies that were present for the waste and recycling RFP opening recently, and one of two companies that submitted a bid for the disposal of solid waste at the Kestrel Hawk Landfill. I understand there's a recommendation to the Board to reject the current proposal.

I just wanted to stop in and give you kind of a brief overview of the value and savings of utilizing Kestrel Hawk Landfill. We propose the rate of \$43 per ton, and that includes all the taxes and fees associated with that. Additionally, the annual price increase proposed in the document was based on a 12 month prior CPI which as it stands right now the CPI is very flat, and there would be no price increase at this time. We'll see where that goes in the future.

But there are a lot of benefits to using Kestrel Hawk Landfill, and we just wanted to bring them to your attention. Also, supporting Wisconsin jobs. We're in Racine, it's a landfill. There are a lot of fees that the landfill collects that go to the cities, in Kestrel Hawk's case the State of Wisconsin. Based on the annual volume from the Village annual fees are around \$84,000 per year that would go to the State of Wisconsin. Over \$250,000 over the three year contract term would be fees and a benefit to the State of Wisconsin. The State would not receive those benefits if an out of state disposal facility were utilized.

Additionally, using a local landfill in Wisconsin wouldn't require [inaudible] or the record keeping associated with the interstate requirements for trucking. So we'd just like to ask if the

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Board would consider re-evaluating the proposals further. We'd like to offer a test load so that the Board can study the actual distance and time it takes to get to our facility and factor that into the rates. We would extend that test load at no charge to the Village for evaluation purposes.

A little background, we're less than 13 miles from the public works building. We have a very short travel time from the street to our scale, about 200 feet on concrete. And then beyond that another 100 feet and u-turn to the [inaudible] so it's a very short haul time at the landfill which helps with maintaining vehicles and tires and things like that when you have longer haul times to the landfills. So we're just hoping that you take a second look at the bids. Rebidding after the RFPs were opened publically really puts those folks that have followed the RFP deadlines at a competitive disadvantage. And as a business and a potential vendor the Village we rely on the integrity of the competitive RFP process. I'd certainly be available later or in the future if you guys have questions on details.

Mike Pollocoff:

We're going to be taking this item up later on in the agenda, so if the Board chooses to ask any questions if you're available I'd recommend you stay around.

Jane Romanowski:

There were no additional signups tonight, Mr. President.

John Steinbrink:

Anyone else wishing to speak under citizens' comments?

S.R. Mills:

Hello. S.R. Mills, 4011 80th Street, Kenosha. I'm here tonight on behalf of the Barnes Creek Neighborhood Plan. Many of you were at the Plan Commission. We don't have too much further to add other than we look forward to working with staff and elected officials to continue to refine the plan and find something that they'll be happy with and proceed. We're here tonight if you have any questions as we move onto that agenda item. Thank you.

John Steinbrink:

Thank you. Anyone else wishing to speak under citizens' comments? Yes, sir?

Dave Salica:

My name is Dave Salica, 9801 Sheridan Road. I would like you to keep with your intents on the first land usage property and not increase that 18 acres on that property, that's the way it was before, to allow the commercial property to stay as originally planned. That's about it.

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John Steinbrink:

Alright, thank you. Anyone else wish to speak under citizens' comments. Anyone else? Hearing none I'm going to close citizens' comments.

8. ADMINISTRATOR'S REPORT

Mike Pollocoff:

Mr. President, I'll meet with the Public Works Director this week, and we'll take a look at the traffic patterns at Hill N' Dale and Rolling Meadows Subdivision post speed bumps that we have put in and evaluate the request as well as any other ones that we might want to look at. I want to remind the Board and anybody that's in the audience that these are considered very temporary measures as a way to slow traffic down in a subdivision that isn't designed for through traffic. But in they are left up for very long they're dangerous as well for bicycles or people who could be speeding and lose control of the car. So I want everybody to know that these things are -- I think in my 30 years this is the first time we've used them. I don't know that we'll use them again. But on the other hand it appears to be an appropriate solution to some of the problems we're having in that subdivision. So we'll look at Mr. Brennan's request tomorrow as well as some other spots in that subdivision and move forward accordingly.

John Steinbrink:

Okay, thank you, Mike.

9. NEW BUSINESS

- A. Receive Plan Commission recommendation and Consider Resolution #15-19 to deny the Comprehensive Plan Amendment to amend Appendix 9-3 Neighborhood Plan #2 for the Barnes Creek Neighborhood bounded by 89th and 91st Streets on the north, STH 32 (Sheridan Road) on the east, STH 165 (104th Street) on the south and the Kenosha County Bike Trail on the west.**

Jean Werbie-Harris:

Mr. President and members of the Board and the audience, as part of the Land Division and Development Control process in the Village of Pleasant Prairie, a neighborhood plan must be prepared in order for us to have a guide for future development for particular areas of the community. The neighborhood plan must be consistent with all the comprehensive plan components such as the Land Use Plan as well as the zoning ordinance for it to proceed. For historical reference the first comprehensive plan was prepared by the Town of Pleasant Prairie along with Kenosha County back in 1967. At that time it was more of an area wide plan, part of the Kenosha urban planning area. The next plan update was completed and adopted in 1996, and it refined in detail that '67 plan.

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After that the next plan that was completed was one that was done in 2009 in December which is our 2035 Comprehensive Land Use Plan. Again, the Land Use Plan along with the neighborhood plans provide a guide and framework how development will occur when and if it does occur. Just like with any other project in Pleasant Prairie that is privately owned with respect to the land, the process is typically driven or initiated by the landowner. And in this case Steve Mills and his family initiated a neighborhood plan modification for the Barnes Creek neighborhood plan area.

As I indicated at the Plan Commission meeting as part of the public hearing process, the planning process is a multi-step process. It begins with the comprehensive plan and the land use plan. Again, that's the overall or the largest view of the entire community. The next step is then the neighborhood plan which is a refinement of the comprehensive plan because it just focuses in on a specific area of the community. We have close to 30 different neighborhood plan areas of the community, and this is one of those areas. It's about a mile and a half square area. The next refined step is the conceptual plan where we look at just a very specific property or specific area of the neighborhood. And then finally a plat or site and operational plans refine that even further, and then construction would typically begin at that point once those approvals have been granted.

Again, the Land Use Plan for Pleasant Prairie was adopted in December of 2009. It's been amended periodically as modifications and others have been presented to the Plan Commission and Board for approval. Again, the comprehensive plan which is on the slide and on the boards is also available online. And what it does is it sets forth the generalized land uses within Pleasant Prairie. Those areas that are specifically identified in the yellow or orange or peach color on the maps are those areas designated for residential-type development. Those that are identified as red on the map are typically more urban commercial areas or community business areas of the community.

The pink areas are special interest, special development areas of the community. The gray is more of the industrial or manufacturing land uses within the community. And then the green areas, and there are a number of significant areas that are green, those are typically our environmental preservation areas or those that warrant some type of conservation, whether they be down in the Chihuaukee Prairie area on the east end of the Village or the large green area that's on the western portion of the Village as part of the Des Plaines River Watershed.

Again, as I mentioned, what's critical is that the development of a neighborhood plan must comply or be consistent with that comprehensive plan. Again, as I mentioned, the Village is broken up into several neighborhoods, sub-neighborhoods and special planning districts. And the area in particular that we're going to be talking about this evening is the Barnes Creek neighborhood. And that is kind of at the eastern portion of the Village. It's primarily bounded by 91st Street, Sheridan Road, 104th Street, the Kenosha County Bike Trail, and then Springbrook kind of runs through, and then 89th Street on the north end.

As you can see, the Barnes Creek neighborhood area is primarily rural or agricultural in nature. There are some more densely developed residential areas in the northwest corner, St. Joseph's Nursing Home, we've got a Village park up in this northwest corner. There's also a residential single family area in the southwest corner as well. But largely the area is pretty rural or agricultural in nature. Two other special areas to note is the archeological site in the very

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southeast corner. And the only true commercial area within the development which is the Keno Theater property which is at the very northeast corner of the property or the site.

So the area that we'd like to talk about this evening is the portion of the land use plan within the Barnes Creek neighborhood plan area. Again, this is the comprehensive plan just blown up for that particular area. The comprehensive plan does show that there's a commercial area in the very northeast corner that's identified for community commercial. There's an area that's identified in blue which is more of an educational elementary school, neighborhood park site. The green areas are more the conservation areas. And then, again, that peach color really represents the lower medium density residential classification for residential development. That could mean some single family, could mean some multifamily, but overall the density will average into that lower medium density residential classification.

So as presented before the Plan Commission, there were three different alternatives that were a matter of public hearing back on April 13th. The first one of which is the alternative number 1 as presented by the developer. This alternative identified in the northeast corner a large or big box commercial development, 150,000 square foot big box along with some other commercial development. At that time we do have a conceptual plan that had been submitted for a Wal-Mart at that location. Some of the other uses that they are identifying as part of their alternative plan, the yellow designates single family development, the typical third acre lot single family development. And then the areas that are identified in the orange on the map are multifamily development. And these can range anywhere from two units up to 40 units in size for the various size of the buildings.

A couple of things that I'd like just to note is that because of the need for travel through the community in a north, south, east, west direction some of the things that you'll see in common amongst all three of the neighborhood plans is that there is an east/west corridor that eventually connects down to Highway 165, and a north/south corridor that links from 104th up to Springbrook Road. Again, it's very important for these collector streets to be a part of any neighborhood in order to allow for development within that neighborhood in order to travel through the neighborhood without having to go out to an arterial perimeter street. So this is the first neighborhood plan that was presented by the developer.

The second Barnes Creek Plan alternative number 2 was prepared by the Village staff. This was in response to that first neighborhood plan. There was some disagreement and concern from the staff's perspective as to whether or not the first neighborhood plan would be the correct fit or be the correct type of land use for this community. So we prepared an alternative number 2. Primary differences in the northeast corner is we eliminated the big box, and we identified some smaller area community commercial land uses in that corner.

We've reduced some of the density with respect to the residential areas and encouraged more two family four and some six twenty fours, and we also eliminated some of the four unit buildings at the southern end. One of the things that is consistent with the first alternative plan is that the single family in this particular alternative does show the lots to be about that third of an acre with traditional public streets as we typically see them today as well as that interconnection throughout the neighborhood with the boulevarded roadways and roundabouts at designated locations.

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The third alternative is the most significantly different than the other two. First, one similar feature is that it's a lower square footage, no big box identified up at that northeast corner. With the single family it's a little bit different unique project or proposal that was identified, smaller lots but much more open space but private streets with respect to these developments. They could be gated communities, but they are intended to really emphasize the amount of green space and each lot having a few to open space or green space, again, but with private streets.

The multifamily similar with alternative number two. We've reduced the density so that it matched more clearly with respect to the comprehensive plan. And we introduced, again, more two units or duplex units rather than as many of the six and twenty four units in this particular alternative. All three of the alternatives I just would like to mention that the neighborhood school park, all three of them shifted it down to Highway 165 or 104th Street. No longer was it kind of dead center in the middle of the development.

As presented at the Plan Commission meeting, this is a chart that illustrates the existing and proposed residential units by building type for the plan alternatives. And as you can see the number of single family, multifamily existing as well as proposed by alternative. It also identifies specifically a breakdown of the multifamily units and how many were being shown in each of the alternatives. And then it gave you an overall breakdown of the net residential density or the units per acre as well as the lot size per unit or the square footage.

As you can see while alternative three seemed to be the direction that a lot of people wanted to see taken with respect to the single family, there's actually more units on the private streets than on the public streets. The one thing is that the alternative number one does have a much higher percentage of multifamily units within the development. And then just to follow up with respect to that slide, the population projections by neighborhood plan alternatives, again, current population and proposed population at full build out.

Again, our Land Use Plan and neighborhood plans are typically full building out 2035 or after. These are not areas that will immediately build out. But, again, the whole purpose of having these neighborhood plans put together is to know and to plan for how and when development may occur and how we can plan for community services for a particular area and how we can stage that over time so that we can provide service to the neighborhood.

One of the things that I wanted to mention is that we did somewhat of a more detailed evaluation of the original conceptual plan of that northeast corner. And, again, that original plan as presented was for a 150,000 square foot plus big box facility at that corner. And throughout our analysis, our discussion and all the information that was presented whether through open Village Hall or at the public hearing, we repeatedly heard from the residents as well as from the staff that this was not a suitable location for a big box location. And it would present a number of reservations or concerns and problems from a compatibility standpoint, from a transportation standpoint, from availability of sewer and water at least initially.

We looked at it from an economic impact concern standpoint, and just examining the urban design features as well as the community character and the architecture for this site and how it

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would or would not fit in with Pleasant Prairie. And we had agreed that it would not be appropriate or fit in with Pleasant Prairie. One of the main recommendations or results coming from the public hearing was, in fact, that the plan that was proposed did not fit in with and was not compatible with the adjacent land uses, and it did not comply with the comprehensive plan. And it did not comply with the zoning ordinance of Pleasant Prairie. And the zoning ordinance at least for this area would be B-2 which is a Community Business District.

So the staff made our recommendations at the Village Plan Commission meeting. We read over 16 pages of comments into the record. We heard almost three hours of testimony at the Plan Commission and the Village Board meeting. And so as a result the recommendation that I'm about to read is the recommendation that was presented at the Plan Commission meeting by myself as part of the record.

On April 13, 2015, the Village Plan Commission held a public hearing, adopted Plan Commission Resolution 15-12 that recommended that the Village Board deny the Barnes Creek Neighborhood alternatives 1, 2 and 3 as presented at the April 13, 2015 Plan Commission meeting. All the attachments as part of that public hearing record included Exhibit 1, Exhibit 2 and Exhibit 3. And based upon the facts the Plan Commission recommended denial of alternative number 1 neighborhood plan. Specifically the neighborhood plan is in conflict with the comprehensive land use plan and the B-2 Community Business Zoning District regulations and cannot be approved.

Furthermore, the alternative number 1 neighborhood plan commercial area identifying a proposed big box retail store is not compatible with the adjacent land uses. In addition, the negative impacts as discussed at the Plan Commission meeting that would be generated from a big box retail store at this location far outweigh the benefits to the community. The alternative 2 and 3 neighborhood plans warrant some consideration, however with the addition of another property at the southwest corner of Sheridan Road and 91st Street as a community commercial land use as shown on the neighborhood plans there is a conflict with the comprehensive land use plan, and the neighborhood plans cannot be approved until and unless there is an amendment of the comprehensive land use plan to expand the community commercial designations further south. So the Plan Commission also recommended denial of alternatives number 2 and 3 as well. So with that if there are any other questions or comments the staff recommends approval of the denial of the resolution for the three alternative neighborhood plans.

Michael Serpe:

Jean, at the Plan Commission we recommended denial of all three plans. But on the agenda it makes mention of just alternative plan number 2. Is there a reason for that?

Jean Werbie-Harris:

No, it should read all three. It should have read all three, 1, 2 and 3.

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Michael Serpe:

Okay, then I'm going to recommend that we concur with the Plan Commission's recommendation of denial of all three plans with further consideration given to plans 2 and 3 with the reference to the multifamily units that are proposed here. And I also look forward to meaningful discussion with Mills Development and the staff to come up with a plan that will fit the needs of this community and satisfy what the residents are looking for.

Steve Kumorkiewicz:

Second.

John Steinbrink:

We have a motion by Mike and a second by Steve. Further discussion? With that the motion is already amended.

Michael Serpe:

The motion is adopt 15-19 to deny the comprehensive plan 1, 2 and 3.

Steve Kumorkiewicz:

Yes.

SERPE MOVED TO CONCUR WITH THE PLAN COMMISSION RECOMMENDATION AND ADOPT RESOLUTION #15-19 TO DENY THE COMPREHENSIVE PLAN AMENDMENT TO AMEND APPENDIX 9-3 NEIGHBORHOOD PLAN #2 FOR THE BARNES CREEK NEIGHBORHOOD BOUNDED BY 89TH AND 91ST STREETS ON THE NORTH, STH 32 (SHERIDAN ROAD) ON THE EAST, STH 165 (104TH STREET) ON THE SOUTH AND THE KENOSHA COUNTY BIKE TRAIL ON THE WEST; SECONDED BY KUMORKIEWICZ; MOTION CARRIED 5-0.

B. Consider the request of Kwik Trip, Inc. for partial termination and release from three (3) Agreements entered into by and between the previous land Owner (V.K. Development Corporation) and the Village of Pleasant Prairie and two (2) variances granted by the Village for public-related infrastructure improvements in the Prairie Ridge Development installed in the land areas located at the northwest corner of 88th Avenue and 77th Street in the Prairie Ridge Development.

Jean Werbie-Harris:

Mr. President and members of the Board and the audience, Wendy Banasik, agent on behalf of Kwik Trip, is requesting a partial termination and release of three agreements entered into by and between the previous landowner which was V.K. Development Corporation and the Village of Pleasant Prairie, as well as two variances that were granted by the Village. And these had to do

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with related infrastructure improvements specifically for the property located south of Highway 50 between 88th Avenue and 104th Avenue in the Prairie Ridge development.

Specifically, the property that's requesting the release of these agreements is the property that is to be purchased by Kwik Trip for a future Kwik Trip Store from SB1, and specifically when these agreements, memorandums and variances were recorded they were recorded on all of the properties in Prairie Ridge. So as a result as each of these properties closes or the property is transferred the individual landowners would like to see these documents released from title.

Specifically all of these documents that we're considering this evening were documents that were approved by the Village in 1996, 1997 and 1998. All of these public improvements have since been completed, inspected and accepted by the Village. This was several, several years ago. These are the same type of agreements that have been released for the Senior Lifestyle project, for Prairie Ridge West project and some of the other more recent projects, Costco for example, that we considered in the last couple of years.

So the first is the memorandum of development agreement between V.K. and the Village dated September 13, 1996. The memorandum was related to the initial mass rough grading, drainage improvements and erosion control work as part of the preliminary engineering plans for Prairie Ridge. The second is a variance grant document 9614 executed by Pleasant Prairie and V.K. Development on December 16, 1997. The variance allowed for roadways within the single family portion of the development to extend more than 600 feet in length.

Number three is the memorandum of agreement executed by the Village and V.K. on October 29, 1997. This memorandum was related to the completion of the final required public and private infrastructure improvements for the final plat for Prairie Ridge. The fourth is a variance grant document 9709 executed by the Village, and this was related to the timing of the installation of public improvements for Prairie Ridge. And number five is the memorandum of development agreement executed between the Village and V.K. on March 9, 1998. It was related to the completion of the final required public and private infrastructure improvements for the final plat for the Prairie Ridge Subdivision Phase #1 or addition #1. So, again, all of these related improvements have been completed. And actually the release documents were drafted by our Village attorney for consideration this evening.

Kris Keckler:

Move to accept Admin's recommendation for the termination.

Steve Kumorkiewicz:

Second.

John Steinbrink:

Motion by Kris, second by Steve. Any discussion on this item?

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Steve Kumorkiewicz:

For some more information all the area that [inaudible] back in the '90s it was the largest multi-zoning project in the State of Wisconsin. There was 525 acres, and it took two years just to develop the [inaudible] profile. Is that right, Jean?

Jean Werbie-Harris:

And we are still working on that development today.

Mike Pollocoff:

Mr. President through the Chair, Jean on the 1998 agreement for exception number 12 does any of that charges identified relate to the surcharges for Highway 50 improvements?

Jean Werbie-Harris:

None of them.

Mike Pollocoff:

Okay. Have those been moved over to another parcel?

Jean Werbie-Harris:

No. There are two releases that the developer would like us to make for the special assessment fees for Highway 50. And when they close on that project on May 30th and the payments are made to the Village, then they're going to come back and ask for those to be released at that time.

Mike Pollocoff:

Okay. That's all I had, Mr. President.

John Steinbrink:

We had a motion and a second. Any further discussion?

KECKLER MOVED TO GRANT THE REQUEST OF KWIK TRIP, INC. FOR PARTIAL TERMINATION AND RELEASE FROM THREE (3) AGREEMENTS ENTERED INTO BY AND BETWEEN THE PREVIOUS LAND OWNER (V.K. DEVELOPMENT CORPORATION) AND THE VILLAGE OF PLEASANT PRAIRIE AND TWO (2) VARIANCES GRANTED BY THE VILLAGE FOR PUBLIC-RELATED INFRASTRUCTURE IMPROVEMENTS IN THE PRAIRIE RIDGE DEVELOPMENT INSTALLED IN THE LAND AREAS LOCATED AT THE NORTHWEST CORNER OF 88TH AVENUE AND 77TH STREET IN THE PRAIRIE RIDGE DEVELOPMENT; SECONDED BY KUMORKIEWICZ; MOTION CARRIED 5-0.

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C. Consider an award of contract for the maintenance of the Bentz Estates Pond located at the northwest corner of 85th Street and 60th Avenue.

John Steinbrink, Jr.:

Mr. President and members of the Board, on October 23, 2014 homeowners in Bentz Estates Subdivision were issued letters regarding an update and subsequent options to be taken pertaining to the costs associated with the maintenance responsibilities for outlet 1 as you have identified on the map on the screen. The homeowners are responsible to maintain the water and the grass areas within that outlot number 1 on the corner of 60th and 85th Street.

On April 24, 2015, a request for a proposal for Bentz Estates Pond maintenance for a three year term was offered to all landscape companies prequalified to work within the Village of Pleasant Prairie. Two companies requested bid packets. One bid was received for the packet. That was received from Kenosha Grounds Care for the cost of \$1,925 per year. Kenosha Grounds Care has been maintaining the pond for the last several years. And I do recommend that the Village enter into an agreement with Kenosha Grounds Care in the amount of \$1,925 a year for a contract total of \$5,775. I can answer any questions.

John Steinbrink:

And this is bid every year?

John Steinbrink, Jr.:

That's correct, but now we're going to a three year term. Kenosha Grounds Care I believe has gotten it every year but one. And the one year we did not give it to them we had a lot of problems with the contractor that we had awarded it to, and we actually unqualified them at that time from bidding. So they have been low bid every year since we've done this and have done a great job completing the maintenance. But keep in mind the only maintenance that we're looking at awarding this evening is just the maintenance of the water. So installation of the pond, treatment of the pond, the water surface area. The landscaping and grass itself is still the responsibility, and I believe that the homeowners association has some sort of rotating schedule within the members that they take care of the grass and maintenance of the trees.

Michael Serpe:

Are the homeowners association entirely keeping up with their portion of the payments on this to maintain this?

Mike Pollocoff:

No, they haven't been. In fact, we received correspondence from them last year that the association as it existed there was an elected individual, he was receiving bills for outstanding services related to the electricity for the pump, for the fountain and for maintenance. And the homeowners weren't paying their share of it. So he said I can't pay this out of my own funds and

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no one is really stepping forward to do it. So under the plat the Village has the ability if there's non-performance by the property owners we can step in, identify the work to be done, put it out to a competitive bid and special charge it back to the affected property owners in that development. And I think what happens is taking care of a pond is a little unusual for most people. This pond needs to be -- you've got to get the pump out there, you've got to maintain it, you've got to make sure you're not tugging on it and pulling the wires out. That's happened before. You need to put a certain amount of dye in the water so you don't have algae growth. There's things to do.

And as John indicated we had another contractor in the area bid it, and they weren't able to do it. And just in the Kenosha area along there aren't a lot of contractors that have that certification to be able to do pond maintenance. So I think that level of maintenance is probably beyond the 11 property owners in that subdivision. So I think it's probably beyond them. I think the price we have I think it's better than what they would have got if they went out on their own. But at least there won't be any taxes on it. And if they continue to mow the grass and keep that down it will help keep the pond in good shape as well, too.

Dave Klimisch:

In the notes it says that the homeowners association went away years ago. Is the grass being maintained. Is somebody besides us having it mowed?

Mike Pollocoff:

As John said they've been trying amongst everybody's kids are taking turns mowing it. But the association didn't go away. They said it went away, but it still legally exists. That's who we're charging for this. But I've noticed this year it may be problematic whether or not they're going to keep mowing it. It's been pretty high as far as the grass growing in there. I think somebody got in and mowed it about a week ago. But if they don't mow it I've indicated to John I want to put a bid out for mowing services for these lots before it gets too high. Rather than issuing a weed complaint and going through that process we'll just get it mowed and put that on the charges as well. They've got money on deposit with the Village for pump replacement and things that we've built up over time. So I don't think it's a problem that there's not enough funds to get the work done. There just doesn't seem to be a will to do the work.

Michael Serpe:

Would it be advisable to get the money up front when a subdivision is approved for maintenance for a ten year period let's say to be reviewed at a ten year period? So in other words you anticipate what the costs are going to be and it's included in the sale of the property. We put that in escrow and if we have to we take care of it? Just a thought.

Mike Pollocoff:

The legislature has taken away that ability for us to do that. Right now we used to have a letter of credit that we could hold that let's say they would pay. And letter of credits are no longer legal. We can only require that a bond be posted. And if it's non-performance the Village would have

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to initiate a suit against the bond company for performance. So the real trick on these things is to keep the nature of the project as small as possible for how much of a bite of improvement costs that the homeowners are going to take on when they buy one of these platted subdivisions and what the exposure is to the Village. Because really the financial securities that exist for the Village now that really got us through the housing collapse at the beginning of 2010 don't exist anymore. That's been altered in favor of home builders.

So what's unusual about this one is the Village was the developer. If you think back the reason we undertook this project and bought that land is we bought a couple parcels of land because we needed to have 60th go through. There was remnant land left over. We used the sales of these lots to pay for 60th Avenue as well as putting that stormwater pond in to handle their water plus diminishing [inaudible] Jerome Creek. So this is a little bit unusual in the sense that we're the developer but we're also the Village that makes sure that the homeowners carry their responsibilities through. We don't have the same tools we used to have.

Steve Kumorkiewicz:

We don't have anything. For the last two years they've taken away everything for us.

Kris Keckler:

Just in reviewing some of this there's still an outstanding invoice for repairs of \$250 that's being tacked on. Are there any late fees that are accumulating with any of these charges?

Mike Pollocoff:

If we let it go to tax roll it would be ten percent. They'll all be invoiced on their individual sewer and water bills. So if they don't pay it there will be a ten percent fee plus any penalties that accrue over that period.

Kris Keckler:

So even though we're dealing with the association it's the individual homeowners that are at the end of this that will be held accountable. And the fact that we only have heard back from half of them.

Mike Pollocoff:

We haven't heard back from any of them really. What we've heard back from them is we don't want to do. And I assume the other ones that we haven't heard from don't want to even talk to us. I think it's just a case where they've really become frustrated with it.

Michael Serpe:

Do we have a motion on this yet?

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John Steinbrink:

This was not a surprise. This was all in the documents when they purchased their properties.

Mike Pollocoff:

Yeah, it was, but in all fairness I think one person built a house that actually bought the lot and came up with a plan. If you think back to 2005 and 2006 it was a speculative market. So almost all these lots were bought by somebody, they built a house on speculation and sold it. And they didn't mind signing the documents saying that these responsibilities existed. Somebody goes to closing, it's one of those things that's in the deed, they buy it and then [inaudible].

Michael Serpe:

John, I'd move for Kenosha Grounds Care getting the contract for \$1,925.

Kris Keckler:

Second.

John Steinbrink:

Motion by Mike, second by Kris. Any discussion?

Dave Klimisch:

Are there any other situations like this at other places in Pleasant Prairie or is this unique?

Mike Pollocoff:

Yeah, this is unique.

John Steinbrink:

Any other comments or questions?

Michael Serpe:

What was the price, John, that we're looking at?

John Steinbrink, Jr.:

The price was \$5,775 for a three year term.

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Michael Serpe:

I stand corrected. Disregard what I see on the board up there.

John Steinbrink:

We have an amended motion and a second. Further discussion that?

SERPE MOVED TO AWARD A THREE YEAR CONTRACT TO KENOSHA GROUNDS CARE IN THE AMOUNT OF \$5,775 FOR POND MAINTENANCE IN BENTZ ESTATES SUBDIVISION; SECONDED BY KECKLER; MOTION CARRIED 5-0.

John Steinbrink:

Do we want to take D and E together or can they be done separately?

Mike Pollocoff:

Separate contracts, but you can discuss them together.

D. Consider an award of contract for the Park and Ride Service Lot Project.

E. Consider an award of contract for construction management services of the Park and Ride Service Lot.

John Steinbrink, Jr.:

Mr. President and members of the Board, this is actually a rebid of a denial that the Board took action on probably about three or four weeks ago, sometime back in that time frame. So originally we were looking at completing construction on the gravel parking lot. Let me back up a little bit. This is on the ball field baseball diamonds at Prairie Springs Park located approximately at Terwall Terrace and State Highway 165. Just Prairie Springs Park by the LakeView RecPlex.

We were looking originally on taking the entire parking lot that we have, pulverizing the section to the right and the center section since they have existing asphalt, completing curb and gutter, lights, improving some of the stormwater improvements. And then also taking the section that's off to the left hand of the screen, and that's the existing gravel parking lot, taking that gravel parking lot, making improvements, curb and gutter, doing the streetlights and all the other amenities and improvements that have to be done in there. And everything would be designed.

And as you see the little structure to the far left is fire station number 3. It's proposed for sometime in the future, but we wanted to make sure when this project came to fruition that we were all in alignment so that the trucks could drive through the main drag in the parking lot, we were going to beef up the asphalt, and they could use that as a parking area or a driving area and work on some staff parking for the firefighters out there.

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And so the bids originally came in much higher than projected and so those bids were rejected. And so we ended up just bidding out the gravel parking lot which is what we have in front of us this evening. And that's just the three tiered section off to the left. And so bids were received until 11:00 on April 28th. And they were publically opened and read aloud. And two bidders put in for it for the prime for the contract, DK Contractors came in with a bid of \$511,609. Payne & Dolan as the prime at \$607,523.14.

And, like I said, this was a project that was bid earlier. It came much higher than what our engineer's estimate was. So we went down and re-evaluated with the administration and thought that what we really don't have to do was the two sections that we have off to the right. And so this is actually an interpretation of what we are bidding now. It's the gravel parking lot and the path and a small section of asphalt underneath the bleachers and extending a sidewalk along Terwall Terrace on the south side and making a couple of ramps improvements. We thought this is something that we could get by.

If you notice on the gravel parking lot where we are having this project take place there is a large area over there. We are looking for a future commercial site to go in there. We haven't had any interest at the time to do it, but we are planning this for an eventual commercial site to have at least a location on that site. And so I guess with all that said we're looking to award the contract as rebid out the second time to DK Contractors in the amount of \$511,609.

Mike Pollocoff:

Just from a semantic perspective on this, like we say this is the park and ride service lot. And the original plans for the park and ride that we just completed the commercial site that proposed initially by DOT when we began that project was up front close to the road. We were going to reroute the stream alongside the railroad tracks and that would be the commercial site that supports the park and ride. Well, ultimately that was impossible with the Army Corps of Engineers and the DNR. We ended up leaving that site with no services permitted there. So this takes the services that were anticipated with the original park and ride lot, moves them down to this other lot, and then we'll finish that lot off. So it's all still part of the Tax Increment District project, but we've narrowed it down.

We thought at one point we'd be able to get a lot more work done with the entire parking lot and have a greater commercial presence along Terwall Terrace. But the numbers just don't make that work. So this in essence really kind of fills our base commitment to the services that support the park and ride with the amount of space we have for commercial use here. And I think as that lot's done then we can begin the marketing of it to fulfill our efforts to get somebody in there. But the commercial market being what it is everybody wants to see something that's done and ready rather than a picture or a map or something that could be done. So I'd recommend that, as John indicated, that the Village Board award a contract in the amount of \$511,609 to DK Contractors for construction of the park and ride. And on the second item tonight, E, John why don't you go through solicitation for construction managers for the project.

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John Steinbrink, Jr.:

Back on February 13, 2015, we did receive bids for the entire project as originally had bid for the component as a whole. We did receive two proposals, RA Smith for \$42,554, and Clark Dietz \$54,630. The original project was rejected as we had just spoken about and rebid at a smaller scope. So we actually went down and negotiated with RA Smith, took off the items that were taken out of the rebid, and they pulled those from the contract price and gave us a new price of \$24,422 for construction inspection services on that project. So I do recommend that the Village award the project to RA Smith for construction management in the amount of \$24,422.

Dave Klimisch:

I've got some questions. The commercial lot is that at the north end of the proposed parking lot?

Mike Pollocoff:

Yes.

John Steinbrink, Jr.:

Yes, it's at the northwest.

Dave Klimisch:

And we're thinking just something to serve the people parking there like a convenience store of some sort?

Mike Pollocoff:

The uses that were presented to us with DNR would be uses -- some uses have been like a restaurant that would be a Subway, Starbucks, McDonald's, whatever, something that would service not only the park and ride users but the people in the corporate park, people that go down Terwall Terrace, RecPlex members. We have I believe it's 1.8 million people that go into the park every year so there's a significant amount of traffic. Usually you see that in some large commercial areas. So we're going to let the market dictate who it is. But that's the type of uses. And they were ranging from dry cleaners. One use that wouldn't be permitted in the park would be a service station. But if there was a sports bar, restaurant, a convenience store, one of those could stand alone.

Dave Klimisch:

Is it a big enough space?

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Mike Pollocoff:

Yeah. We did an evaluation with Tracy Cross & Associates, and they indicated that this site would accommodate a Subway type use, a faster food use as well as a sit down restaurant or a sports bar type restaurant. Now, it would be using up some of the parking lot at peak times, but you could get both those uses on that site, and they felt there was a market for that. Any sale of that use would go back in to retire the bond for the improvements.

Michael Serpe:

So you need an approval for the DK Contractors?

Mike Pollocoff:

Right.

Michael Serpe:

I would make that motion.

Steve Kumorkiewicz:

Second.

John Steinbrink:

Motion by Mike, second by Steve for award of contract for the park and ride service lot project to DK Contractors. Any further discussion?

SERPE MOVED TO AWARD A CONTRACT FOR THE PARK AND RIDE SERVICE LOT PROJECT TO DK CONTRACTORS IN THE AMOUNT OF \$511,609; SECONDED BY KUMORKIEWICZ; MOTION CARRIED 5-0.

John Steinbrink:

And next is Item E.

Kris Keckler:

Move to accept the recommendation for awarding to RA Smith.

Dave Klimisch:

Second.

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John Steinbrink:

Motion by Kris, second by Dave for award of contract for construction management services for the park and ride service lot to RA Smith. Any discussion on this item? Dave?

Dave Klimisch:

Are there bus routes that come through here to serve the park and ride or are there plans for that? Or how are they parking and riding?

Mike Pollocoff:

They have to drive a car. The State would not permit us to provide any spaces for buses or any mass transit. We can have bikes and cars.

Dave Klimisch:

I'm trying to envision how it would be used as a park and ride. For car pooling?

Mike Pollocoff:

People can park at the park and ride that exists, and then take the sidewalks over to this site. The park and ride even though it's built just for cars and we know how it's laid out that there's a potential for a train station there and bus parking. But the State will not permit any municipal governments to ask for any mass transit improvements. So how people get there given the fact we have the wetlands there this is the best way we can get them over is the sidewalks and they would be walking. Or they would be driving the car from the large park and ride over to this park and ride to take care of whatever business they have. I guess if a bus wants to come in there that's on its way to Jelly Belly, and they look at Jelly Belly and they want to stop and have a sandwich we're going to let them stop. But we're not permitted to do anything that would promote mass transit.

John Steinbrink:

We don't make this up. This actually comes from Madison.

Steve Kumorkiewicz:

Surprise.

Mike Pollocoff:

And the only way we could keep our grant was to promise that we wouldn't ask -- they told us if you ask for any mass transit or trains, anything to do with trains we will not process the grant.

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Michael Serpe:

Is there a reason behind their [inaudible]?

Mike Pollocoff:

Well, there's been a policy statement from the Governor that is not supporting rail transit. And mass transit for like buses has been defunded basically outside of the urban cities. And even then the urban cities have had their cut. It's unfortunate because there are a lot of people who would probably like to work in LakeView Corporate Park but they need a bus to get there. They don't have cars. But all that was taken off the table. So for some period of time these guys come and go. This is going to be an auto only. And at some point even though we don't show buses can handle it, I'm sure if a bus wants to come in there we're not going to deny them the opportunity to go to the bathroom and use the restaurant and whatever they need there.

John Steinbrink:

We have a motion and a second. Any further discussion?

KECKLER MOVED TO AWARD A CONTRACT FOR CONSTRUCTION MANAGEMENT SERVICES OF THE PARK AND RIDE SERVICE LOT PROJECT TO R.A. SMITH IN THE AMOUNT OF \$24,422; SECONDED BY KLIMISCH; MOTION CARRIED 5-0.

F. Consider an award of contract for the painting of the Timber Ridge Water Tower.

John Steinbrink, Jr.:

Mr. President and members of the Board, the water utility is budgeted to repaint the Timber Ridge elevated water tower. That's the water tower located approximately 103rd Place and Green Bay Road by the Big Oaks Golf Course. Kind of on the southern end of the Village limits. The water tower was originally constructed in 1976 and holds a volume of 200,000 gallons of water. Recent tower inspection determined that the water tower wet interior and exterior are due to be repainted. In addition, the DNR is requiring us to have this project by August 31, 2015 based on the results of their recent inspection.

A total of five proposals were received for this project as listed on the screen. And the lowest price was from L.C. United Painting for a cost of \$198,000. High bid on that was Classic Protective Coatings for \$389,200. The professional services and inspection of the water tower painting contract was prior awarded to Dixon Engineering on March 2nd at a cost of \$19,871. So the total budget for this project was \$252,000 even.

Painting services will be paid through from the Water Utility enterprise fund. L.C. United Painting is a contractor in good standing with the Village, and Dixon Engineering is our consulting engineer for this. And the staff and Dixon Engineering recommend award of the contract to L.C. United Painting in the amount of \$198,000 to complete this painting project.

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Michael Serpe:

What did we budget for that, John?

John Steinbrink, Jr.:

We budgeted \$252,000, so we're looking at \$198,000 for the painting from the contractor, and just under \$20,000. So so far we'll be processing for around \$220,000 so still \$32,000 under our estimated budget.

Michael Serpe:

Move approval.

Kris Keckler:

Second.

John Steinbrink:

Motion by Mike, second by Kris. Any further discussion?

Steve Kumorkiewicz:

One question for John. John, the paint they use is it an epoxy paint?

John Steinbrink, Jr.:

It is a tmemec paint is the brand of paint that we use. And it's actually a special paint. And one of the things that they'll do with the paint is it actually has like a sun inhibitor. It's like a sealer that they roll on and mix in with the paint. So it's not something you can buy at Menards or Lowe's or any local hardware store. It actually is a special paint that's made just for the application of painting exterior and interior water towers.

Steve Kumorkiewicz:

Thank you, John.

Kris Keckler:

Are we keeping the same theme and colors?

John Steinbrink, Jr.:

Yes, we are. It is going to look exactly the same except the little sun spots are going to be gone, and it will be nice and fresh and look good. And these pictures show some of the rust spots on

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the interior and some of the spots on the outside. I guess one thing they noted that if you look at the tower, especially this tower, you'll kind of see like white blotches that are on there. And what happened is that when they applied sealer the back go around 15 years ago the process is where you blast it off, you put the primer, you put the paint and then you put the sealer. Well, the sealer is clear so you really couldn't see hanging off a tower where it was. And so any spots where that sealer was missed the UV rays actually hit that paint and kind of deteriorated it, broke it down, and just kind of leaving a white spot on there. And so we ended up -- so now the new paint actually has that sealer built right into it and so it should last a little bit longer than what the old process was.

John Steinbrink:

So are we pretty close to the 15 year schedule then?

John Steinbrink, Jr.:

Yeah, we're actually just over the 15 year basis. It was really due to be painted a couple years ago, and so we painted some other towers first. We painted the I-94 tower I believe four years ago, and then two years ago we painted the 165 tower. And so now we're on schedule to have this one painted. And then in a couple years we'll be bringing forward the Ladish tower. And then that will complete all of our elevated storage.

John Steinbrink:

Okay, thank you. We have a motion, we have a second. Is there any other discussion?

Dave Klimisch:

What's the time line?

John Steinbrink, Jr.:

We're looking to have it done this summer. We haven't talked with the contractor yet, but it is going to be a summer paint. And that's one of the reasons why we got a little bit of a better price I believe is because we did allow the contractors to work during summer. The Village holds 2.2 million gallons of storage elevated up in the air. Being the smaller tower that this is we can actually afford to only have 2 million gallons up in the year and so effectively operate our system. Where a lot of places if they just have one or two towers they really struggle with that summer peak demand on there. So we got a little bit of a cheaper price by allowing them to do it when the tower painting really isn't in prime. Most places want the towers painted in early spring, late fall, and they really want to shy away from that peak summer. We're actually allowing them to work in that peak summer time frame. So it will be painted over the course of the summer and then done by our DNR deadline which I believe is August 31, 2015.

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John Steinbrink:

Other discussion?

SERPE MOVED TO AWARD A CONTRACT FOR THE PAINTING OF THE TIMBER RIDGE WATER TOWER TO L.C. UNITED PAINTING IN THE AMOUNT OF \$198,000; SECONDED BY KECKLER; MOTION CARRIED 5-0.

G. Consider the recommendation to reject the bids received for disposal services of Municipal Solid Waste.

John Steinbrink, Jr.:

Mr. President and members of the Board, on April 10, 2015 a request of proposal for municipal solid waste disposal was issued on VendorNet. It was sent out to 111 solid waste management companies. It was also advertised in the *Kenosha News* legal section for two weeks. In addition, bid packets were sent out to seven landfill companies.

In 2014, just to give you a little bit of historical perspective of some of our volumes, the solid waste department collected 6,698 tons of solid waste from the Village of Pleasant Prairie at a tipping fee of \$290,000. On Thursday, May 7, 2015 at 10 a.m. bids were received for municipal solid waste disposal, and only two bids were received. And so we are recommending that -- it was not a comprehensive list of all the landfills within a 30 mile radius. So we are looking at rejecting these bids and rebidding the project.

Steve Kumorkiewicz:

So moved.

Mike Pollocoff:

Before a motion comes we visited with the Village counsel concerning this. And it's one thing since we do know there's other firms that do this, our goal is not to expose anybody else's bids to being viewed by someone else and then having them go back at it again. On the other hand, this is a long-term contract. We know there are other people that do it. And I think that the alternative that our attorney is not comfortable with is negotiating a contract based on with any of the other bidders and using that as a way to get another bid out of the people who haven't bid.

So we've heard a representative from one of the respondents who indicated that they felt they submitted their best bid packet, and I'm glad they did. We expect nothing less, and that there are some things that they would be willing to provide the Village with as far as evaluating how effective their proposal was and why they think it's a better bid. Putting this out to bid again doesn't negate that possibility to do it and evaluate as public works goes through and evaluates the bids from the vendors whether or not that's a good bid. The price certainly we have to consider that, but if there's a way to get that price that's the best bid then I'm sure John is going to look at that.

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But I really think in order to be able to look at this from the best platform from the people we know that do this work I think the fairest thing to do is reject the bids and go out again. And we'll still do that due diligence with the bid packets that were submitted by the various vendors to evaluate who really submitted the lowest and best bid.

John Steinbrink, Jr.:

And, Mike, if I could just say one more thing. When we go through and we do our evaluation the tipping fee and taxes are just one component of how we evaluate. There are some other charges like the lady from Kestrel Hawk had talked about. We do evaluate the actual hauling charge. We have three trucks that have to go to this landfill whether it's 13 miles away or 1 mile away or 100 miles away, wherever that it is. And so we take into evaluation the tipping fee and taxes which is a pretty straightforward. But then we also take the hauling costs, and that includes any Village labor and State rate on our vehicles that we have. That includes the depreciation, the fuel, the oil changes, tires replacement, everything that the vehicle may have. And so we do have pretty comprehensive costs.

So even if all bids would come in for the tipping fee at the exact same dollars, the evaluation would not show an even evaluation because there's other factors that we -- or I should say that there's other costs that it takes to actually get that material to a landfill. And that's absolutely evaluated in our proposal. We did not do that for this one because we did not receive one of the bids that we're currently using. And so we really feel that we need to just rebid it out and then do that comprehensive evaluation. And then come back at the mid June meeting with another recommendation.

Kris Keckler:

Are we to assume that no conditions or criteria will change with the expected bid or rebidding process?

Mike Pollocoff:

They'll be the same.

John Steinbrink, Jr.:

Yeah, the bid document is exactly the same. We're not changing the bid document.

John Steinbrink:

Was the late bid exposed?

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John Steinbrink, Jr.:

It was not. We never opened the late bid. And we have not publically put out any of the other bids, and that's why you do not see them in front of you this evening. So there were two landfill companies that were present at the bid opening. And as far as the Village staff, as far as the bid numbers have gone. It is public information, but we have not let that out at this time yet. And the late bid that was received based on a computer error at their company we have still not opened that bid. And our legal counsel advised us not to so we did not open that.

Dave Klimisch:

I move that we reject the bids.

Steve Kumorkiewicz:

I made a motion.

Dave Klimisch:

Oh, you already did. I'll second.

John Steinbrink:

Motion by Steve, second by Dave.

Kris Keckler:

So somebody could still put in an open records request to view the results of the two submitted on time bids?

John Steinbrink, Jr.:

I believe, Mike --

Mike Pollocoff:

Yeah.

John Steinbrink, Jr.:

Yeah, it's open record so we would have to comply with that. And that would have to be a request put through the Village Clerk, and then there's a time frame that we have to respond to that. So it is our intention to have bids out tomorrow and then have them bid back. I believe bids will be due again, and this isn't out yet, but we're looking at receiving the bids on June 4th. And so it's a pretty aggressive time line, but it really shouldn't take very long to do the entire process. And then we'll have them back to the Village Board for June 15th.

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John Steinbrink:

Okay, ma'am you had a question. I know during citizen's comments you stated your bid.

[Inaudible]

Mike Pollocoff:

Can we get you to the mic?

John Steinbrink:

Yes, please come up. You've waited around here so we're going to let you speak.

Jodi Acheson:

Well, we want your business.

Jane Romanowski:

Name and address for the record again.

Jodi Acheson:

Jodi Acheson, business address 1989 Oakes Road, Racine, Wisconsin. It's our understanding that the other landfill company did have a representative present although they didn't submit it. But there was somebody there from Advanced.

John Steinbrink, Jr.:

It wasn't from the landfill.

Jodi Acheson:

Right, but I would say that they're privy to certainly the rates that were read that day without evening [inaudible]. It really puts the other folks at a competitive disadvantage.

Mike Pollocoff:

Well, in a sense it's a rebid. Everybody has got the chance to look at how they want to present their bid again and redo it. The very best we can do in a situation like that is what we have, make an effort or do what we can to keep it quiet. But we're a public entity. If somebody really wants to see this information we have to share it and we will share it.

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John Steinbrink:

And I expect we are going to be proceeding on the advice of legal counsel on all these.

Mike Pollocoff:

Yes.

Kris Keckler:

The unopened late that is then returned or destroyed?

Mike Pollocoff:

Returned. We don't want it in house so it goes back.

Steve Kumorkiewicz:

I've got a question for Mike, John. If these bids weren't opened we don't know [inaudible] release it to somebody else?

Mike Pollocoff:

No, if the bids aren't opened, like the bid from the one company that submitted late, we don't want that bid. That bid goes back to the vendor. If all the bids for whatever reason we decide that we're not going to open them, we want to change our [inaudible] or whatever, the statutes are really clear that the Board's really single option in dealing with these nuisances or things that occur with the bidding process is to reject all bids and start again. That's really the one tool you have to yourself. If they're sealed we don't keep them. If they're still sealed at the time of the opening, and for whatever reason we don't open them, they go back to the vendor. So we don't keep those and they're not subject to open records because the document hasn't come to fruition that we can act on. So it's going back to where it started, and the vendor can resubmit again.

Steve Kumorkiewicz:

Thank you.

John Steinbrink:

Again, acting upon legal advice. We had a motion and a second, is that correct? Any other discussion?

KUMORKIEWICZ MOVED TO CONCUR WITH STAFF'S RECOMMENDATION TO REJECT THE BIDS RECEIVED FOR DISPOSAL SERVICES OF MUNICIPAL SOLID WASTE; SECONDED BY KLIMISCH; MOTION CARRIED 5-0.

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John Steinbrink:

Thank you for coming down.

H. Consider an award of contract for the recycling of electronics.

John Steinbrink, Jr.:

Mr. President and members of the Board, on April 10, 2015 an RFP for electronics recycling was issued to VendorNet. It was sent out to 114 vendors. It was also advertised in the *Kenosha News* legal section for two weeks. In addition, the request was sent to 25 State of Wisconsin registered electronic recyclers as approved by the Wisconsin DNR.

On May 11, 2015 bids were publically opened and read aloud. Three bids were received, and they were evaluated on proposed costs and rebates offered by each recycler on the type of the electronics material, and we did attach a bid analysis sheet. So what we did was we took the volume of electronics recycling as a report as is broken down, and we took how much would that be per an 8,000 pound truckload. That's the average amount of weight that we can haul within a half a semi load of electronics recycling. And so we kind of prorated our historical information, calculated what the cost would be, and that's how we kind of came up with the total number on the right hand side of the screen up there. So even though there is a cost to remove all these electronics, it's just a smaller cost for universal recycling.

And so that's why we're looking at recommending a contract for electronics recycling to Universal Recycling. We have used them in the past. We had another company that came in a little bit lower in bid I believe it was three years ago. We've been having a lot of problems with that company. And actually that company was unqualified by the DNR, and so we stopped using them obviously, put it out for rebid, and now we're looking for a new contract with Universal Recycling to take all the Village's electronics. And those are the things at the Village drop off at either compost site located at 8600 Green Bay Road, or we can go and pick them up at the curbside for residents. It's generally TVs, computer monitors, miscellaneous electronics, computers, laptops, and then there is a fee for transportation. And then the supplies are the boxes and the pallets that we use to temporarily store them in our electronics recycling shed also located at the Prange.

Mike Pollocoff:

So I'd recommend approval of the bid for electronics recycling.

Kris Keckler:

Move to accept.

Steve Kumorkiewicz:

Second.

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Kris Keckler:

I just had one question. In your three year historical, for some reason 2012-13 had a 50 percent increase in amount in quantity. Was it just an increased collection year?

John Steinbrink, Jr.:

I really have no idea why those numbers are up or down. We've really been marketing about the same. We have the same collection process per se. So why we received that many more pounds I really don't have an honest answer for you.

Kris Keckler:

Thanks.

John Steinbrink:

People like me that horde this stuff finally one day say I've got to get rid of it.

Kris Keckler:

I did notice the mention of vintage technology.

John Steinbrink:

Vintage technology, yes. Either the mouse has died or something. Okay, we had a motion by Kris, second by Steve. Question by Dave.

Dave Klimisch:

So I'm looking at the bids. Are the fees that we collect from the people that use this is that reflected in here?

John Steinbrink, Jr.:

No, the revenues are not included on there because those revenues are consistent across the board. So we had the same revenues, we wouldn't charge any differently per se. And so these are just the fees that we would be paying to the three responsive bidders.

Dave Klimisch:

Do you know about how much we get?

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John Steinbrink, Jr.:

I do not have that number off the top of my head, but that's something that I can find out. It does cover the cost of the program. But I do know it's under \$20,000.

Dave Klimisch:

And maybe this is for Mike. I'm just thinking have we looked at other municipalities? We have a fee. Other cities if they charge more or less do they get more or less response? Is our fee structure set to get the most recycling?

Mike Pollocoff:

Our fee structure is set to manage our overall fee that we charge the residential users to stay within that range that we're at. So as far as the electronics recycling, one thing about that we do it, but then there's also other groups that do it as well. So what we try to accomplish in solid waste, and we do this in the budget process, our two major goals are solid waste collection and recycling collection. And the average household there's some steps that occur, but it's \$16.50 a month. So out of that \$16.50 a month we want to be able to recover the cost of solid waste, recycling, have that all set somewhat by the recycling rebate, leaf collection, operation of the compost site. And then the collection of hazardous waste we put that in the budget last year.

But things such as batteries, oil, the electronic recycling those kind of float. We do charge a fee for it, but right now the enterprise runs a surplus. We're hitting our positives on our revenue. And this is the first as a matter of fact that utility has no debt. We've retired all the debt in that utility, and we just recently purchased two trucks with cash that we've set aside in reserve. And as far as I think the effort that goes through for the televisions, monitors and things like that it's not that great. It's not really taxing us that much. We haven't made an effort to do more of that. What we've done is, and I hate to say this publically, but we've done enough to meet our required statutory needs to say we have a place where people can do it, and this is what it costs. What we've really been doing is working to manage those big areas of solid waste, recycling and leaf collection to manage those numbers. Because those are numbers if we don't keep those under control that will start really jacking our rates up for the residential users that pay us.

John Steinbrink, Jr.:

And, Mike, if I could just add also, the main component of the cost of the electronics recycling isn't really disposal of the electronics. It's actually going out and collecting them, it's managing them while we have them within our possession. That really is the lion's share of the cost, not so much the disposal. We need to get rid of it, but the cost of us collecting it or managing it or moving it around for the recycling storage or the semi to come in is the majority of the cost. And really the recycling which you see for 8,000 pounds to only pay \$400 is a pretty nominal fee per pound.

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John Steinbrink:

Motion and a second. Further discussion?

KECKLER MOVED TO AWARD A CONTRACT FOR THE RECYCLING OF ELECTRONICS TO UNIVERSAL RECYCLING SECONDED BY KUMORKIEWICZ; MOTION CARRIED 5-0.

I. Consider Ordinance #15-19 to amend Chapter 285 of the Municipal Code relating to Sewer Service Charges.

Kathy Goessl:

Mr. President and Village Board, back a couple months ago we received a letter from Kenosha Water Utility notifying us that there is going to be a three percent rate increase in both the sewer utility and the water treatment and purchased water from them. The water part goes to the Public Service Commission, and we've filed that with them to pass that three percent rate increase onto our residents. And that as approved by them this past week. The effective date of the Kenosha Water increase is June 1st, so we're asking for June 1st also for the Public Service Commission. Their rate increase was a little over one percent, a little over 30 cents per month for the average residential customer.

The sewer portion did not go through the Public Service Commission, but it's an ordinance that we have in the Village. And this here now to present the rate increase to pass onto our residents for the sewer part. Up on the overhead is the actual different components of our sewer rates, and this is like a chart taken from our ordinance Section 285. It shows the old rates and the new rates. We have the volume charge which we're looking at 6 cent per thousand gallon increase. An average residence discharges about 6,000 gallons. And then a monthly customer charge which is based on the meter size, their water meter size. I've listed all those different sizes and the different rate increases there, but the one that pertains mainly to resident is the 5/8 or 3/4 meters which is going up 15 cents per month.

So all the rest of the meters are mainly commercial industrial meters. And you can see the respective rate increases for them also. The average residential would go up 53 cents a month. The rate increase we're getting from Kenosha is three percent, but this equates once we figure out our cost and how it relates to our overall operations only comes to 1.11 percent which is 53 cents per month. As I said before the water is a little bit less, it's like 30 cents, 30 some cents.

The next slide shows we're also looking at increasing the industrial surcharging. This pertains to commercial industrial. It doesn't affect our residential customers. So we're also looking at a 1.11 percent increase also on these charges. These would be effective if approved June 1st just like the water for the Public Service Commission would be, and the rates being passed to us from the Kenosha Water Utility.

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Michael Serpe:

I have a question. This increase although it's not that much [inaudible] amount of sewer that Kenosha is treating is has to do with the operation of what they're doing, is that correct? Because I think with all the improvements we made with the I & I correction on Cooper Road and elsewhere we should be sending less sewer to the City than we were years ago.

Mike Pollocoff:

And I think we are in the case of sewage. So the way the City's looked at that is we're not charging Pleasant Prairie as much. As we decrease our flow profiles of what's going in there they're saying, well, we still need money. Now they're not collecting as much from Pleasant Prairie so that's the bad news. The good news it they're charging across the entire rate base, so Pleasant Prairie doesn't have to make up that whole spread itself, and the same thing with the water.

So when we first dealt with the Kenosha Water Utility a couple years back they initially asked, and I'm going from memory here, but I think it was a 14 percent increase. We objected, and then they increased it to 34 percent. And we went to the Public Service Commission, and the Public Service Commission made it 10 percent. It wasn't 14, it was 17 percent. So we knew going into this that Kenosha Water Utility, and as you remember it was a protracted case. They wanted their rates to go up 17 percent. So they got 10 percent from us so they went up 3 percent, and we feel that they're going to go back in for another -- they effectively said they're going to back for another 3 percent next year so until it gets them to the 16 percent to where they wanted to be.

In the intervening time there's been a major water user that's come to the Village that's buying their water from the City of Kenosha. And the amount of revenue that that user places for the City of Kenosha, because Kenosha is really not providing any additional infrastructure for this user, is really helping their bottom line. So I think next year we're going to take a look and see what their rate of return is having a full year of operations for Niagara. And I think at that point we go back and raise a question, one, do you really need seven and a half percent or anything more of a rate of return to do what you're doing. What are you doing with this money? Are you using it for other purposes?

And I think the other issue with the sanitary sewer, and I think this would be a little more problematic, but I think one of the questions the Village has to come to grips with and ask them is given the amount of work that Pleasant Prairie has been doing on inflow and infiltration and managing that, and managing the BODs solids and phosphorous that's coming to the Kenosha Water Utility and the level of effort the City hasn't been engaged in on their one I & I, is this rate really rational and is this something that's just being arbitrary, or are they using the Village of Pleasant Prairie to generate additional revenue? That would also have to go to the Public Service Commission.

And in the case of sanitary sewer if we don't feel that the Commission's response is complete or accurate or whatever, then that can be appealed to Circuit Court. So it's a step we don't want to take lightly. But I think if you look at those charges that we're paying, that the Village residents

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are paying, they're significantly higher than what the City of Kenosha users are paying. And, again, we're Kenosha's largest waste water customer, and we're their largest water customer. So I'd recommend both ordinances, Chapters 285 and 194 be amended as requested by the Finance Director and set our rates to take effect June 1st of this year, or rate increases to take effect June 1st of this year.

Michael Serpe:

Move approval of Ordinance 15-19.

Steve Kumorkiewicz:

Second.

John Steinbrink:

Motion by Mike, second by Steve. Any further discussion? Dave?

Dave Klimisch:

I just have to give a shout out. There's a lot of complex stuff in this water scenario. And when I read a copy of The Village this front cover summarized all of the issues beautifully and succinctly. So Christine and the team that makes this I thought it was very nicely done.

Mike Pollocoff:

She makes me sound smart all the time.

Steve Kumorkiewicz:

I've got a question for Mike. This \$47.84 that's the rate we're going to charge to the people that are connected to the water but do not have sewer?

Mike Pollocoff:

Well, we charge all sorts of different rates. What that is is what the average rate is, the 6,000 gallon per month user. Some people use less, some people more, some people pay more, but for that average user that's it.

Kathy Goessl:

So if they're on a well they'll be charged the 6,000 gallons, the average that I have up there.

Steve Kumorkiewicz:

That's what I was talking about, yes. Okay, thank you.

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John Steinbrink:

Anything else?

SERPE MOVED TO ADOPT ORDINANCE #15-19 TO AMEND CHAPTER 285 OF THE MUNICIPAL CODE RELATING TO SEWER SERVICE CHARGES; SECONDED BY KUMORKIEWICZ; MOTION CARRIED 5-0.

J. Consider Ordinance #15-17 to amend Chapter 194 of the Municipal Code relating to Intoxicating Liquor and Fermented Malt Beverages.

Jane Romanowski:

I'm proposing a few amendments to this chapter regarding intoxicating liquor and fermented malt beverages. Our current ordinance incorporates Section 125 of the State Statutes regarding the sale and the use and the transferring of alcohol. And in our ordinance you could never include all those regulations and rules or scenarios. So our ordinance obviously touches on the most common sections of Section 125. And quite honestly the liquor laws are as convoluted and extensive and the election laws and they change rapidly. So some of the ordinance amendments are for maintenance and cleanup, and some of them are proposed amendments for new changes.

So I'll just go through the memo real quick. For Section 194-2 we're specifically adding a clause that is allowed by statutes that we should have in our ordinance for employees of the Village or officials to sell fermented malt beverages at the RecPlex or Prairie Springs Park for Village sponsored events. And it's actually for public parks, but Prairie Springs Park is the only park that alcohol is allowed to be sold with a permit. So that's just referring back to our parks license.

In 194-3 I deleted the paragraphs listed, and that comes back later in the ordinance. I'd like to add a fee of \$10 to create a duplicate operator's license. We've had problems with this, or just lots of requests, people going back to get a duplicate license. So we think that's warranted, and it is done in other municipalities.

The biggest change I'm proposing is having the Village Clerk's office issue operator's licenses. Now, I've given you quite a bit of information in your packet, the legal opinion by the League, and then I referred back to our matrix so to speak, our guidelines that were put in place back in 2008 at the request of former Police Chief Brian Wagner which have worked very well. They became a part of our ordinance. They are followed succinctly. And as you can see from 2009 to 2014 we've issued 534 operator licenses and only 7 applications were denied based on the recommendations of the Police Chief and myself. Basically the Police Chief. They're doing all the checks over there, and they're following our guidelines in giving his recommendations.

So I'd like to propose that the Village Clerk's office issue these as they come in rather than holding onto them, making copies for the Board, just adding another license that can be issued out of my office. And there also are paragraphs 6 and 7 were added to establish those guidelines if a denial was taking place and for them to come back to the Board to be reconsidered.

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194-3 M, Provisional Retail Licenses. All this language is in the statutes but we had an instance in December where I had to issue a provisional retail license. It wasn't listed in our ordinance under a paragraph although it is, again, under Chapter 125 allowed. So I'm just requesting that we document that in the ordinance because we have used it now.

And then we go back to 194-3 N, and that's where I took the information off a previous paragraph and it was expanded. We're getting more requests for these temporary Class B, they're called picnic licenses for certain clubs to apply for and receive. And we've issued four of those in the last only two years actually. One of them was for the Music in the Park, the Lemon Street Gallery held that license. One went to the Kenosha Chamber of Commerce for Business After Five. And St. Anne's has requested two of them. One was for a music event, and one was for a nightly music event, very minor. So we've added some additional regulations to that paragraph. And also they get a whole packet of information if they come and apply for a license like that.

194-3 O this is another item that I've been wanting to add into or proposing to add into the ordinance - requesting a \$100 late fee be charged for any licensee who is late in turning in their paperwork. By law they have to have their paperwork to us by April 15th. And year after year we have the same offenders. And I'm calling, writing letters and trying to take care of them. And this will go in the renewal letter. Germantown does it. I know a lot of other municipalities do it. And I bet I won't have any more late applications or if I do it's worth the effort I have to put into it.

We're deleting that paragraph E as it's pretty clear that paragraph is no longer needed. That was just when that ordinance with the new guidelines was adopted. We've expanded on the tax delinquency and claims splitting out premise and person under subcategories. And then we added the forfeiture collections which is allowed by statute which was not included in this paragraph, but it's very important that I go to the courts as well and find out if anybody has outstanding forfeitures. 194-6 F2 conforms directly to State law. They keep changing that paragraph as to where you can serve alcohol. I think they just added painting studios which is something new. And they took out something else. So we're just updating that information.

And then Section 194-3 P that was a reserve section. The Board rescinded an ordinance back in 2010 for outdoor extensions of alcohol or licenses. And so I used that paragraph. We get lots of questions about what can happen with the premise description. Very important with the license, where alcohol can be stored, sold, consumed or served. And I just thought it was important that we reiterate that that is your premise. So if somebody is a first story only and they happen to walk out the backyard with a beer or they put upon a tent or something, it's not on the premise, it's not allowed. They can get ticketed for it. It's just something that comes up quite a bit. I think it's important to just kind of place it in here, and we can refer to it even more than just Section 125 of the Statutes.

And then also any licensee can come in mid year and ask for a premise change. It's always at the Board's discretion. But then we've had some requests that have come through for maybe a special event for like a one shot deal. Radigan's had their anniversary party I think it was last year or the year before. The Board allowed an outdoor extension just for that certain time for that

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certain premise to be added. So I think this kind of allows at the Board's discretion, not setting any precedent, if something like that would come in that you could amend the licensed premise. And then a few other minor changes. And both Mike and the Chief have looked at the ordinance amendments and concur with my recommendations. So I can answer any questions if you have them.

Michael Serpe:

I have a couple of questions.

John Steinbrink:

First we had a motion, right?

Kris Keckler:

Move to accept Ordinance 15-17.

Steve Kumorkiewicz:

Second.

John Steinbrink:

Motion by Kris, second by Steve. Now we can have comments or questions. Mike?

Michael Serpe:

I'm satisfied with this as long as the police department is doing the backgrounds and we have the option -- if the person is denied they have the option to appeal it to the Board which is good. Caterers, say a caterer comes from Racine.

Jane Romanowski:

That's a whole other --

Michael Serpe:

It's really got to be addressed at some point. Not by us but State law.

Jane Romanowski:

State law does address it.

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Michael Serpe:

What do they say?

Jane Romanowski:

I actually couldn't even tell you right off the bat.

Michael Serpe:

I don't think anybody else knows either.

Jane Romanowski:

I've read about it. We haven't ever had a caterer's license. But it's basically where the point of service is where you sell that alcohol. So you sold it I believe on their licensed premise. If you're having a contract for a wedding and you go pay for it at the caterer's, they can sell it to you with their license from Racine. So it's a whole -- you can't even incorporate all that happens with Section 125 into our ordinance. And we haven't had to deal with that yet. But then again . .

Michael Serpe:

It might come.

John Steinbrink:

Other questions? Kris?

Kris Keckler:

Real quick. In your historical chart every two years the number of licenses issued that you provided, in '12-'14 it was the lowest amount over the last several years, but then it's spiking up for '13-'15.

Jane Romanowski:

It kind of flips. It flips one way and then the other way every two years.

Kris Keckler:

I didn't know if that was tied to the forecasting with the annual budget.

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Jane Romanowski:

No, I think it happened when we made it a two year license instead of a one year license. I think that's how it happened. I don't remember. But it seems like every other licensing term it goes higher and lower.

Kris Keckler:

Okay, thanks.

John Steinbrink:

Other questions?

Dave Klimisch:

What's the cost of the license?

Jane Romanowski:

It's \$65 for a two year license.

Dave Klimisch:

And then a \$100 late fee after all of your reminders?

Jane Romanowski:

Oh, no. An operator license we're not going to charge a late fee. The late fee is going to be for the liquor licensees for your taverns and your restaurants.

Dave Klimisch:

And how much is that?

Jane Romanowski:

It depends what kind of license they have. There can be your Class A is \$500, Class A intoxicating. Class B beer is like \$100, then you go \$250. So they're all over the board.

John Steinbrink:

Don't be late.

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Jane Romanowski:

It's been a problem for 20 years.

John Steinbrink:

Other questions or comments? Hearing none we have a motion and second.

KECKLER MOVED TO ADOPT ORDINANCE #15-17 TO AMEND CHAPTER 194 OF THE MUNICIPAL CODE RELATING TO INTOXICATING LIQUOR AND FERMENTED MALT BEVERAGES; SECONDED BY KUMORKIEWICZ; MOTION CARRIED 5-0.

K. Consider Ordinance #15-18 to amend Chapter 214 of the Municipal Code relating to Licenses and Permits.

Jane Romanowski:

And this is just updating the general licensing and permit ordinance because of what was just adopted in 15-17. So a duplicate license fee is added. The premise description, person description on the text, the delinquency has been added. And in this section it indicates a dog license issued under the statute is not subject to the forfeiture check. I'm not quite sure but that's how the law reads. And then also the two licenses, the operator license and temporary Class B beer and wine licenses can be issued out of the Village Clerk's office. Just general licensing update.

Michael Serpe:

Move approval of 15-18.

Kris Keckler:

Second.

John Steinbrink:

Motion by Mike, second by Kris. Any further discussion on this ordinance?

SERPE MOVED TO ADOPT ORDINANCE #15-18 TO AMEND CHAPTER 214 OF THE MUNICIPAL CODE RELATING TO LICENSES AND PERMITS; SECONDED BY KECKLER; MOTION CARRIED 5-0.

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L. Consent Agenda

- 1) **Approve Operator License Applications on file.**
- 2) **Approve a Certified Survey Map to subdivide the property located at 2629 89th Street.**
- 3) **Approve a lot line adjustment to add 30.03 feet from a vacant parcel to 3514 93rd Street and 3516 93rd Street.**

Michael Serpe:

Move approval of consent agendas 1, 2 and 3.

Dave Klimisch:

Second.

John Steinbrink:

Motion by Mike, second by Dave for approval of consent agenda items 1, 2, and 3. Any further discussion on any or all of those items?

SERPE MOVED TO APPROVE CONSENT AGENDA ITEMS 1-3; SECONDED BY KLIMISCH; MOTION CARRIED 5-0.

10. VILLAGE BOARD COMMENTS

John Steinbrink:

Today we have a luncheon at the public works, and this was to recognize public works week and also to recognize some individuals for the service time they have with the Village. John, do you want to mention those folks?

John Steinbrink, Jr.:

Every year at public works we do recognize any of our individuals who have been there on a five year increment. Just going off of memory here we had Lora Laas, one of our clerical secretaries that does all of our billing has been there five years. She's been doing a great job. We have Al Zirbel who is our Village mechanic who has been doing an excellent job. Bob Hagar one of our garbage guys that does the route on a regular week. We had myself, I've been there 20 years. Kevin Meyers and Dennis Kammerzelt both have been there 25 years, so that's quite a big accomplishment. And so we did recognize everyone with a lunch and even had some cake today. So it was a nice lunch and everybody had a good time. And I want to thank everyone that showed up today for that.

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John Steinbrink:

Once again to recognize those people that do such a good job for us plowing snows, maintaining our utilities, picking up garbage, all those things that people really come to expect from the Village and appreciate it.

John Steinbrink, Jr.:

Thank you very much. I'll make sure to pass those comments along.

John Steinbrink:

Also, there was an award at an APWA event. Mike or John, do you want to tell us about that?

Mike Pollocoff:

The Village received an award for the park and ride project. As I indicated before it was a really difficult project to do because of the design constraints, wetlands. Every environmental regulator between here and D.C. was pushing and pulling at this. But it's a project that happened. And what was unique about this project from my standpoint is if you think back we had a single bid for construction of the bridges themselves, really high number.

And when we were looking at it John told me I can do it for a lot less than that. And I'll be darned he did. So public works crews built the bridges, two bridges. These are just not small bridges. These are heavy bridges. They built both the bridges. It came in \$100,000 under budget. The DNR said it was one of the best controlled projects for erosion control. The contractor could never blame us for being late because we were ahead of the contractor that was doing the paving and the other work. So it was one of the projects that won a State award. And I believe it goes off to be considered for a national award.

But for the people that actually saw that project evolve it was an amazing project, and public works did a really good job on it. I think John had more fun than anybody. He was out there in his tank top and shorts playing in the creek and moving concrete panels that were being lifted by a crane into place. So it was a nice project. I just got to go up there and get my picture taken, but the other people were really doing the work on this. So it's something that the Village can be proud of for a long time to come and the guys that worked on it from public works. So it was nice to see them recognized for it.

John Steinbrink:

Any other Village Board comments?

Kris Keckler:

Safe travels for Memorial Day especially around construction sites.

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John Steinbrink:

And we have one here so be careful and don't speed through the neighborhood behind the Village Hall. I know this is a school night and Dave and Kris have to get home early.

11. ADJOURNMENT

**SERPE MOVED TO ADJOURN THE MEETING; SECONDED BY KLIMISCH;
MOTION CARRIED 5-0 AND MEETING ADJOURNED AT 8:10 P.M.**

MEMORANDUM

TO: Village Board of Trustees

FROM: Jane M. Romanowski
Village Clerk

DATE: June 11, 2015

RE: Liquor License Renewal Application – BP AM/PM

Syed Hussain, Agent, for R & D #IV, Inc. submitted an application to renew the Class “A” Fermented Malt Beverage License for the BP AM/PM station located at 10477 120th Avenue, Pleasant Prairie. On June 1, 2015 I recommended to table consideration of this application pending the June 8th Plan Commission meeting wherein the status of the conditional use permit was to be discussed. The conditional use permit was approved by the Plan Commission subject to the payment of the outstanding invoices by June 22, 2015 at 5 p.m.

The Building Inspection and Fire & Rescue Departments have indicated there are no outstanding code violations at the BP facility. The Community Development Department indicates there are issues with the operation of DSIS Cameras, landscaping/site clean-up, parking lot paving and secondary monument sign work.

I recommend the license for the BP AM/PM be renewed for the July 1, 2015 through June 30, 2016 licensing term subject to the following conditions:

- Payment of license and publication fees.
- Payment of the delinquent 2014 real estate taxes in the amount of \$59,373.48,
- Payment of outstanding invoices in the amount of \$19,360.96.
- A resolution of the Community Development Department issues listed above.

The Village Board has the option to conduct a revocation hearing at any time if concerns or violations arise.

* * * * *

Resolution No. 15-20

**Resolution in Memory and Celebration of
Barbara C. Peterson - RecPlex Member # 1**

WHEREAS, Barbara C. Peterson, with her husband William Peterson, made her home in Pleasant Prairie and lived here for more than 60 years; and

WHEREAS, Barbara C. Peterson became the first official member of Pleasant Prairie RecPlex upon its opening in the year 2000 and was a member for the remainder of her life; and

WHEREAS, Barbara C. Peterson was known to be a valued volunteer and an outstanding member of the community; and

WHEREAS, Barbara C. Peterson was also known to be a passionate advocate of living a healthy and balanced life and was a strong supporter of Pleasant Prairie RecPlex; and

WHEREAS, the Village of Pleasant Prairie would like to acknowledge and express sincere thanks for Barbara C. Peterson's many years of membership at RecPlex and for her support of its mission to serve as a community gathering space and to provide healthy recreational opportunities to members of our community.

NOW, THEREFORE BE IT RESOLVED, that the Village of Pleasant Prairie does hereby wish to remember and celebrate Mrs. Barbara C. Peterson, RecPlex Member #1. The Village also wishes to extend our most sincere respect and appreciation to Barbara C. Peterson for all that she has done to build up our community.

Considered and adopted this 15th day of June, 2015.

John P. Steinbrink, President

Attest:

Jane M. Romanowski, Clerk





KENOSHA AREA CONVENTION & VISITORS BUREAU

2014 Annual Report



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A Message from the Chairman & President

Tourism in the Kenosha Area means a fun and memorable family vacation, sports competition, or couple's getaway to the numerous travelers who visit each year. Meanwhile, to those who live and work here, tourism means jobs, lower personal taxes, and an improved quality of life.

Working together with local businesses, community leaders, and local government, we are helping to drive a positive economic force to ensure a bright future for our community. According to research by Tourism Economics, an Oxford Economics Company, tourism expenditures were an estimated \$181.3 million in 2014, a 7.9% increase over 2013. Visitor spending has grown an average of 6% annually since 2010.

Our team is proud of these results and they can be attributed to the partnership and cooperative efforts of our local units of government: City of Kenosha, Village of Pleasant Prairie, Village of Bristol, and Town of Wheatland, plus more than one hundred local hotels, restaurants, attractions, and retail establishments that come together to represent the many unique experiences that bring visitors to our community year after year.

Special events play a key role in allowing visitors to experience our community and Kenosha Area partners came together to host many wonderful existing and new events in 2014. The Wisconsin Marathon hosted over 3,500 athletes in its 5th Annual event. The Salute to Freedom showcased the Civil War Museum and Kenosha's beautiful lakefront. The Pleasant Prairie and Iron Girl Triathlons hosted over 3,000 athletes in Prairie Spring Park. The Taste of Wisconsin greeted over 43,000 people to Kenosha's lakefront. The Pleasant Prairie RecPlex hosted over 30 events in the Aqua Arena, as well as figure skating, hockey, and basketball events that drew visitors from around the Midwest.

We look forward to working with our partners to grow tourism in the Kenosha Area. We will continue to provide



exceptional service to the thousands of travelers visiting our I-94 Visitor Information Center. We'll carry on our commitment to support the revitalization of Downtown Kenosha and assist local businesses reach their goals. We will also continue to partner with organizations to grow and develop events with a regional and national draw.

Please take some time to review this report and our involvement in the Kenosha Area. All of us at the Kenosha Area Convention and Visitors Bureau take great pride in showcasing our community to visitors throughout the world. We look forward to continuing our existing partnerships and forging new ones within the community; because together we achieve great things for the Kenosha Area.

Sincerely,

Michael Pollocoff
Chairman, Board of Directors

Dennis A. DuChene II
President



Who We Are



The Kenosha Area Convention & Visitors Bureau (KACVB) is a private, not-for-profit corporation and is the official tourism promotion agency for Kenosha County, Wisconsin. The KACVB is the only marketing organization whose sole purpose is to promote the Kenosha Area as a travel destination, and thereby contribute positively to the greater Kenosha Area economy. In its role as a destination marketing organization (DMO), the primary goal of the KACVB is to attract travelers who will spend time in the Kenosha Area and patronize its hotels, restaurants, attractions, retail stores, and other businesses. The KACVB promotes the Kenosha Area to key travel markets in the Midwest using a number of strategies including advertising, social media, e-marketing, public relations, and customer care efforts. As publisher of the Official Kenosha Area Visitors Guide and companion website VisitKenosha.com, the KACVB proudly represents and supports more than 100 local businesses and organizations – our Tourism Partners – that cater to travelers. KACVB’s primary target market is the Leisure Traveler, defined as individuals traveling for business or leisure, as well as families or small groups traveling together for leisure activity. It also focuses on specialty target markets including Sports, Meetings & Conventions, and Motorcoach/Group Tours.

Kenosha Area CVB Mission

The Kenosha Area Convention & Visitors Bureau (KACVB) was established in 1986 with the express purpose of increasing visitor appreciation of and spending in Kenosha County, Wisconsin, thus improving the economy of the region.

The KACVB promotes the Kenosha Area as a fun, friendly, refreshing and affordable overnight tourism and meeting destination to individuals, groups, and businesses throughout the world.

Funding

The Kenosha Area Convention & Visitors Bureau's operating budget is wholly funded by the Transient Occupancy Tax – commonly known as “room tax” – that is collected when a person purchases a hotel/motel room in a municipality that has established a room tax ordinance. Municipalities within Kenosha County which have this ordinance in place are: the City of Kenosha, Village of Pleasant Prairie, Village of Bristol and Town of Wheatland. Lodging properties within these communities remit room tax revenues to their respective municipal government, and a portion of those revenues is redirected to the KACVB to support tourism promotion and development. The KACVB's budget is entirely dependent upon hotel/motel occupancy, and is therefore directly related to the results it achieves in marketing the Kenosha Area as an appealing overnight travel destination.



2014 Board of Directors

CHAIRMAN	Michael R. Pollocoff <i>Village of Pleasant Prairie</i>
VICE-CHAIRMAN	Frank Pacetti <i>City of Kenosha</i>
SECRETARY	Paul Whiteside, Jr.
TREASURER	Larry Nelson <i>Bane-Nelson, Inc.</i>
	Paul Hegland <i>Carthage College</i>
	Margaret Hodge <i>Regner Veterinary Clinic</i>
	Kathy Jalensky <i>Jalensky Real Estate Services, LLC</i>
	Marty Stewart-Huff
	Paula Touhey
	Carol Willke <i>Village of Pleasant Prairie</i>

2014 Operating Staff

Dennis A. DuChene II <i>President</i>
Deanna Goodwin <i>Marketing Director</i>
Eva Hoey <i>Visitor Information Center Director & Group Sales</i>
Kris Jensen <i>Sports Development Director</i>
Meridith Jumisko <i>Public Relations Manager</i>
Laura Tyunaitis <i>Marketing Coordinator</i>
Brigit Correa <i>Customer Service Coordinator</i>
Samantha Sullivan <i>Customer Service Coordinator</i>
John Gulig <i>Travel Information Specialist</i>
Jake Hoey <i>Travel Information Specialist</i>
Bill Marshall <i>Travel Information Specialist</i>
Steve Rawson <i>Travel Information Specialist</i>
Brian Wajda <i>Intern (Carthage College)</i>



2014 Tourism Highlights

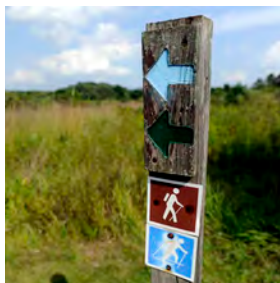
- Tourism spending in Kenosha County reached \$181.4 million in 2014, an increase of \$13.3 million or 7.9% over 2013.
- The 29th edition of the Kenosha Area Visitors Guide was published featuring a 64-page, full-color format and a circulation of 100,000 copies. Ninety-five area businesses purchased advertising in the 2014 Guide.
- The KACVB's website VisitKenosha.com received a makeover with a fresh design, updated navigation and reorganized content. The site continued to break records, achieving 372,028 visits in 2014, a 25% increase over 2013, as well as more than 1.1 million page views, a 22% increase over prior year.
- The Kenosha Area CVB Blog completed its first full year, publishing over 60 posts written by all members of the KACVB staff. The blog posts received 8,915 views by the end of 2014.
- The KACVB Facebook page ended the year with 10,043 Likes - nearly double the amount at the start of the year. KACVB's social media channels generated more than 23,000 clicks to the VisitKenosha.com website in 2014.
- KACVB's public relations efforts netted 61 media placements in non-local markets in 2014, with an earned media value of \$115,660. Local media efforts garnered 323 placements with value of \$85,339.
- 61% of KACVB's Visitor Survey respondents indicated their primary reason for visiting was a weekend getaway or vacation, followed by 24% who were visiting friends and family and 22% attending a special event. 100% of respondents said they would recommend the Kenosha Area to family and friends and 77% indicated they plan to visit the Kenosha Area in 2015.
- The KACVB's two Visitor Information Center locations served 38,432 traveler parties, with more than 50% hailing from Illinois.

- The Kenosha Area CVB honored its 2014 Tourism Partner of the Year, Brat Stop and Hotel Partner of the Year, Country Inn & Suites at its annual Tourism Partner Appreciation Night at Hobnob Restaurant & Cocktail Lounge.
- KACVB and several community organizations were awarded funding from Wisconsin Department of Tourism grant programs totaling \$68,150. Grants supported KACVB projects including operations at the I-94 Visitor Information Center and special events such as Get Behind the Arts Studio Tour, 2016 U.S. Regional Figure Skating and Mid-America World Series.
- Kenosha Area hotel occupancy experienced 12 consecutive months of year over year growth, ending 2014 with an 8.7% increase in occupancy levels over 2013.
- The Kenosha Area CVB's third Discover Wisconsin TV episode – "Unexpected Fun: Only in Kenosha" – debuted in May in television markets across the Midwest. The 30-minute broadcast is part of a three-year marketing package that includes two additional TV airings, as well as interviews on the Discover Wisconsin radio network, and "Choice Destination" status on DiscoverWisconsin.com.
- The KACVB also was featured on a collaborative Discover Wisconsin TV episode entitled "We're Thinking Beaches", which promoted a handful of Wisconsin communities along Lake Michigan. This episode also will re-air twice over the next year.
- The restoration of Kenosha's Historic Simmons Field was completed in 2014 and welcomed the collegiate league team Kenosha Kingfish to the community. KACVB was a proud promoter of this popular new attraction in Kenosha, which attracted 72,838 fans in its inaugural year.
- In 2014, KACVB retained a new agency – Equity Creative – to provide creative services for its 2015 advertising campaign. The new campaign will be launched in spring featuring new print, digital and billboard ad designs, which will be further integrated into KACVB's website, social media and e-marketing products.





Economic Impact

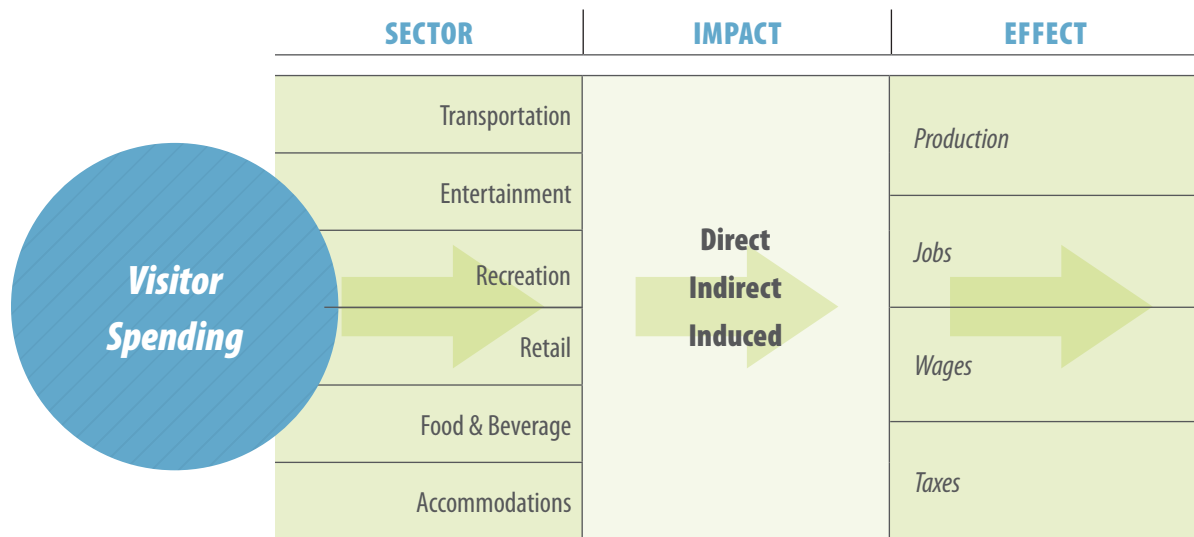


Kenosha County's tourism industry is driven by leisure and business travelers, athletic event participants, conference and meeting attendees, and group/motorcoach travelers. These visitors leave behind tax revenues that contribute significantly to our local and state economy. Revenues generated directly and indirectly by visiting tourists supplement property taxes and support local programs that benefit residents and tourists alike. Tourism dollars also increase local tax revenues and provide employment for thousands of people living in our community.

Each year, the Wisconsin Department of Tourism commissions a comprehensive economic impact study of traveler expenditures throughout the state. As a whole, direct traveler spending in Wisconsin totaled \$11.4 billion in 2014, up 5.3% over 2013. The study – The Economic Impact of Tourism in Wisconsin – by the independent research firm Tourism Economics also provides a county by county breakdown of the impact of tourism on local economies. The figures garnered from this research demonstrate the importance of tourism as a key segment of the Kenosha County economy and underscores the need to continue to support the expenditure of time, effort and funding for tourism promotion and development.

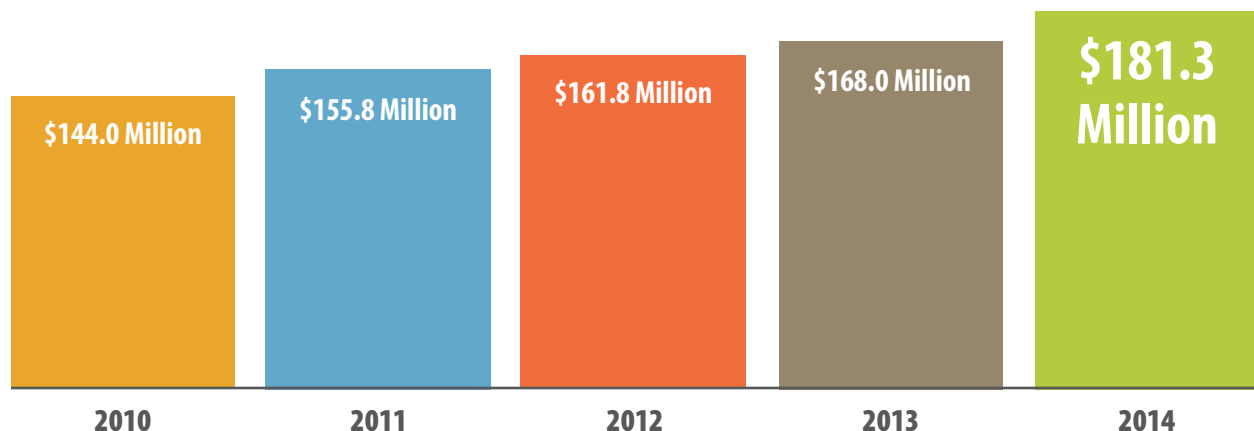
How Visitor Spending Generates Impact

- Travelers create **DIRECT** economic value within a discrete group of sectors (e.g. recreation, transportation). This supports a relative proportion of jobs, wages, taxes, and GDP within each sector.
- Each directly affected sector also purchases goods and services as inputs (e.g. food wholesalers, utilities) into production. These impacts are called **INDIRECT** impacts.
- Lastly, the **INDUCED** impact is generated when employees whose incomes are generated either directly or indirectly by tourism, spend those incomes in the state economy.

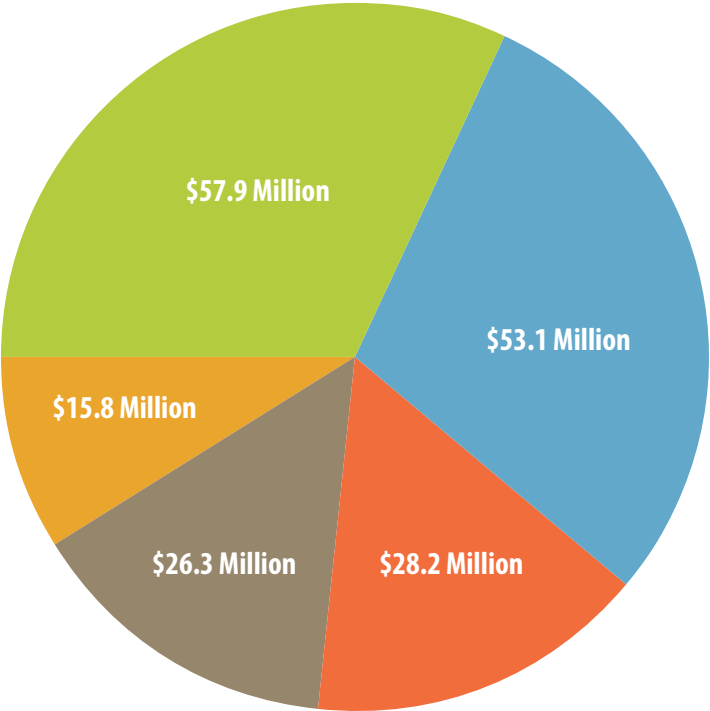


Visitor Spending in Kenosha County

Total direct spending by visitors to Kenosha County in 2014 was \$181.3 million. This is an increase of \$13.3 million or 7.9% over 2013 expenditures. This growth more than doubles the 3.8% increase in 2013. Visitor spending has grown an average of 6% annually since 2010. Kenosha County ranked 19th highest in total traveler expenditures among 72 Wisconsin counties in 2014.



Distribution of Spending



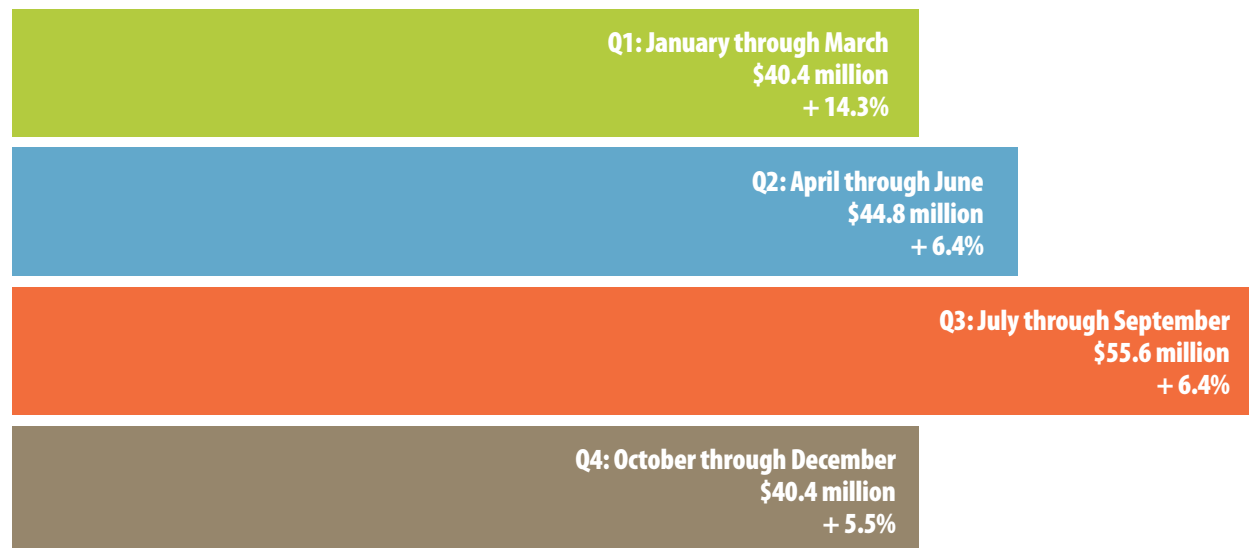
- 👁 **RETAIL** purchases comprised nearly one-third of the visitor dollar in Kenosha County in 2014—a total of \$57.9 million, an increase of \$3.9 million over prior year.
- 👁 **FOOD & BEVERAGE** followed at 29.3% of all spending, or \$53.1 million in 2014. This represents an 7.9% growth over 2014.
- 👁 Spending growth was strongest in the **LODGING** and **RECREATION & ENTERTAINMENT** categories, with both sectors seeing double digit growth.

- **Retail** 31.9%
- **Food & Beverage** 29.3%
- **Recreation & Entertainment** 15.6%
- **Lodging** 14.5%
- **Transportation** 8.7%

SPENDING CATEGORY	2011 (MILLIONS)	2012 (MILLIONS)	2013 (MILLIONS)	2014 (MILLIONS)	PERCENTAGE CHANGE
Lodging	\$22.6	\$23.1	\$23.5	\$26.3	+ \$11.6%
Food & Beverage	\$43.5	\$46.5	\$49.2	\$53.1	+ \$7.9%
Retail	\$53.4	\$53.4	\$54.3	\$57.9	+ 6.5%
Recreation & Entertainment	\$23.6	\$24.2	\$25.5	\$28.2	+ 10.9%
Local Transportation	\$14.3	\$14.6	\$15.4	\$15.8	+ 2.0%
Total	\$157.4	\$161.8	\$168.0	\$181.3	+ 7.9%
Growth Rate	-	+ 2.8%	+ 3.9%	+ 7.9%	-

Visitor Spending by Season

- Examining the seasonality of key tourism sectors, visitor spending peaked in Q3 of 2014 with \$55.6 million in visitor spending, an increase of \$3.4 million or 6.4% over Q3 2013.
- This year's strongest growth was posted in Q1 as visitor spending grew 14.3% to over \$40 million—over \$5 million over 2013 Q1.



Tax Revenue

- Tax revenues collected by state and local governments from traveler spending in Kenosha County amounted to \$20.6 million in 2014.
- In the absence of the state and local taxes generated by local tourism, each Kenosha County household would need to pay \$325 more in taxes to maintain the current level of government services.

	STATE & LOCAL TAX REVENUES MILLIONS				FEDERAL TAX REVENUES (MILLIONS)		
	2012	2013	2014	% CHANGE	2012	2013	2014
Kenosha County	\$18.7	\$19.2	\$20.6	+7.06%	\$12.3	\$13.0	\$14.1
Wisconsin	\$1,300.9	\$1,356.3	\$1,412.3	+ 4.13%	\$9.83.6	\$1,011.0	\$1,052.3
Share of state	1.43%	1.42%	1.46%	-	-	-	-

Employment

- The 2014 Tourism Economics study also shows that the tourism industry in Kenosha County provided 3,006 full-time job equivalents, a 4.3% increase over 2013 that also surpassed a statewide jobs increase of 1.16%.
- Resident income realized from Kenosha County tourism jobs totaled an estimated \$75.2 million, an increase of 9.22% over prior.

EMPLOYMENT					
	2011	2012	2013	2014	% CHANGE
Kenosha County	2,855	2,854	2,882	3,006	+4.20%
Wisconsin	181,320	183,786	185,495	187,643	+1.16%
Share of state	1.57%	1.55%	1.55%	1.60%	-

LABOR INCOME (MILLIONS)					
	2011	2012	2013	2014	% CHANGE
Kenosha County	\$64.2	\$66.0	\$68.8	\$75.2	+9.22%
Wisconsin	\$4,376.0	\$4,512.1	\$4,657.6	\$4,657.6	+3.70%
Share of state	1.47%	1.46%	1.49%	1.56%	-



Hotel Data

The KACVB's operating budget is funded by the Transient Occupancy Tax – or “room tax” – that is collected from travelers purchasing hotel/motel rooms in Kenosha County municipalities with room tax ordinances. These include: the City of Kenosha, Village of Pleasant Prairie, Village of Bristol and Town of Wheatland. Hotel properties within these communities remit the room tax revenues to their respective municipal government, and a portion of those revenues is redirected to the KACVB to support tourism promotion and development. The KACVB's budget is entirely dependent upon hotel/motel occupancy, and is therefore directly related to the results it achieves in marketing the Kenosha Area as an overnight travel destination. The following figures represent collective hotel performance data in the Kenosha Area as reported to the independent research firm, Smith Travel Research.

- Year-to-date hotel occupancy increased 8.7% over 2013, compared to 0.1% decline in 2012.
- All twelve months of 2014 realized an increase in occupancy over the same period prior year. March and February saw significant increases at 25.7% and 17.7% respectively.
- Overall revenue per available room (RevPar) increased 14.4% over 2013 figures, while the average daily rate (ADR) grew 5.3% over prior year.
- Revenue from rooms sold was up 14.3% over 2013, and Demand (sales) were up 8.6% over prior year.





Marketing



The Kenosha Area CVB employs a number of marketing and communications efforts to promote the Kenosha Area as a travel destination, and to maintain its relevance as a destination marketing organization for our community.

The primary objective of our 2014 marketing plan was to build brand awareness that would drive visitation to the Kenosha area by new and repeat customers. To optimize its reach in the various travel marketplaces, the KACVB uses an integrated marketing approach, using a number tools including:

- 👁 Publications
- 👁 Website
- 👁 Print and digital advertising
- 👁 Social media
- 👁 E-marketing

The Official Kenosha Area Visitors Guide

The Kenosha Area Convention & Visitors Bureau is proud to publish the Official Kenosha Area Visitors Guide, an annual magazine-quality publication that serves as the primary resource for visitors to the Kenosha Area. In January 2014, the 29th edition of the Visitors Guide was published, featuring details about area hotels, restaurants, attractions, recreation, retail



shopping and other businesses and community organizations. The Guide also contained maps, special event highlights, creative editorial and photography, as well as details about resources and opportunities found on VisitKenosha.com. The Visitors Guide is a complimentary publication and is designed to guide travelers in planning their visit to the Kenosha Area, as well as serve as a resource once they've arrived here. The Visitors Guide is also commonly used by local residents, event planners, travel writers/bloggers and media, as well as those looking to relocate to the Kenosha Area.

In 2014, 91,785 copies of the Kenosha Area Visitors Guide were distributed to consumers across the United States and Canada.

Guides were mailed to people who requested travel information via phone, e-mail or reader-reply programs from various advertising efforts. Guides also found their way into visitors' hands via a wide network of distribution points across Illinois and Wisconsin, as well

as at 100+ locations within Kenosha County. The Guide also continued to be prominently featured on VisitKenosha.com, where visitors could order a copy by mail or view online in a fully interactive digital format including links to event and advertiser websites.

KENOSHA AREA VISITORS GUIDE DISTRIBUTION POINTS:

- Over 100 Kenosha Area businesses —hotels, restaurants, retailers, attractions, etc.
- Local municipal offices
- Kenosha Electric Streetcars
- 14 Official Wisconsin Travel Information Centers
- 20+ CVB/Chamber Visitor Centers across Wisconsin
- I-94 Hotels in Illinois and Wisconsin
- Corporate break rooms along I-94 in Illinois and Wisconsin
- Sport and travel expos and shows
- Major local events
- Major state-wide events

VISITORS GUIDE REQUESTS BY MONTH													
TYPE	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC	YTD
Email	167	287	234	161	176	254	116	116	126	77	50	36	1802
Reader Reply	396	972	1015	718	604	730	259	25	447	456	319	56	5997
Phone	7	20	34	23	26	14	12	15	14	13	4	4	186
Total	570	1279	1283	902	808	998	387	156	587	546	373	96	7985

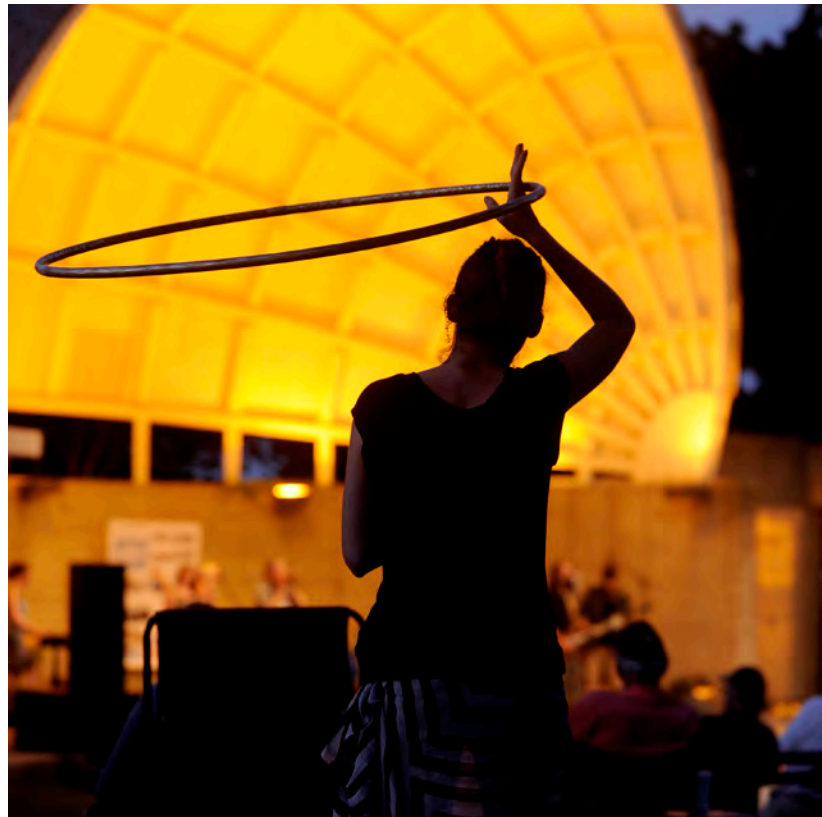
GUIDE REQUESTS BY STATE/PROVINCE

Illinois 2,490	California.....50	Maryland 16	Connecticut.....6
Wisconsin 2,239	Colorado.....44	New Jersey..... 16	Nevada.....6
Michigan 881	Kentucky.....36	Georgia 12	Wyoming6
Minnesota 631	Arkansas35	Louisiana..... 12	Quebec.....5
Iowa 494	Pennsylvania.....29	Massachusetts..... 12	Hawaii.....3
Indiana.....406	Tennessee29	South Carolina..... 11	Montana3
Missouri227	Arizona.....28	Idaho.....9	Washington, D.C.....2
Ohio188	Oklahoma27	Oregon9	Maine.....2
Nebraska.....173	Virginia27	Mississippi.....7	Utah.....2
Florida.....76	South Dakota24	New Mexico7	Deleware.....1
Kansas.....71	North Carolina.....23	Ontario.....7	Manitoba1
North Dakota64	Washington23	West Virginia7	New Hampshire1
Texas54	New York.....21	Alabama6	

- The top 5 states accounted for 91% of the total Guide requests in 2014.
- Illinois and Wisconsin together accounted for over half of all requests in 2014 – 55%.

FUN 101

The popular “FUN 101 - 101 Things to See & Do for \$10 & Under” brochure was printed twice in 2014 with a distribution of 11,500 copies between the spring and fall editions. Copies were placed at the KACVB’s two Visitor Information Centers, in local hotels and mailed with Visitor Guide requests. The FUN 101 list was also one of the most visited pages on VisitKenosha.com, receiving 42,865 page views, a 61% increase over 2013 page views.



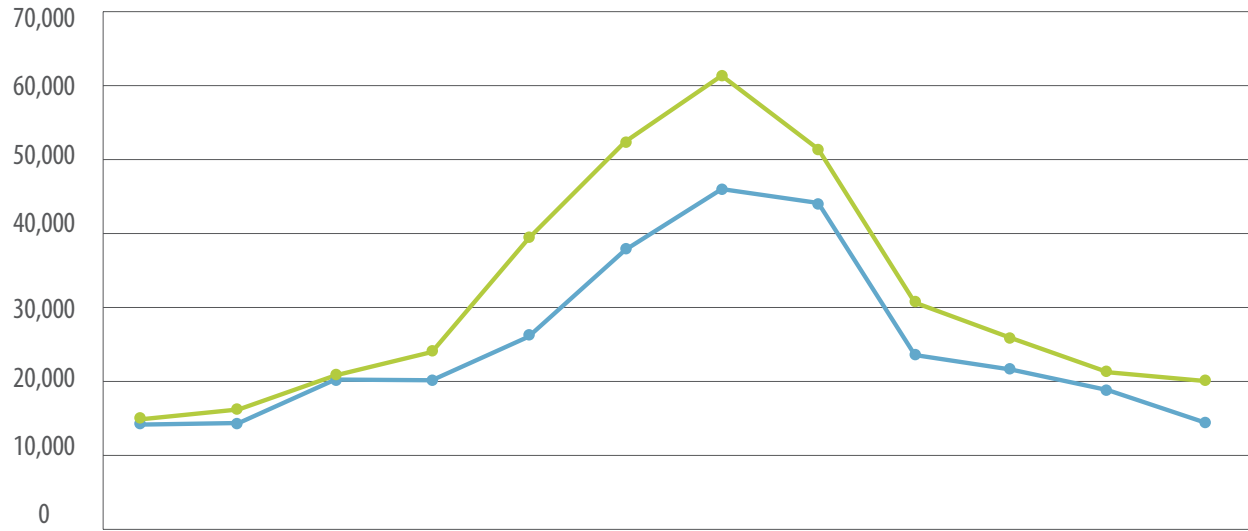
VisitKenosha.com

VisitKenosha.com, the official website of the Kenosha Area CVB, saw 372,028 visitor sessions and delivered over 1.1 million pageviews in 2014. The KACVB continued to position the website as the primary online tool for Kenosha Area travel information by integrating the VisitKenosha.com moniker in its marketing and communications efforts including digital and print advertising, public relations and community outreach, and social media. The KACVB continually strives to keep the website fresh and engaging, and in 2014 completed a full redesign of the site's home page and interior template. Using data gleaned from a site audit and user testing studies, the site was enhanced with a more intuitive navigation and placement of relevant and engaging content.

- VisitKenosha.com achieved a total of 372,028 visitor sessions, a 25.4% increase over 2013.
- Pageviews surpassed the one million mark in 2014 at 1,107,909, a 22.1% increase over 2013 figures.
- 47% of sessions on VisitKenosha.com were via a desktop computer, while visits via mobile devices grew significantly to 40% of the total (up from 29% in 2013). Tablet use accounted for 13%.
- Organic searches by engines such as Google, Yahoo and Bing were the most common traffic source to VisitKenosha.com at 60% of sessions, followed by visits direct to the site's URL (20%), paid search (9%), referrals from other websites (8%), social media (4%) and email (1%).
- VisitKenosha.com also continues to rank among the top search results for "Kenosha" on Google (#4), Bing (#3) and Yahoo (#5).

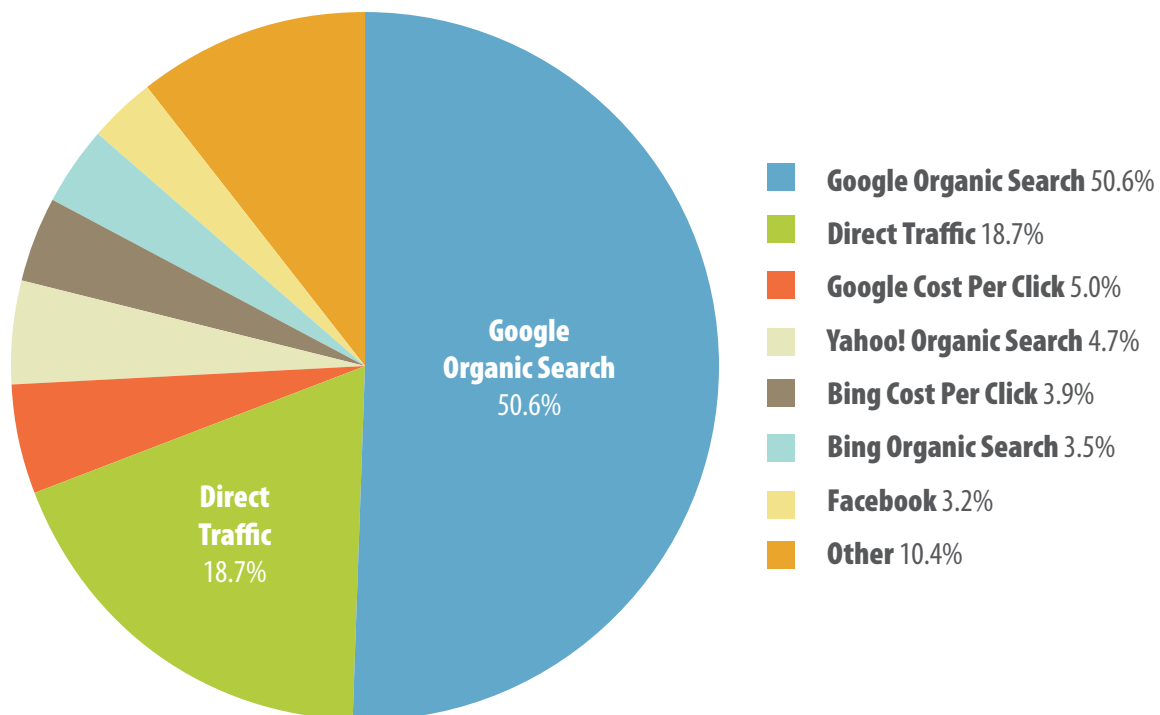


YEAR OVER YEAR SESSIONS



YEAR	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC
2014	14,444	15,763	20,361	23,529	38,871	51,951	60,891	50,954	30,204	25,395	20,786	19,608
2013	13,715	13,925	19,801	19,705	25,594	37,316	45,555	43,676	23,117	21,143	18,364	14,004

TRAFFIC SOURCES

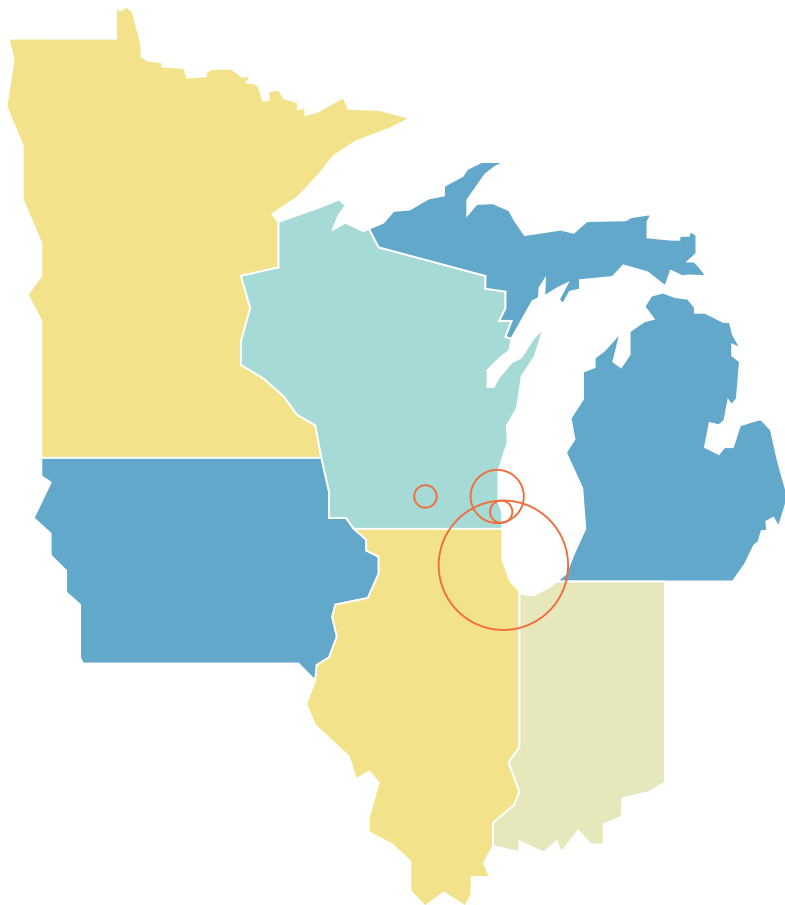
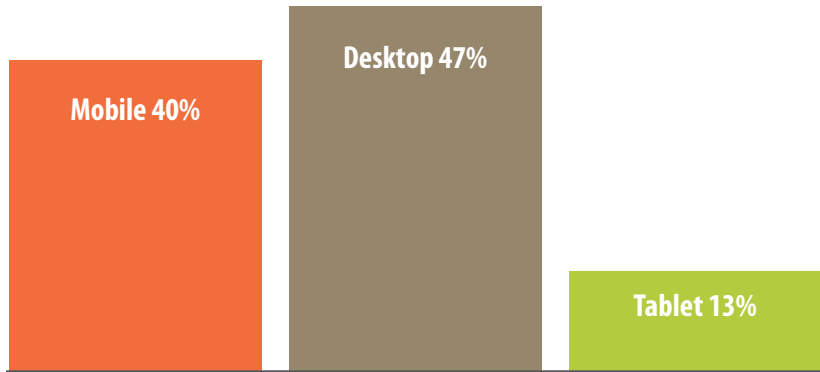




TOP PAGES

- 👁 Home
- 👁 Events
- 👁 Fun 101
- 👁 Play / Top Attractions
- 👁 Shop / Malls
- 👁 Shop
- 👁 Play / Parks & Nature
- 👁 Dine
- 👁 Play
- 👁 Stay

DEVICE TYPE



TOP FEEDER MARKETS

Chicago	53,199 website visits
Milwaukee	21,935
Racine	9,319
Madison	9,196

Print & Online Advertising

The primary objective of the Kenosha Area CVB's advertising campaign was to build brand awareness and drive consumers to the VisitKenosha.com website where further content motivated them to plan a visit to Kenosha County. KACVB's primary target markets included Illinois and Wisconsin, while secondary markets reached Michigan, Indiana and Iowa. Ads were designed with a humorous nod to Kenosha's unique offerings and included colorful images of fun experiences to be had. The 2014 marketing plan employed digital advertising in the form of rich media video ads which delivered 5.6 million impressions. Additional digital strategies included retargeting of online ads and search engine marketing. Traditional magazine and newspaper advertising also continued in 2014, refreshed with new images and messaging and increased size for greater awareness and ROI.

DIGITAL

- Video Ad Media Network
- Google, Yahoo and Bing CPC
- Facebook, Twitter
- TravelWisconsin.com Email Blasts

PRINT

- AAA Living Magazine
- Midwest Living Magazine
- Chicago Parent Magazine
- Experience Wisconsin Magazine
- Chicago Daily Herald
- Milwaukee Journal Sentinel
- The Times of NW Indiana
- Chicago Sun Times
- Chicago Athlete Magazine
- Group Tour Magazine
- Group Travel Leader
- Small Market Meetings
- Midwest Meetings Guidebook
- Lake Michigan Circle Tour Lighthouse Map



Local Advertising

- Downtown Kenosha Magazine
- Happenings Summer Fever
- RecPlex Play by Play
- Kenosha Kingfish Baseball Season Program
- Kenosha Area Chamber of Commerce Business Directory
- Kenosha County Golf Course Signage Program



Television

The Kenosha Area CVB's third Discover Wisconsin TV episode – “Unexpected Fun: Only in Kenosha” – was completed and debuted in May in 19 television markets across the Midwest. The 30-minute broadcast package also included radio interviews on the Discover Wisconsin network of 39 stations, and the full episode video available 24/7 on DiscoverWisconsin.com. Kenosha was also featured on a cooperative Discover Wisconsin episode titled “We’re Thinking Beaches”, highlighting seven Lake Michigan harbor towns.



Social Media

Social media continues to be one of the KACVB's most essential marketing and communications tools. Targeting new audiences and extending the reach of traditional print and online advertising, social media allows us to engage in authentic conversations with visitors and local residents in real-time, and drive significant traffic to VisitKenosha.com. In 2014, KACVB was active on five social media platforms including Facebook, Twitter, Instagram, YouTube and Pinterest.

2014 HIGHLIGHTS

- Over 23,000 clicks to VisitKenosha.com were generated via social media channels.
- Surpassed 10,000 likes and 2.3 million impressions on the Visit Kenosha Facebook page.
- Nearly 12,000 minutes (200 hours) of Kenosha Area videos were watched on the Visit Kenosha YouTube Channel in 2014.
- Over 150 experiential photos were shared on the Visit Kenosha Instagram account highlighting #KenoshaFun.
- Visit Kenosha Pinterest boards highlighting media coverage, blog posts, and Fun 101 items continued to be popular amongst pinners.
- Twitter continues to be a platform that allows the KACVB to interact with travelers and to provide digital customer service. The Visit Kenosha Twitter account had 2,543 followers at the end of 2014.
- Over 60 blog posts were published sharing information about events, exhibits, attractions, restaurants and more.



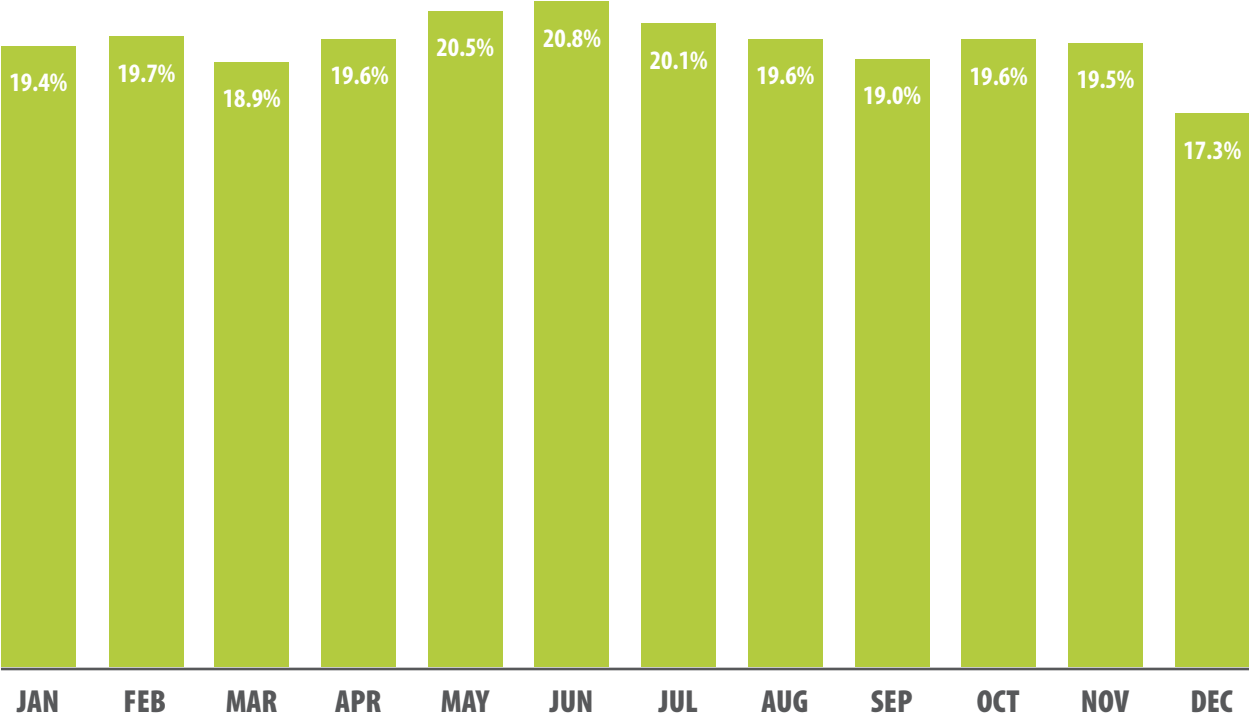
I ♥ KENOSHA

The KACVB launched the “I ♥ Kenosha” photo frame campaign during National Travel & Tourism Week in 2014. The free frames were distributed from select Tourism Partner locations, as well as both KACVB Visitor Information Centers. The frames encouraged visitors and locals alike to take photos of their fun Kenosha Area experiences and share them on social media using the KACVB's #KenoshaFun hashtag. The campaign was also instrumental in establishing KACVB's new hashtag, with people using it more regularly across multiple platforms since the campaign launch.

E-Mail Marketing

In 2014, the Kenosha Area CVB continued its focus on digital marketing by publishing a monthly email newsletter, as well as participating in the Wisconsin Department of Tourism’s monthly e-blast. Subscribers to KACVB’s newsletter received monthly messages that offered information about unique attractions, special events, offers, and new developments in the Kenosha Area. The KACVB grew its database by 2,365 subscribers in 2014, finishing the year with 10,577 subscribers. Analytics show that the KACVB’s e-newsletter monthly open rate averaged 19.5% and its average click-through rate was 20.8% in 2014. In addition to KACVB’s own e-newsletter, participation in the TravelWisconsin.com e-blast program promoted the KACVB’s Getaway Giveaway contest on VisitKenosha.com and reached over 45,000 households each month.

2014 NEWSLETTER OPEN RATES





Specialty Markets

Sports

The KACVB's Sports Development team continued its efforts to draw visitors to the Kenosha Area who were traveling with teams or as individuals to participate in sporting events, tournaments or other events of a competitive nature. This market drew not only competitors and athletes, but also staff, family members and spectators, which heightened the economic impact for the community.

The KACVB and Kenosha Area sports facilities received a number of grants through the Wisconsin Department of Tourism's Ready, Set, Go and Meetings Mean Business grant programs. Thanks to such grants, the Pleasant Prairie RecPlex Ice Arena was able to host the 2014 ISI Figure Skating Championships and Kenosha hosted the 2014 Mid America Baseball World Series at Carthage College and Nash Park. During 2014, the Kenosha Area hosted more than 100 events that brought an estimated 35,000+ athletes to our local hotels and an additional 70,000 to 140,000 spectators.

Building on these successes, the KACVB continued its rigorous efforts to expand this market segment and submitted grant applications totaling more than \$30,650 for upcoming 2015 sports events. The KACVB also continued to position itself within the sports market by maintaining memberships in national sports affiliations and by attending state, regional and national conferences throughout the year. The Kenosha Area Sports Planner publication was a key marketing tool to provide prospective event planners with an in-depth overview of Kenosha's many sporting event venues including key facilities such as RecPlex, UW-Parkside and Carthage College.

Group Tour

The Group Tour market is comprised of groups that travel together seeking a destination to spend leisure time. The KACVB actively marketed the Kenosha Area through its membership in group tour industry associations and collaboration with fellow Visitors Bureaus seeking to generate interest in southeast Wisconsin. The Kenosha Area Group Tour Profile publication was sent to 513 qualified group tour operator leads in 2014. Of them we saw a large number come to Kenosha and spend a day or overnight. Special 2-day itineraries were also developed, including Destination: Shopping, Feeling Museumical, and A Taste of History. The KACVB also continued its involvement in various industry associations including Circle Wisconsin, VisitMilwaukee, National Tour Association (NTA) and American Bus Association (ABA).

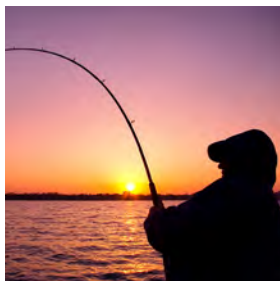
Meetings and Conventions

The Meetings and Conventions Market is comprised of businesses, associations, fraternal organizations, or government agencies which hold meetings, conferences, tradeshow or other events that utilize meeting space and often overnight lodging. The KACVB continued to provide assistance and guidance to meeting planners looking to host small to mid-sized functions in the Kenosha Area in 2014. Resources included site inspections and recommendations, welcome packets, and hospitality services for meeting delegates. The Kenosha Area Meeting Planner publication and complementary Plan a Meeting pages on VisitKenosha.com provided meeting planners with detailed information about the area's facilities and KACVB's complimentary services. The Kenosha Area was host to a number of meetings and conferences in 2014 including the Wisconsin Federation of Museums and Wisconsin Genealogy Association, and distributed more than 5,000 Visitors Guides and Welcome bags to meeting groups.





Public Relations



The Kenosha Area CVB reaches out to travel writers, bloggers, newspapers, magazines, radio stations, and television stations throughout the Midwest utilizing news releases, social media, an online Media Room, personal contact, and customized press tours. In turn, our community is exposed to both new and existing audiences that we hope to inspire to travel here.

2014 PR Highlights

- Total earned media value for the Kenosha Area reached an estimated \$201,000 in 2014. Sixty-one media placements valued at \$115,660 were achieved in markets outside of Kenosha, with the remaining articles, interviews and other media mentions appearing within the local Kenosha market.
- Media placements appeared in nearly 40 different newspapers and magazines, TV and radio stations, and online blogs throughout the U.S., including states as far away as Washington and South Carolina.
- KACVB hosted eight travel writers and bloggers in 2014, who promoted the Kenosha Area in over a dozen newspaper columns, magazine articles and blogs reaching audiences across the U.S.
- The Fox 6 TV show Real Milwaukee responded to the KACVB's efforts to promote local business Forever Grateful's unique claim to have the "Universe's largest ugly Christmas sweater department". Host Chip Brewster came to Kenosha to feature the shop and its sweater collection on the show.
- KACVB-produced columns appeared in the Kenosha News each week and in Action Magazine monthly in 2014. KACVB staff was also featured on weekly live interviews on WLIP and WRJN Radio, while Kenosha's local television station Channel 14 featured KACVB during an episode of Just Talkin'.
- The KACVB exhibited at the 2014 Recreation & Leisure Travel Expo at Naval Station Great Lakes, as well as the 2014 Wisconsin State Fair in a partnership with Discover Wisconsin TV.



Community Outreach

TOURISM WEEK

National Travel & Tourism Week was recognized to bring awareness to Kenosha Area residents of the positive economic impact that tourism has on our local economy. A four-page, color insert was distributed in the Kenosha News, reaching over 24,000 households. The insert, along with a dedicated Tourism Week page on VisitKenosha.com, media outreach and a photo campaign featuring Tourism Partners, provided locals with insight into how tourism positively impacts their community.



THIRD GRADE PRESENTATIONS

In its 18th year of conducting entertaining and educational presentations to local third grade classes, KACVB visited nearly 900 students at 17 area schools in 2014. Students were encouraged to explore Kenosha with their families – to be tourists in their own town – and participate in KACVB’s Essay Contest. Students were asked to write about and draw their favorite places in Kenosha County and five winners were selected to receive prize packages donated by Tourism Partner businesses.

THE BIG BUNDLE UP

The KACVB’s two Visitor Information Centers once again served as collection sites for the Wisconsin Department of Tourism’s annual “Big Bundle Up” campaign. At the conclusion of the Winter 2013-2014 campaign, the KACVB collected and distributed nearly 1,200 warm clothing items through First Step Services Inc. in Kenosha.

PHOTO CONTEST

More than 80 people submitted 360 photos to the KACVB’s annual Picture Perfect Photo Contest in 2014. The contest encourages residents to submit their favorite photos from around the Kenosha Area, which the KACVB in turn may use in its marketing efforts. Three winners were awarded a dining gift certificate, courtesy of KACVB restaurant partners.





Tourism Partner Relations



Tourism Partners are essential to the success of tourism in Kenosha County.

The Kenosha Area CVB continued its effort to grow and enhance the benefits offered to Partners in 2014. As local businesses invest in an advertisement in the Official Kenosha Are Visitors Guide, the KACVB delivers an array of complimentary benefits, services and resources that help to further promote their business to visitors.

First Fridays

The KACVB hosted multiple Tourism Partner businesses at its monthly First Friday event at the I-94 Visitor Information Center. The events drew 6-8 Partner businesses each month, whose exhibits engaged visitors with information, samples and fun activities. First Fridays continues to be a popular benefit among KACVB Tourism Partners.

Training & Consulting Services

The KACVB's hospitality training program, Kenosha Area To Go, served nearly 100 front line employees in 2014. The free workshop was customized to individual Tourism Partner businesses and held at their respective locations. Employees were provided training on other Tourism Partner businesses, customer service skills, and frequently asked questions about the Kenosha Area.

Social Media training also continued to be a valuable service to Tourism Partners in 2014. KACVB consulted with business owners and organizations to help them navigate the ever-changing social media landscape, an important part of making the Kenosha Area socially and digitally attractive to travelers.

Partner Meetings

The KACVB's quarterly breakfast meeting series, Tourism Talks, was hosted at local Partner business locations and continued to offer educational programming and networking opportunities to Tourism Partners in 2014. The KACVB also established regular Hotel Partner meetings to address issues specific to Kenosha's lodging industry and share best practices to ensure positive visitor experiences.





Industry Partnerships

Partnerships within the tourism industry and the community are vital to the success of the Kenosha Area CVB and its mission to positively impact the local economy. We thank the following organizations for their continued and loyal support of tourism in Kenosha County in 2014:

MUNICIPALITIES

City of Kenosha
 Village of Pleasant Prairie
 Village of Bristol
 Town of Wheatland

LODGING

Best Western Executive Inn
 Best Western Harborside Inn
 and Kenosha Conference
 Center
 Candlewood Suites
 Comfort Suites

Country Inn & Suites
 Holiday Inn Express Hotel
 & Suites
 LaQuinta Inn
 Lily Lake Resort
 Merry Yacht Inn
 Park Ridge Inn
 Radisson Hotel
 & Conference Center
 Southport Bed & Breakfast
 Super 8 Motel
 Value Inn

RESTAURANTS, CAFES, COFFEE SHOPS & BREWERIES

Big Star Drive-In
 Boat House Pub & Eatery
 Brat Stop
 Breakwater Bar & Grill
 Bristol 45 Diner
 Captain Mike's Beer
 & Burger Bar
 Casa Capri
 Char's Café
 Culver's – Pleasant Prairie
 Franks Diner

Harborside Common
 Grounds
 Hobnob Restaurant
 & Cocktail Lounge
 House of Gerhard
 Kaiser's Pizza & Pub
 Linnea Bakery
 Mangia Wine Bar
 Mikey's
 Pazzo
 PUBLIC Craft Brewing Co.
 Ray Radigan's
 Rustic Road Brewing
 Company

Sazzy B
Sol D’Licious Café
Soon’s Sushi Café
tg’s Restaurant & Pub
The Coffee Pot
The Good Garden Café
The Spot Drive-In
Trolley Dogs
Twisted Cuisine
Uncle Mike’s Highway Pub
Wine Knot Bar & Bistro

ATTRACTIONS, ARTS & RECREATION

Actor’s Craft
Alpaca Art Pottery Painting
Anderson Arts Center
Brighton Dale Links
Golf Course
Bristol Renaissance Faire
City of Kenosha Parks
Civil War Museum
Dinosaur Discovery Museum
Electric Streetcar Circulator
Jelly Belly Visitor Center
Kemper Center
Kenosha Art Association
Kenosha Charter Boat
Association
Kenosha Community Sailing
Center at Kenosha Yacht
Club
Kenosha County Parks
Kenosha HarborMarket
Kenosha History Center
Kenosha Kingfish Baseball
Kenosha Municipal
Golf Course

Kenosha Pops
Kenosha Public Libraries
Kenosha Public Museum
Kenosha Streetcar Society
Kenosha Union Park Project
Lakeside Players
Lemon Street Gallery &
ArtSpace
National Register Historic
Districts
Petrifying Springs Park &
Golf Course
Pierhead Lighthouse
Pike Bike Trail
Pollard Gallery
Pringle Nature Center
RecPlex
Rhode Center for the Arts
Richard Bong State
Recreation Area
Southport Light Station
Museum
Washington Park Velodrome
Wilmot Mountain Ski &
Snowboard Area
Wilmot Raceway at Kenosha
County Fair

RETAIL & SERVICES

A Summer’s Garden Florist
Andrea’s & Jack’s Café
Anytime Fitness
Cypress Tree
DeBerge’s Framing
& Gallery
Equinox: The Body
& Soul Boutique
Forever Grateful Resale
Boutique & Gallery

Gottfredsen & Nicoll
Jewelers
Harborside Bait & Tackle
Heim’s Downtown Toy Store
Indian Trail Plaza
Inner Child Comics
& Collectibles
Jockey Factory Store
K9 Kibble
Kenosha YMCA
LaMacchia Travel
Lou Perrine’s Gas & Grocery
Mike Bjorn’s Fine Clothing
NovelTea Bookstore
Pleasant Prairie Premium
Outlets
Red Rose’s Bead Haven
Robin’s Nest Cakery
Sandy’s Popper
Scoops Ice Cream & Candy
Seebeck Gallery
Sixth Avenue Boutique
Something Different
Southport Plaza
Southport Rigging Bikes
& Boards
Tenuta’s Delicatessen &
Liquors
Uke’s Harley-Davidson/BRP
Utopia Nails & Spa

BUSINESSES & ORGANIZATIONS

Action Magazine
Ascedia
ATC Leasing Co.
Badger Press
& Photographics
Bane-Nelson, Inc.
Bank of Elmwood
Bank of Kenosha
Bryce Ulmer Design
Carthage College
Chase Bank
CliftonLarsonAllen
Discover MediaWorks
Discover Wisconsin TV
Downtown Kenosha, Inc.
Downtown Kenosha
Magazine
Equity Creative
Guttormsen, Hartley,
Wilk & Higgins, LLP
Happenings Magazine
Hoot Communications
Insty Prints
Jalensky Real Estate
Services, LLC
Johnson Bank
Kenosha Area Business
Alliance
Kenosha Area Chamber
of Commerce
Kenosha County
Kenosha Lakeshore BID
Kenosha News
Kenosha Noon
Optimist Club
Kenosha Unified
School District

Kiwanis Club – Western Kenosha
 Kiwanis Club – Downtown Kenosha
 Northwestern Mutual Insurance
 Platypus Advertising + Design
 Regner Veterinary Clinic
 Shagbark Apartments
 Southport Bank
 Thomas VanBeckum Law Office
 United Food & Commercial Workers
 University of Wisconsin-Parkside
 Valeri Insurance
 VBL & Associates
 W.D. Hoard & Sons
 WLIP 1050AM
 WRJN 1400AM
 Wisconsin Department of Natural Resources
 Wisconsin Department of Tourism
 Wisconsin Department of Transportation

2014 BOARD & COMMITTEE SERVICE

- Governor’s Council on Tourism JEM Committee
- Governor’s Council on Tourism TIC Grant Committee
- Governor’s Council on Tourism Sports Committee
- Sports Wisconsin
- Wisconsin Harbor Towns Association
- Kenosha History Center Board of Directors
- Kenosha’s Lakeshore Business Improvement District (BID) Board of Directors
- Downtown Kenosha Inc. Board of Directors
- Kenosha Boys & Girls Club Board of Directors
- Kenosha Noon Optimist Club Board of Directors
- Pleasant Prairie Patriots Swim Team Board of Directors
- Youth As Resources Endowment Committee
- Wisconsin Marathon
- Pleasant Prairie Triathlons
- Grill Games

2014 INDUSTRY AFFILIATIONS

- Destination Marketing Association International (DMAI)
- Wisconsin Department of Tourism
- Wisconsin Association of Convention & Visitors Bureaus (WACVB)
- Wisconsin Hotel & Lodging Association
- Wisconsin Harbor Towns Association
- Association of Wisconsin Tourism Attractions
- National Association of Sports Commissions
- Travel, Event and Management in Sports
- Sports Wisconsin
- Circle Wisconsin
- Kenosha Area Business Alliance
- Kenosha Area Chamber of Commerce
- Downtown Kenosha, Inc.
- Kenosha Lakeshore Business Improvement District (BID)

Consider **Resolution #15-21** for a **Floodplain Boundary Adjustment** for the request of Matt Carey with Pinnacle Engineering Group, agent for Route 165, LLC owner of the property located at 12575 Uline Drive to remove 2,226 cubic yards from the 100-year floodplain and to create 3,795 cubic yards of 100-year floodplain to compensate for the 100-floodplain begin filled for the purpose of constructing a conference center between the Uline Corporate Office building and the easternmost warehouse building on the property.

Recommendation: On June 8, 2015 the Village Board held a public hearing and recommended that the Village Board approve the Floodplain Boundary Adjustment pursuant to Resolution #15-21 as presented at the June 15, 2015 Village Board meeting.

VILLAGE STAFF REPORT OF JUNE 15, 2015

Consider **Resolution #15-21** for a **Floodplain Boundary Adjustment** for the request of Matt Carey with Pinnacle Engineering Group, agent for Route 165, LLC owner of the property located at 12575 Uline Drive to remove 2,226 cubic yards from the 100-year floodplain and to create 3,795 cubic yards of 100-year floodplain to compensate for the 100-floodplain begin filled for the purpose of constructing a conference center between the Uline Corporate Office building and the easternmost warehouse building on the property.

The petitioner is requesting approval of a floodplain boundary adjustment to remove 2,226 cubic yards from the 100-year floodplain and to create 3,795 cubic yards of 100-year floodplain to compensate for the 100-floodplain begin filled for the purpose of constructing a conference center to be located between the Uline Corporate Office building and the easternmost warehouse building on the property. In addition, the petitioner is requesting Preliminary Site and Operational Plans to begin mass grading for the proposed Conference Center Building after obtaining all of the required permits and approvals.

The petitioner is requesting approval to place fill within the current 100-year floodplain limits along the northwest corner of the retention facility and to create additional 100-year floodplain storage in the southern portion of the retention facility. The Floodplain will be impacted within the interior of the reetention pond. Proposed mitigation is proposed at the south end of the pond to provide additional storage to maintain current floodplain elevations. Floodplain will be contained on the Uline property and will not impact any adjacent properties.

According to the Village's Floodplain Ordinance the Village shall not permit amendments to the floodplain boundary that are inconsistent with the purposes of Section 420-131 of the Village Zoning Ordinance, or in conflict with the applicable rules of the Wisconsin Department of Natural Resources (WI DNR) or the Federal Emergency Management Agency (FEMA). Section 420-131 of the Village Zoning Ordinance indicates that amendments to the 100-year floodplain shall comply with the following Village requirements:

- The floodplain boundary adjustment shall be consistent with Section 420-131 of the Village Zoning Ordinance or in conflict with the applicable rules of the WI DNR and FEMA.
- Amendments to the floodplain boundaries shall not be permitted where the change will increase the regional flood stage elevation unless the applicant has made appropriate legal arrangements with the Village, any other affected governmental units, and any other property owners affected by the flood stage increase.
- Petitions for the floodplain amendment shall provide adjusted water surface profiles and adjusted floodplain limits to reflect the increased flood elevation.
- Any area removed from the floodplain shall be contiguous to land lying outside the floodplain.
- Whenever any volume of flood storage capacity is removed from the floodplain, as defined by the ground surface and the regional flood elevation, an equal volume of flood storage capacity shall be created within the existing or newly created floodplain boundary, in the vicinity of the removal, to compensate for the lost flood storage capacity. Excavation below the ordinary high-water mark shall not be considered as providing any equal volume of storage capacity for compensation purposes. Any such area of compensating flood storage capacity shall drain freely to the receiving stream.

- Removal of land from the floodplain shall not be permitted unless the land has been filled to an elevation at least two (2) feet above the elevation of the floodplain.

The petitioner intends to obtain Village and WDNR approvals to fill the floodplain as depicted on the application on the basis that the grading will satisfy volumetric compensatory mitigation requirements for a Conditional Letter of Map Revision based on fill (CLOMR-F) and ultimately a Letter of Map Revision based on fill (LOMR-F) after construction is completed.

The site is intended to be used for the construction of a 2-level 22,500 square foot Uline conference center, to be generally located on the north side banks of an existing retention pond, which is located on the southwest side (approximately 300' away) of Uline's corporate office headquarters.

This new facility will be for private use and will only be used on average of once per week for corporate meetings, conferences, and events with approximately 200 to 300 attendees. Events at the conference center will be held in the morning, afternoon, and evenings, weekdays and weekends. The building includes a large gathering area on main level overlooking the existing pond. There is also an outdoor deck at the south end overlooking the pond. No new employees will be hired to run the conference center and existing corporate staff will be utilized for coordinating events. Catering companies may be used as well. Parking requirements for events will be met by the existing 800-stall parking lot adjacent to the Uline office building. No new parking stalls are proposed. Detailed Final Site and Operational Plans will be submitted for Village staff and Plan Commission review and approval in the next few months.

Plan Commission recommended approval of the 100-year floodplain pursuant to the conditions of the attached Village Board Resolution. In addition, the following changes are required to be made to the Preliminary Site and Operational Plans for prior to the issuance of permits and mass grading of the site:

1. The plans have been reviewed for conformance with the Village Ordinances and generally accepted engineering practices and Village policies. Although the data has been reviewed, the design engineer is responsible for the thoroughness and accuracy of plans and supplemental data and for their compliance with all State and local codes, ordinances, and procedures. Modifications to the plans, etc. may be required should errors or changed conditions be found at a future date. **The following changes shall be made to the Preliminary Site and Operational Plans. Three (3) revised plans, a pdf copy of the plans along with notations explaining how each of the items below have been addressed shall be submitted for final staff review and approval prior to issuance of the erosion control permit for mass grading or an early footings and foundations permit:**
 - a. The following changes shall be made to the Grading, Paving & Erosion Control Plan (Sheet C-3):
 - i. Show removal limits of the existing asphalt walk.
 - ii. Add a note stating that all excess excavated material is to be placed south of the compensatory storage area as stated on the responses to the previous Village of Pleasant Prairie review. The note should include requirements that the slopes not exceed 4:1 and existing drainage patterns are maintained. Verify this area has adequate capacity to waste the excess material.
 - b. The following changes shall be made to the *Utility Plan (Sheet C-5)*:
 - i. Why does the new water service not connect to the existing 12" water main located only 56' feet north of the Utility Building?

- ii. The note on Sheet C-6 specifies a wet tap water main connecting utilizing a tapping sleeve. A wet tap requires the installation of a valve at the tap location. Please clarify if this is to be a wet tap or if the connection will be made by shutting off the water main. Regardless of the means of connection to the existing main we recommend installing a valve at the connection point.
 - iii. A water meter will be required at both the Utility Building and the Conference Center.
 - iv. Specify a proposed water main size.
 - v. The note to install an air release assembly at any highpoints created by avoiding the existing utilities should be a general note located on the sheet as the note applies to all utility crossings, not just the duct bank.
 - vi. The pumper pad for the building is to be located within a concrete pavement area. Add a detail for the pumper pad construction.
 - vii. Revise invert elevations for sanitary manhole 2.0 to include 10" W.
 - viii. Will the design of the proposed chilled and hot water pipes be included on the plumbing plans.
- c. The following changes shall be made to the *Construction Details (Sheet C-5)*:
- i. The Village will be sending specifications to be used for sanitary sewer and water main construction.
 - ii. Add a detail for the pumper pad.
- d. The fire protection designer shall verify that adequate fire flow requirements are met for the building.
- e. Approval from the Fire & Rescue Department shall be obtained for fire hydrant and pumper pad locations and fire suppression system design.
2. Upon approval of the revised Preliminary Site and Operational Plans the following shall be submitted for review and issuance of the erosion control permit for mass grading including the floodplain boundary adjustment:
- a. Three (3) copies of the revised plans and an electronic pdf of the Preliminary Site and Operational Plans.
 - b. Conditional Letter of Map Amendment from FEMA for the proposed Floodplain Boundary Adjustment.
 - c. An Erosion Control Permit Application and plans with a copy of the Wisconsin Department of Natural Resources N.O.I. [Note: The required \$2,000 street sweeping cash deposit shall be deposited with the Village. The street sweeping cash deposit is refundable, less 6% for administrative processing upon issuance of a Certificate of Compliance for the entire project, if the amount is not drawn upon by the Village in maintaining the adjacent roadways free from dirt, mud clumps and mud tracking during the construction process. Silt fence shall be installed and inspected prior to any work starting.]
 - d. Written approval of the Preliminary Site and Operational Plans from Prairiewood Commercial Association.

3. **Prior to mass grading work and the floodplain boundary adjustment commencing on the site, all required permits shall be issued by the Village, all required erosion control measures are in place on the site and a pre-construction conference shall be held at the Village Offices. The preconstruction conference shall be scheduled and moderated by the designing Engineer of record.**
4. For security reasons, the Village recommends surrounding the construction site with a six (6) foot high temporary chain link fence. A fence permit is required for the temporary fencing.
5. In addition to providing all of the detailed Final Site and Operational Plans, documents and sample materials, an amendment to the Uline PUD (Zoning Text Amendment) will be required to be submitted. The Zoning Text Amendment and the Final Site and Operational Plan application shall be considered by the Plan Commission at the same time.
6. A Comprehensive Land Use Plan Amendment will need to be filed with the Village.
7. **General Comments**
 - a. See **attached** comments from the Building Inspection Department dated May 5, 2015.
 - b. See **attached** comments from the Fire & Rescue Department dated May 7, 2015. These comments will be updated when detailed Final Site and Operational Plans are submitted and reviewed by the Department.
 - c. Impact fees shall be paid prior to issuance of the building permit. (Based upon \$1.94 per \$1,000 of new valuation as determined by the Village Assessing Department). Further discussion is warranted.
 - d. The hours of construction activity, operating heavy machinery or equipment associated with the grading, erosion control device installation, and overall site development shall be limited to Monday through Friday from 7:00 a.m. to 10:00 p.m. and Saturday and Sunday from 7:00 a.m. to 10:00 p.m. unless approved in writing by the Village for extended hours.
 - e. On-site (off-street) parking shall be designed to accommodate all construction related workers and site visitors.
 - f. The Village shall approve of the location of all construction trailers parked on the site during construction activities. No construction trailers shall be parked in Village rights-of-way. All construction related signage shall be approved and permitted by the Village.
 - g. As-built graphical data of all private sewer, water, and storm sewer facilities shall be provided to update the Village's Geographical Information System. Information shall conform to the Village's format requirements.
 - h. A record drawing of the utility plans will be required to reflect actual construction records and utility alignments. Record drawing(s) of all water main, sanitary sewer(s), and storm sewer(s) shall be prepared by the Engineer of Record for the project.

**VILLAGE BOARD RESOLUTION #15-21
VILLAGE OF PLEASANT PRAIRIE
RESOLUTION AND COMMUNITY CONCURRENCE
TO AMEND THE 100-YEAR FLOODPLAIN BOUNDARY**

WHEREAS, Matt Carey with Pinnacle Engineering Group, agent for Route 165, LLC owner of the property located at 12575 Uline Drive has requested approval of a Floodplain Boundary Adjustments for the proposed Uline Conference Center at 12575 Uline Drive; and

WHEREAS, the Floodplain Boundary Adjustment proposes to remove 2,226 cubic yards from the 100-year floodplain and to create 3,795 cubic yards of 100-year floodplain to compensate for the 100-floodplain begin filled as shown on Flood Insurance Rate Map (FIRM) and Flood Insurance Study (FIS) for the Village of Pleasant Prairie (Effective Date of June 19, 2012) for a part of U.S. Public Land Survey Section 25, Township 1 North, Range 21 East, Village of Pleasant Prairie, Kenosha County, Wisconsin and further identified as Tax Parcel Number 91-4-121-252-0203 as shown on **Exhibit 1**; and

WHEREAS, the petitioner is requesting approval to place fill within the current 100-year floodplain limits along the northwest corner of the retention facility and to create additional 100-year floodplain storage in the southern portion of the retention facility. The Floodplain will be impacted within the interior of the retention pond. Proposed mitigation is proposed at the south end of the pond to provide additional storage to maintain current floodplain elevations. Floodplain will be contained on the subject property and will not impact any adjacent properties; and

WHEREAS, the floodplain boundary adjustment shall be consistent with Section 420-131 of the Village Zoning Ordinance or in conflict with the applicable rules of the WI DNR and FEMA; and

WHEREAS, amendments to the floodplain boundaries shall not be permitted where the change will increase the regional flood stage elevation unless the applicant has made appropriate legal arrangements with the Village, any other affected governmental units, and any other property owners affected by the flood stage increase; and

WHEREAS, the floodplain amendment shall provide adjusted water surface profiles and adjusted floodplain limits to reflect the increased flood elevation; and

WHEREAS, any area removed from the floodplain shall be contiguous to land lying outside the floodplain; and

WHEREAS, whenever any volume of flood storage capacity is removed from the floodplain, as defined by the ground surface and the regional flood elevation, an equal volume of flood storage capacity shall be created within the existing or newly created floodplain boundary, in the vicinity of the removal, to compensate for the lost flood storage capacity. Excavation below the ordinary high-water mark shall not be considered as providing any equal volume of storage capacity for compensation purposes. Any such area of compensating flood storage capacity shall drain freely to the receiving stream; and

WHEREAS, removal of land from the floodplain shall not be permitted unless the land has been filled to an elevation at least two (2) feet above the elevation of the floodplain; and

WHEREAS, a public hearing was held by the Village Plan Commission of the Village of Pleasant Prairie, Kenosha County, Wisconsin, at the Village Municipal Building, 9915 39th Avenue on the 8th day of June 2015, at 6:00 P.M. of said day, for the purpose of determining the application of the floodplain boundary map amendment; and

WHEREAS, the Pleasant Prairie Village Board has been provided with sufficient

evidence that the petition for a floodplain boundary map adjustment and amendment is consistent with the requirements of the Village Zoning Ordinance.

NOW, THEREFORE, the Village Board does hereby resolve to amend the 100-year recurrence interval floodplain boundary as delineated and shown on the Flood Insurance Rate Map (FIRM) and Flood Insurance Study (FIS) for the Village of Pleasant Prairie (Effective Date of June 19, 2012) for a part of U.S. Public Land Survey Section 25, Township 1 North, Range 21 East, Village of Pleasant Prairie, Kenosha County, Wisconsin, subject to compliance with the terms and conditions hereinafter stated:

1. Upon receiving Village Board approval, the petitioner is responsible for transmitting all required applications to FEMA with a request for a CLOMR-F.
2. The petitioner shall receive a CLOMR-F prior to work commencing on the site. Upon receipt of the CLOMR-F and any required permits from the WI DNR, copies of said approvals/permits shall be submitted to the Village with the required Erosion Control Permit Application and related plans and specification (paper copies and a pdf copy) for review and issuance of the required Village Permits.
3. All required permits from the WI DNR shall be obtained and provided to the Village prior to the Village issuance of permits to commence work on site.
4. Upon completion of the floodplain boundary adjustment an as-built survey and calculations shall be submitted to verify the compliance with design plans. The as-built survey and calculations shall be reviewed by the Village and the WI DNR prior to being submitted to FEMA for review. Upon review of the documents by the Village, the petitioner shall submit and receive a final LOMR-F from FEMA.
5. Upon completion of the work, an as-built grading plan and supporting documentation certified and stamped by a Wisconsin registered and licensed professional engineer shall be submitted to the Village to verify compliance with design plans. The as-built grading plan and calculations shall be reviewed by the Village and the WI DNR prior to being submitted to FEMA for review and obtaining the required LOMR-F. A paper and pdf copy is required to be submitted.
6. Once a LOMR-F is issued and the final as-built grading plans and supporting documentation is approved by the WI DNR and the Village, **the petitioner shall submit an application to amend the Village Comprehensive Land Use Map and the Village Zoning Map and Text.** All applicable applications and fees shall be submitted and paid by the petitioner. A Certificate of Compliance shall not be issued by the Village until the Comprehensive Land Use Plan Amendment, the Zoning Text Amendment and Zoning Map Amendment are approved by the Village.

RESOLUTION by action of the Village Board of the Village of Pleasant Prairie adopted this 15th day of June 2015.

Attest:

John P. Steinbrink
Village President

Jane M. Romanowski
Village Clerk



rev. 8/05

Filed 4/23 2015 Published 4/27 2015
Public Hearing 5/11 2015 5/11 2015
Fee Paid 20 Approved 20
Notices Mailed 4/24 2015 Denied: 20

VILLAGE OF PLEASANT PRAIRIE, WISCONSIN
COMMUNITY DEVELOPMENT DEPARTMENT
FLOODPLAIN BOUNDARY ADJUSTMENT

TO THE PLEASANT PRAIRIE VILLAGE PLAN COMMISSION AND BOARD OF TRUSTEES: The undersigned hereby applies for a permit to do the work herein described, and as shown on the required engineered drawings and analysis and hereby agrees that all work will be done in accordance with all the laws of the State of Wisconsin and all of the Village of Pleasant Prairie Ordinance.

Subdivision/Development Name: Uline Corporate Campus Lot Block
Property location: 12575 Uline Drive, Pleasant Prairie 53158 Zoning District(s): M-1
Abutting Body of Water/River/Stream: Existing Detention Pond - 100-yr Floodplain
Section NW 250 1/4 SECTION 25 Township TIN Range: R21E
Tax Parcel Number(s): 91-4-121-252-0203

Project Specifications: This information shall also be provided with or shown on the required site plans.

Reason and purpose for the Floodplain Adjustment: Placement of a new 22,500 SF Conference Center with in the existing 100-year floodplain on the existing pond.

Total volume proposed to be removed from the 100 year floodplain: 3795 cu. ft.
Total volume proposed to be added to the 100 year floodplain: 2326 3,795 cu. ft.

Type of fill materials (soil types) proposed to be used: Natural fill mostly consisting of clay with stabilization.

Mitigation measures or restoration methods to be used: Compensatory Storage

Time Schedule for filling: July 2015

Each applicant applying for a floodplain adjustment permit is charged with the knowledge of the requirements of the Village Zoning Ordinance. Copies of the Ordinance or portions thereof are available for sale or inspection upon request. Any statement made, site plan submitted, any project improperly constructed, any assurance given or permit erroneously issued contrary to this Ordinance is null and void and may be subject to prosecution.

Section 420-131 of the Village Zoning Ordinance entitled "FPO, FLOODPLAIN OVERLAY DISTRICT" shall be complied with. In particular, Section 420-131t of the Village Zoning Ordinance entitled "REMOVAL OF LANDS FROM FLOODPLAIN/AMENDMENTS" outlines the general requirements to amend the floodplain boundary. Please be advised that compliance with the provisions of these regulations shall not be grounds for removing lands from the floodplain, unless they are removed by filling to a height of at least two feet above the regional flood elevation, the fill is contiguous to land lying outside the floodplain, the official floodplain map is amended, and FEMA revises the Flood Insurance Rate Map or issues a Letter of Map Amendment or Revision.

I (We), have provided the non-refundable filing fee and 12 copies of the required plans and engineering data necessary to amend the floodplain boundary and one (1) copy reduced onto a 8 1/2" x 11".

I, (We), hereby certify that all the above statements and attachments submitted herewith are true and correct to the best of my knowledge, and understand the above requirements and procedures.

PROPERTY OWNER:

Name: BRIAN SCHNEIDER
(Please Print)

Signature: Brian Schneider

Address: 12575 ULINE DR

PLEASANT PRAIRIE WI 53158
(City) (State) (Zip)

Phone: 262 612 4200

Fax: 262 612 4250

Date: 4/23/15

OWNER'S AGENT:

Name: Matt Carey
(Please Print)

Signature: Matt Carey

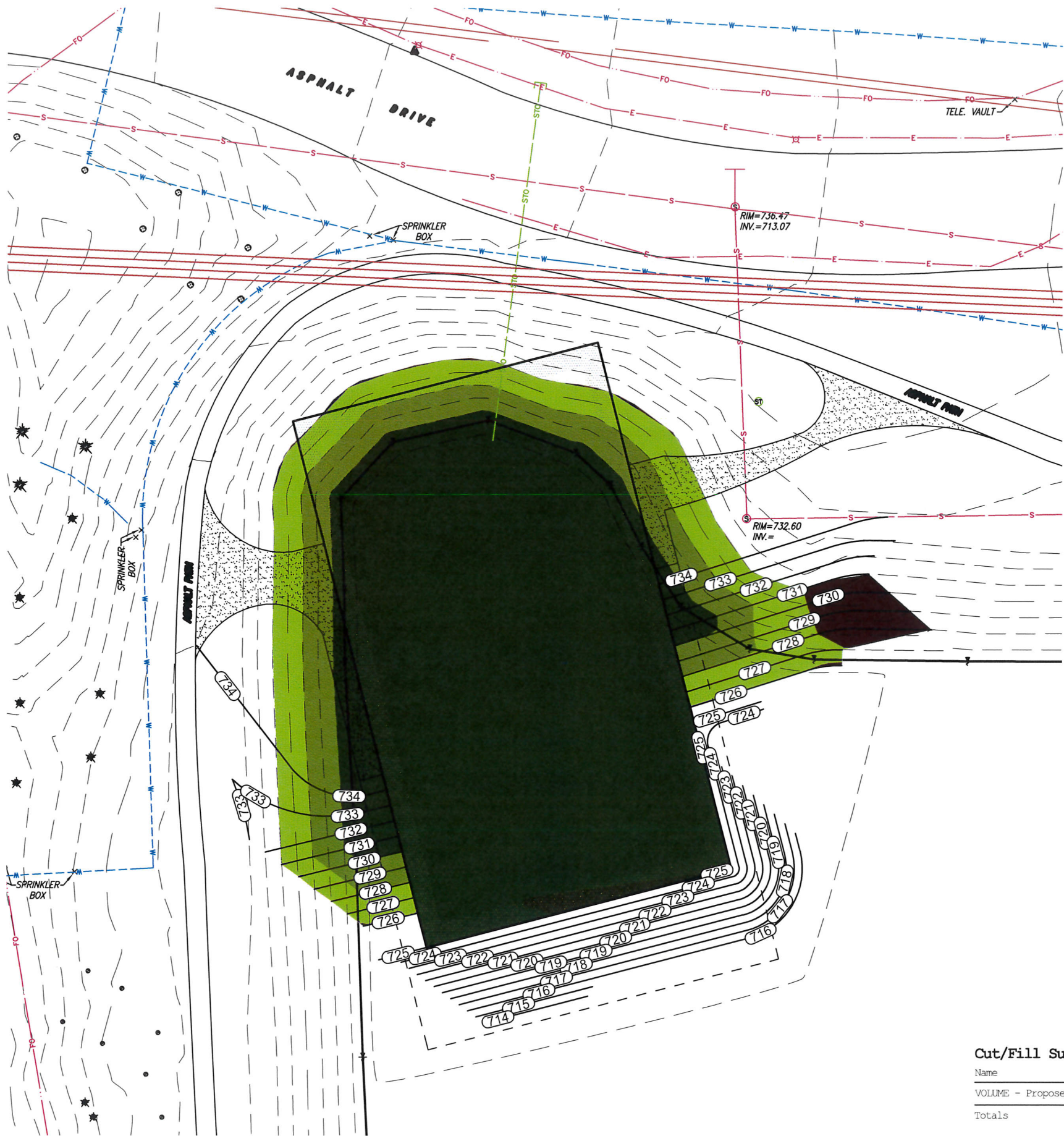
Address: 15850 W. Bluemound STE 200

Brookfield, WI 53005
(City) (State) (Zip)

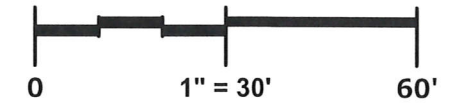
Phone: 262-754-8888

Fax: 262-754-8850

Date: 4/23/15



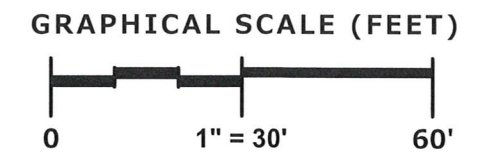
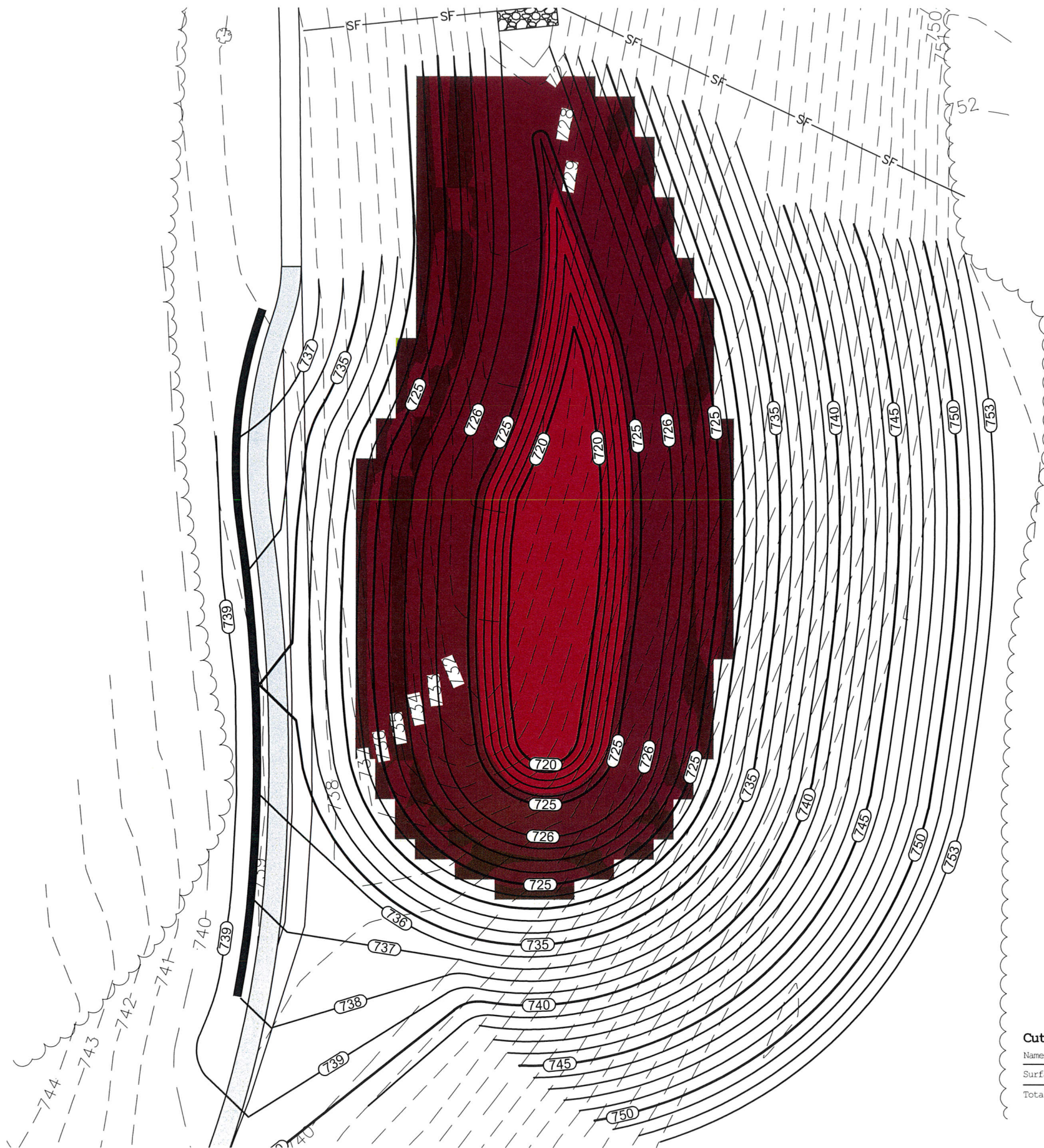
GRAPHICAL SCALE (FEET)



Elevations Table			
Number	Color	Minimum	Maximum
1	Dark Brown	-2.00	0.00
2	Light Green	0.00	2.00
3	Medium Green	2.00	4.00
4	Dark Green	4.00	6.00

Cut/Fill Summary

Name	Cut Factor	Fill Factor	2d Area	Cut	Fill	Net
VOLUME - Proposed Fill	1.00	1.00	15547.78 Sq. Ft.	4.64 Cu. Yd.	2225.98 Cu. Yd.	2221.35 Cu. Yd.<Fill>
Totals			15547.78 Sq. Ft.	4.64 Cu. Yd.	2225.98 Cu. Yd.	2221.35 Cu. Yd.<Fill>



Elevations Table			
Number	Color	Minimum	Maximum
1	■	-16.00	-12.00
2	■	-12.00	-8.00
3	■	-8.00	-4.00
4	■	-4.00	0.00
5	■	0.00	4.00

Cut/Fill Summary

Name	Cut Factor	Fill Factor	2d Area	Cut	Fill	Net
Surface - Mitigation below 732.00	1.00	1.00	15700.00 Sq. Ft.	3790.22 Cu. Yd.	0.00 Cu. Yd.	3790.22 Cu. Yd.<Cut>
Totals			15700.00 Sq. Ft.	3790.22 Cu. Yd.	0.00 Cu. Yd.	3790.22 Cu. Yd.<Cut>



Office of the
Chief of Fire & Rescue
Doug McElmury

VILLAGE STAFF MEMORANDUM

TO: Jean Werbie-Harris, Community Development Director
FROM: Doug McElmury, Chief Fire & Rescue Department
CC: Deputy Chief, Craig Roepke
Lt. Thomas Clark, Fire & Rescue Department
Peggy Herrick, Assistant Planner, Community Development
SUBJECT: Review of the Preliminary Site and Operational Plan for Uline Conference Center, 12575 Uline Drive
DATE: 7 May 2015

This is the review of the Preliminary Site and Operational Plan for Uline Conference Center, 12575 Uline Drive

The Fire and Rescue Department will be responsible for providing fire prevention inspections of this facility, twice annually. The concerns of the Fire & Rescue Department are as follows:

1. **Distribution of Comments:** *the person who obtains the building permit to all Contractors and Subcontractors affected by this document shall distribute Copies of these comments. This document outlines critical times and deadlines. All recipients of this document need to become familiar with the contents.*
2. **Compliance:** A letter shall be submitted to the Fire & Rescue Department prior to receiving a building permit, stating that the project will comply with all requirements addressed within this document.
3. In the event a conflict in code(s) is identified, or a conflict with the insurance carrier criteria occurs, the more stringent shall apply. In the event this conflicts with any codes adopted by the State of Wisconsin, the owner must petition the State directly for a variance. The Owner must demonstrate that they will provide materials or design equivalent to the code or that they will exceed the code when petitioning the State and or Village when applicable.



Upon review of the limited plans submitted, we have the following concerns:

- The proposed building is 22,500 sq. ft. with very limited access due to the location and proximity to the pond. The location of exits, exit discharge pathways and fire department access are not identified and will need to be reviewed after more detailed plans are submitted.
- The building will need to be equipped with a fire sprinkler system, a fire alarm system and recessed Knox Boxes. The combination water main feeding the building will need to be sized by a WI Licensed Fire Sprinkler Designer. Because of the limited access to the building there will need to be standpipe connections located within the building. These items will need further review once more detailed plans are submitted.
- AED. Because of the overall building size the owner shall install one or more public access Automatic External Defibrillator (AED) onsite for employee and customer use in the event of a sudden cardiac arrest. The Fire and Rescue Department can provide the training necessary to perform CPR and to operate the AED.
- Fire Alarm Control Panel: The main FACP will be placed in the fire pump room. Remote annunciator panel location(s) will also need to be determined during the pre-construction meeting.
- Both the fire department sprinkler connection and the fire hydrant, (pumper pad), shall be installed remote from the building and located a minimum distance from the building equal to the highest wall. There is no pumper pad location on the current drawing. Location and access will need to be determined and submitted on the underground plans.
- Fire hydrants are not indicated on the drawing. Fire hydrants shall be spaced no more than 350 feet apart around the perimeter of the building, per Village Ordinance 180-16
- Fire safety system plans, such as fire sprinkler and fire alarm plans, will need to be submitted to the State of Wisconsin Department of Safety and Professional Services and also to this fire department for review. No installation of any fire protection system is allowed until a satisfactory review is obtained from both departments.
- Severe Weather Shelter: The architect shall identify the area within the building that can be used as a "severe weather shelter" or "safe haven" during severe weather such as a tornado. That area will be identified with signage.
- All outside doors must have access to the interior. Such as a lock and handle provided at each door.

4. **Fire and Rescue Department Review and Comments:**

A. Site and Operational Permits

1. Site accessibility
2. Fire Pump Location
3. Pumper Pad
4. Fire hydrant spacing

B. Conditional Use and Operational

- | | |
|--------------------------------|--------------------------------|
| 1. Standpipe outlet locations | Not shown at this time. |
| 2. Fire alarm pull stations | Not shown at this time. |
| 3. Emergency and Exit Lighting | Not shown at this time. |
| 4. Fire extinguishers | |

5. **Plan Review, Permits and Fees:** The plans for the fire protection underground, aboveground and fire alarm system shall be submitted for review a minimum of four (4) weeks before installation is scheduled to begin. The Village will use an independent fire safety consultant for review of all fire protection plans submitted. A satisfactory review must be completed before any permits will be issued and before construction can begin.

6. **Insurance Carrier:** The Owner of this project shall submit to the insurance carrier for review the plans for both underground water distribution and fire protection prior to construction. The Fire & Rescue Department shall receive a copy of the comments when plans are submitted for review.

7. **The following information must be submitted with the sprinkler plans for review:**

- Building height:
- Number of stories/floors:
- Mezzanines:
- Elevators:
- Hazard class:
- Commodity:
- Maximum storage height:
- Square footage, office space:
- Square footage, receiving space:
- Square footage, shipping space:
- Square footage, warehouse space:
- Exterior storage:
- Fire protection:

8. **The following Fees and Permits are generated directly from the Fire & Rescue Department.**

NOTE: Permits are required from the Fire & Rescue Department for the installation of water main in addition to any permits required by other Village of Pleasant Prairie Departments.

- Water Usage
- Fire Protection Plans for Underground and Aboveground
- Fire Alarm System Plans
- Kitchen Hood Systems Plans
- Occupancy Permit & Re-Inspection fees

Permit fees must be paid at time of submission for review. Work cannot begin until all permits have been issued. A typical review turnaround is four weeks.

9. **Required Licenses:** A Wisconsin licensed fire protection contractor and Wisconsin licensed sprinkler fitters must install underground fire mains and aboveground fire protection. Periodic inspections of the job site will be made by fire inspectors to assure compliance.

10. **Pre-Construction Meeting:** A pre-construction meeting shall take place with the general contractor, the fire protection contractor, the Fire and Rescue Department and any other sub-contractor prior to the installation of any underground fire protection. The purpose of this meeting is to assure that the requirements of the State of Wisconsin that only a Wisconsin licensed sprinkler fitter shall perform the installation of all devices, etc. All parties will be asked to initial this document and or permit. Any violation of the installing requirements will be reported in writing to the State of Wisconsin Department of Safety and Professional Services.

11. **Site Access:** Access shall be provided to the site for all Fire Department apparatus, and must comply with the State of Wisconsin and the International Building Code, 2009 edition. A minimum wall-to-wall turning radius of 45'-0" shall be allowed for apparatus movement.

- a. All entrances from public streets, as well as road and driveways around the proposed building must be a minimum of 30 feet wide.
- b. All exterior exit pathways as well as access to the Fire Riser Room shall have a hard surface, leading to a hard surface.
- c. An exterior personnel door shall be located in close proximity to each fire sprinkler riser.

12. **Sprinkler System:** The building shall be equipped with an "automatic fire sprinkler system". The systems shall be designed and constructed to the current edition of NFPA 13, Automatic Fire Sprinklers and the Village of Pleasant Prairie Ordinance 180-16, Automatic Fire Sprinklers.

13. **Water Service:** If it is determined that the building will be serviced by a combination municipal water and fire protection main, that main must be sized by the fire protection (sprinkler) contractor. No main is allowed to travel underground, under the building.

14. **Plan Review (Underground):** A review of the underground drawings is required along with the fire protection drawings before a permit will be issued by the Fire and Rescue Department. Underground plans shall be submitted a minimum of four (4) weeks before installation begins.

15. **Standpipes:** Standpipes shall be provided in the building. In lieu of 1.5 inch hose stations, the building shall be equipped with standpipes that shall consist of 2-½ inch NST valve, capable of delivering 250 GPM, at 75 PSI measured at the standpipe valve. The standpipes shall be wet and placed adjacent to selected exterior exit doors, same side as the door handle/knob. Village Ordinance 180.16 G.
16. **Fire Hydrants:** Fire hydrants shall be spaced no more than 350 feet apart around the perimeter of the building, per Village Ordinance 180-16. The insurance carrier must agree in writing to the hydrant spacing. As many hydrants as possible shall be supplied directly by municipal water. The distance from the finished grade line to the lowest discharge shall be no less than 18 inches and no more than 23 inches. The Fire Department connections shall be located, and of sufficient height where typical snow fall or snow removal operations will not obstruct access.
17. **Fire Hydrant Acceptance:** This project will include the installation of water mains for domestic and fire protection use. Prior to the fire sprinkler system connection to any new water mains (including water mains, fire hydrants, laterals leading to the building and risers) must be hydrostatically tested flushed according to National Fire Protection Association (NFPA–National Fire Code) Standard 24 and witnessed by the Fire Chief and or the Chief’s representative, the installing contractor and the fire sprinkler contractor at a minimum.
18. Fire hydrant and water main flushing can be disruptive to the job site and requires significant coordination of all sub-contractors by the General Contractor. Nonetheless flushing is an essential part of assuring public safety.
19. The General Contractor is highly encouraged to coordinate the flushing of all new water mains, fire hydrants, laterals leading to the building and risers with both the sub-contractors responsible, the Village of Pleasant Prairie Engineering Department, Fire & Rescue Department and the Water Utility Department, prior to seeking a ‘clean water sample’ on this site.
NOTE: The Fire Protection Designer must meet with the Fire and Rescue Department before the underground drawings are submitted for review to finalize the placement of the hydrants.
20. **Pumper Pad:** There shall be dedicated space for a fire engine to have unobstructed access to the Pumper Pad. Both the Fire Department Sprinkler connection and the fire hydrant shall be installed remote from the building and located a minimum distance from the building equal to the highest wall. The fire hydrant shall be located no more than five (5) feet from the roadway and the Fire Department sprinkler connection shall be placed no more than five (5) feet from the fire hydrant. The Fire Department connection shall be constructed along with an underground drain with access for inspection. A guideline detail is attached and is meant to illustrate the requirements needed to meet the requirements stated in Village Ordinance 180-16.

NOTE: The Fire Department Connection riser shall include a single five (5) inch Storz fitting.

21. **Bollards:** Shall be placed near fire hydrants, remote post indicator valves (PIV) and Fire Department connection(s) to prevent damage. Bollards shall be 6 inches in diameter. Bollards shall not obstruct charged fire hoses. It is recommended that the Fire Department approve the location of the bollard(s) before final placement is made.
22. **Strobe Light:** A strobe light shall be provided for each riser and installed vertically above each sprinkler water flow bell. The strobe light shall operate for a sprinkler water flow. The lens color shall be RED. The strobe light shall meet Village specifications as found in section 180-16 K of the Sprinkler Ordinance.
23. **Fire Alarm System: The system shall be fully addressable so that detailed information will be received about the device in alarm.** Utilizing a fire pull station, sprinkler water flow, or any other fire detection device that maybe installed in this building shall activate the internal fire alarm system.
 - a. **Manual Fire Alarm Pull Stations:** Shall be located at a minimum, immediately adjacent to each exterior door. Any additional exterior doors will be required to meet this requirement. The pull station shall not be placed in the area of the door, but immediately adjacent to the door jamb.
 - b. **Pull Stations and Audiovisual Alarms:** Shall be installed per ADA requirements.
 - c. **Smoke and Heat Detection:** Shall be installed as required.
 - d. **Tamper Switches:** Tamper switches shall be placed on all sprinkler valves and be identified on the annunciator panel.
 - e. **Fire Alarm Control Panel: Shall be addressable.** The annunciator panel type shall be approved by the Fire & Rescue Department. The Fire Alarm Control Panel shall be located within the Fire Pump Room. The panel shall identify a fire sprinkler water flow by riser, and the specific locations of the fire alarm pull stations and any other fire detection devices that may be installed in this building.
 - f. **Annunciator Panel: Shall be addressable.** The annunciator panel type shall be approved by the Fire and Rescue Department. The panel shall identify a fire sprinkler water flow by riser, and the specific locations of the fire alarm pull stations and any other fire detection devices that may be installed in this building.
 - g. **Transmission of Fire Alarms.** The method of transmission to central station must be approved by the Fire & Rescue Department. i.e. Phone line, RF Radio and /or Cellular technologies.
 - h. **Central Station:** The Fire Alarm Control Panel shall transmit all fire alarm, tamper, trouble and supervisory signals to a central station that is certified by Underwriters Laboratories (UL) and/or Factory Mutual (FM) and approved by the Fire & Rescue Department. The owner shall provide such documentation for approval. It is recommended that the owner consult with the Fire & Rescue Department prior to signing any contracts with the Central station.
 - 1) The central station shall be provided with this information regarding the geographical location of this alarm:
Village of Pleasant Prairie, County of Kenosha, State of Wisconsin

Fire:	Pleasant Prairie Fire & Rescue
Medical:	Pleasant Prairie Fire & Rescue

Phone numbers:

Emergency: (262) 694-1402

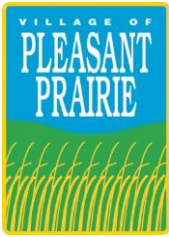
Non-emergency: (262) 694-7105

Business: (262) 694-8027

24. **Knox Box:** Knox Boxes shall be provided for the building, a determination of the exact number required will need to be made during the pre-construction meeting. One by each riser door, fire riser room and other needed access routes. The Knox Boxes shall be Model 4400. Two sets of all keys (Master, fire alarm pull station, annunciator, elevator, etc.) shall be placed within the box, as well as a copy of the pre-fire plan.
25. **MSDS Knox Box:** A minimum of One (1) Knox Box(s) designed for Material Safety Data Sheet storage shall be provided for each tenant to contain the data sheets on all products that are considered hazardous within the facility. The MSDS Box(s) shall be installed within the Fire Riser Room.
26. **Fire Extinguishers:** Shall meet NFPA 10 (Portable Fire Extinguishers) for the specific use of the building and be in sufficient number. Final approval, of fire extinguisher locations and quantity, will not be given until occupancy is taken, to see how a tenant furnishes the space. The company providing the fire extinguishers shall submit a letter to the Fire and Rescue Department stating the locations and size of the extinguishers are in compliance with NFPA 10.
27. **Emergency and Exit Lighting:** Exit and Emergency Lighting shall be provided and shall have battery backup. Combination units are acceptable and recommended. An Emergency Generator eliminates the need for battery backup. Exit and Emergency Lighting shall not be placed on electrical circuits that cannot be disturbed or interrupted, this is for test purposes. These circuits shall be clearly labeled. An Emergency light shall be placed within the fire riser room.
28. **Final Inspection:** The General Contractor shall provide the following documentation at the time the Final Inspection takes place and before a building occupancy certificate will be issued.
- a. The fire protection contractor shall provide the owner with a letter (upon completion of the sprinkler work) stating the sprinkler system, or portion thereof, is "100% operational and built according to the design", Village Ordinance, 180-16 N.
 - b. Copy of contract with fire alarm central monitoring station.
 - c. Copy of UL and/or FM certificate(s) for the fire alarm central monitoring station.
 - d. Copies of the fire protection underground flushing documents.
 - e. Copies of the underground and fire sprinkler hydrostatic test certificates.
 - f. Copies of the fire sprinkler operational test certificates.
 - g. Copies of the fire alarm test documents.
 - h. Copies of other test documents such as, hood/duct, smoke, etc...
 - i. The Pleasant Prairie Fire and Rescue Department shall have all information needed for our pre-fire plan prior to occupancy.
 - j. Provide two- (2) CD's, one for the property owner and one for the Fire & Rescue Department. The disks shall include all Floor plans and fire protection plans for the building in an as-built condition.
 - k. Severe Weather Shelter: The architect shall provide for both the Owner and the Fire &

Rescue Department the area within the building that can be used as a “severe weather shelter” or “safe haven” during severe weather such as a tornado.

- l. Maps of the fire alarm and fire sprinkler system shall be placed in the fire pump room, near the fire alarm control panel; the maps shall be hung on the wall, with a waterproof covering and accessible to firefighters wearing bulky clothes and equipment.
 - m. AED, in place at such time a tenant takes occupancy.
 - n. A copy of the tenants Emergency Plan must be submitted to the Fire & Rescue Department before occupancy.
29. **Fees:** Occupancy inspection fee and re-inspection fee will be assessed at the final inspection in accordance with ordinance 180-17.
30. **Occupancy:** All fire and life safety requirements must be in place prior to any building being occupied.



Village Staff Memorandum

To: Jean Werbie-Harris, Community Development Director
From: Sandro Perez, Building Inspection Superintendent
Subject: Uline Conference Center
Date: May 5, 2015

1. Building Inspection Department information:
Hours: Mon-Fri, 8am-5pm.
Phone# 262-694-9304
Email: buildinginspection@plprairiewi.com
2. Permit applications can be found online at pleasantprairieonline.com
3. Inspections are performed Mon- Fri 9am-4pm except electrical inspections; those are only Tue & Thu mornings. 48hr notice is required to schedule an inspection. Please note you must call and speak with a Building Inspection Department representative a voicemail or email will not constitute an inspection please plan accordingly. Final occupancy inspections require coordination with multiple departments and staff members there for a minimum of 72 hour notice is required. Any re-inspection fees due must be paid prior to scheduling a re-inspection.
4. All contractors requiring permits shall not commence work until permit issuance. We have 10 business days to review and issue permits please plan accordingly.
5. We inspect to the 2009 IBC, IEBC, IMC, IECC, IFGC, 2003 ANSI A117.1, 2011 NEC and WI. Plumbing code SPS 381-386.
6. All state approved drawing must be available at job site for inspector review.
7. Please submit emergency egress path / lighting plan and energy compliance worksheets form SBD 10512 to Building Inspection Department prior to issuance of building permit.
8. Fire alarm systems require two permits from both the Fire Department and Building Inspection Department.
9. Any building fire protection loop and combination water main will require approval by the Fire Department prior to issuance of exterior plumbing permit.
10. Both fire alarm systems and fire protection loops will require inspections by both the Fire Department and Building Inspection Department.

11. VOPP recommends all exterior lot and building lighting to be LED and Dark Sky compliant with a color temperature between 4000K-5000K.
12. Any tradesmen requiring state license will be “carded” on the jobsite for compliance.
13. All equipment must be “LISTED” by a nationally recognized testing laboratory.
14. Documentation must be provided for available fault current at equipment, to verify short circuit current rating compliance per 2011 NEC 110.10
15. All equipment, materials, etc. must be rated for the environment in which they will be used.



rev. 8/05

Filed 4/23 2015 Published 4/27 2015
Public Hearing 5/11 2015 5/11 2015
Fee Paid _____ 20 Approved _____ 20
Notices Mailed 4/24 2015 Denied: _____ 20

**VILLAGE OF PLEASANT PRAIRIE, WISCONSIN
COMMUNITY DEVELOPMENT DEPARTMENT
FLOODPLAIN BOUNDARY ADJUSTMENT**

TO THE PLEASANT PRAIRIE VILLAGE PLAN COMMISSION AND BOARD OF TRUSTEES: The undersigned hereby applies for a permit to do the work herein described, and as shown on the required engineered drawings and analysis and hereby agrees that all work will be done in accordance with all the laws of the State of Wisconsin and all of the Village of Pleasant Prairie Ordinance.

Subdivision/Development Name: Uline Corporate Campus Lot _____ Block _____
Property location: 12575 Uline Drive, Pleasant Prairie 53158 Zoning District(s): ~~A1A2~~ M-1
Abutting Body of Water/River/Stream: Existing Detention Pond - 100-yr Floodplain
Section NW 250 1/4 SECTION 25 Township T1N Range: R21E
Tax Parcel Number(s): 91-4-121-252-0203

Project Specifications: This information shall also be provided with or shown on the required site plans.

Reason and purpose for the Floodplain Adjustment: Placement of a new 22,500 SF Conference Center with in the existing 100-year floodplain on the existing pond.

Total volume proposed to be removed from the 100 year floodplain: 3795 2,226 cu. ft.
Total volume proposed to be added to the 100 year floodplain: 2326 3,795 cu. ft.

Type of fill materials (soil types) proposed to be used: Natural fill mostly consisting of clay with stabilization.

Mitigation measures or restoration methods to be used: Compensatory Storage

Time Schedule for filling: JULY 2015

Each applicant applying for a floodplain adjustment permit is charged with the knowledge of the requirements of the Village Zoning Ordinance. Copies of the Ordinance or portions thereof are available for sale or inspection upon request. Any statement made, site plan submitted, any project improperly constructed, any assurance given or permit erroneously issued contrary to this Ordinance is null and void and may be subject to prosecution.

Section 420-131 of the Village Zoning Ordinance entitled "FPO, FLOODPLAIN OVERLAY DISTRICT" shall be complied with. In particular, Section 420-131t of the Village Zoning Ordinance entitled "REMOVAL OF LANDS FROM FLOODPLAIN/AMENDMENTS" outlines the general requirements to amend the floodplain boundary. Please be advised that compliance with the provisions of these regulations shall not be grounds for removing lands from the floodplain, unless they are removed by filling to a height of at least two feet above the regional flood elevation, the fill is contiguous to land lying outside the floodplain, the official floodplain map is amended, and FEMA revises the Flood Insurance Rate Map or issues a Letter of Map Amendment or Revision.

I (We), have provided the non-refundable filing fee and 12 copies of the required plans and engineering data necessary to amend the floodplain boundary and one (1) copy reduced onto a 8 1/2" x 11".

I, (We), hereby certify that all the above statements and attachments submitted herewith are true and correct to the best of my knowledge, and understand the above requirements and procedures.

PROPERTY OWNER:

Name: BRIAN SCHNEIDER
(Please Print)

Signature: Brian Schneider

Address: 12575 ULINE DR

PLEASANT PRAIRIE WI 53158
(City) (State) (Zip)

Phone: 262 612 4200

Fax: 262 612 4250

Date: 4/23/15

OWNER'S AGENT:

Name: Matt Carey
(Please Print)

Signature: Matt Carey

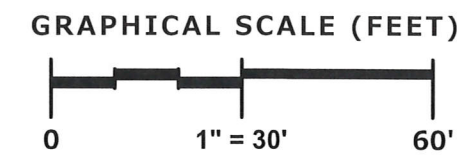
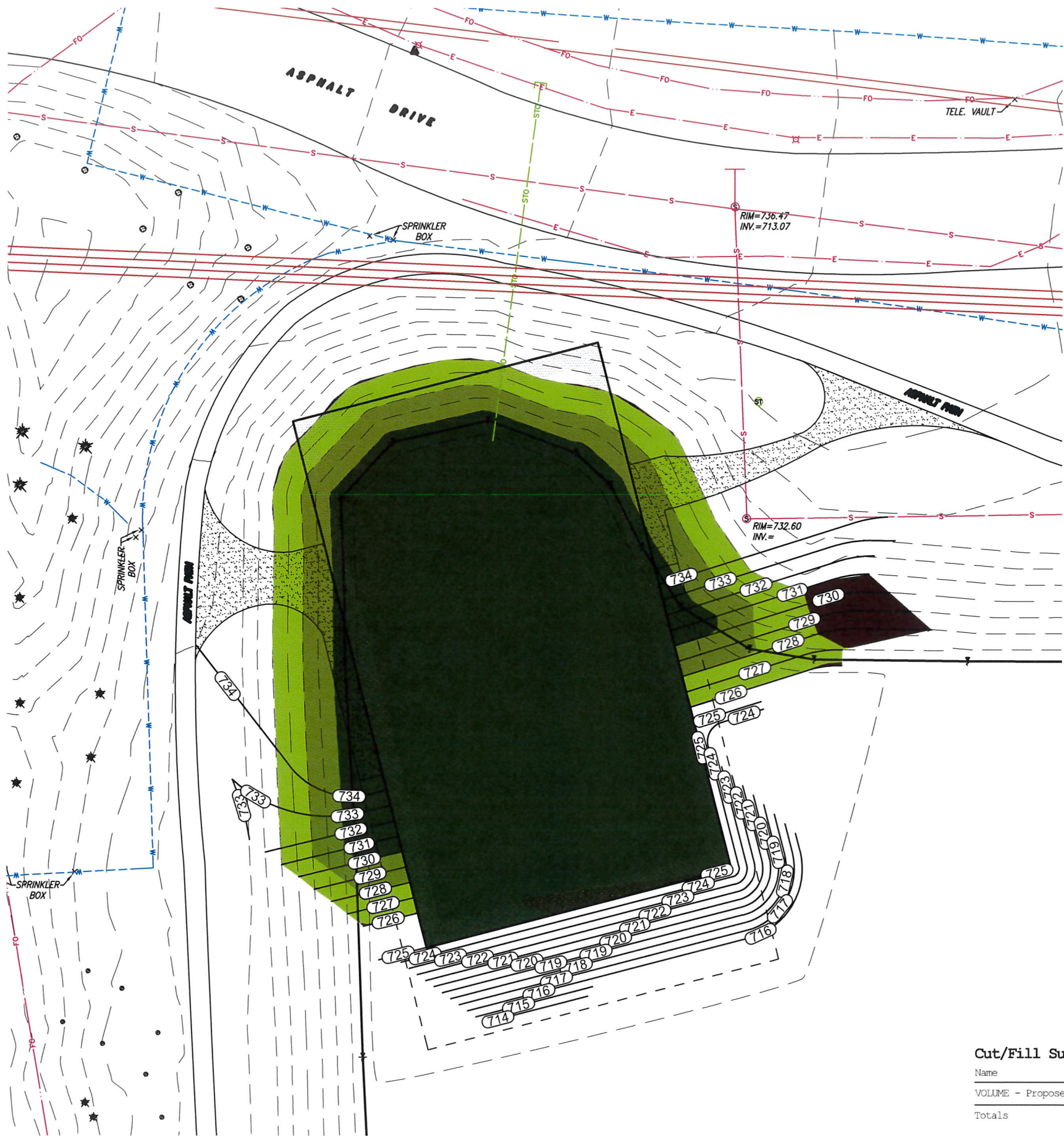
Address: 15850 W. Bluemound STE 200

Brookfield, WI 53005
(City) (State) (Zip)

Phone: 262-754-8888

Fax: 262-754-8850

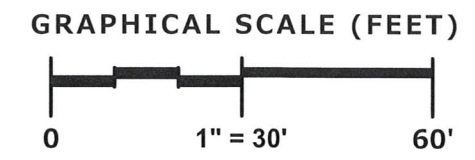
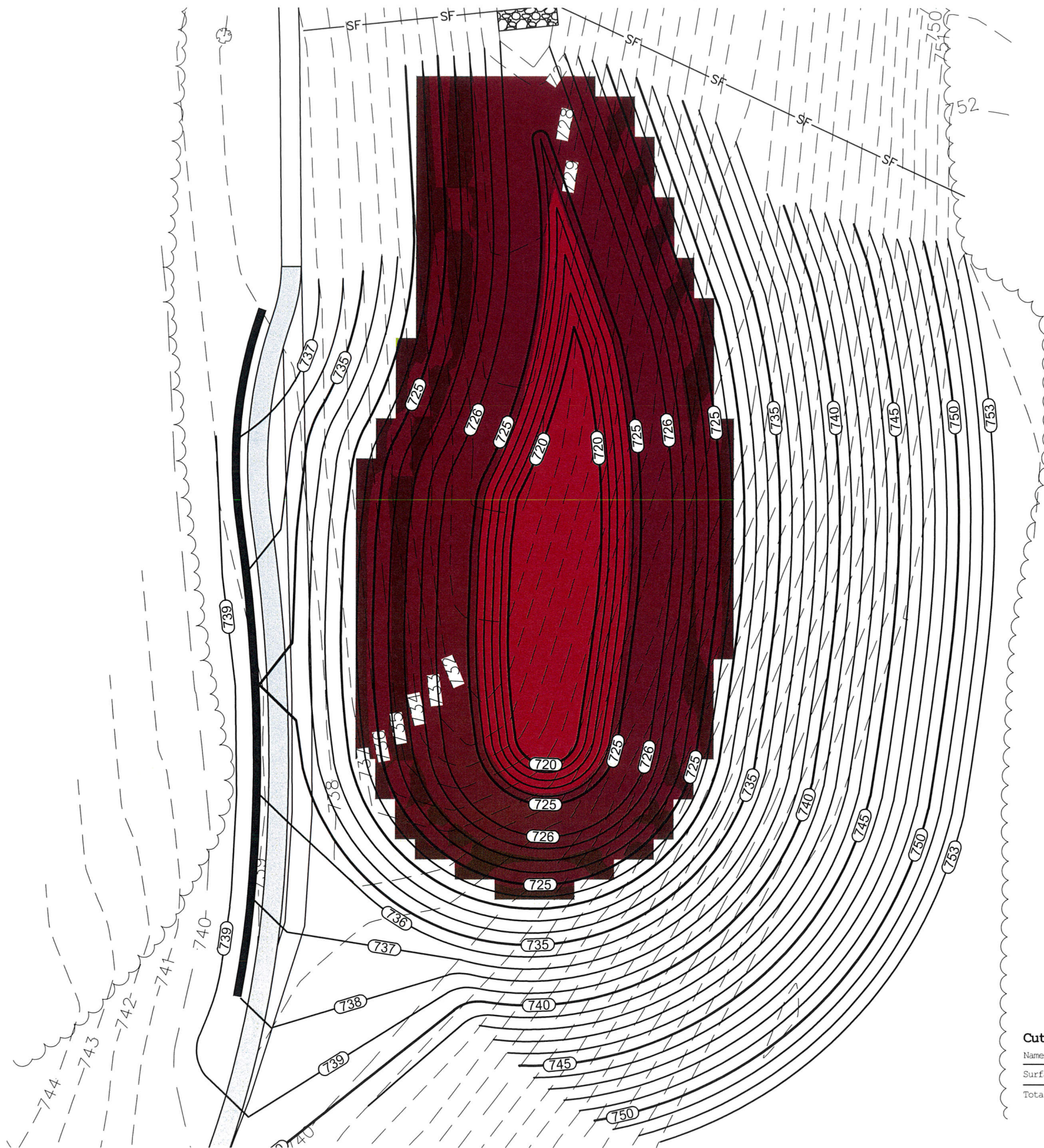
Date: 4/23/15



Elevations Table			
Number	Color	Minimum	Maximum
1	Dark Brown	-2.00	0.00
2	Light Green	0.00	2.00
3	Medium Green	2.00	4.00
4	Dark Green	4.00	6.00

Cut/Fill Summary

Name	Cut Factor	Fill Factor	2d Area	Cut	Fill	Net
VOLUME - Proposed Fill	1.00	1.00	15547.78 Sq. Ft.	4.64 Cu. Yd.	2225.98 Cu. Yd.	2221.35 Cu. Yd.<Fill>
Totals			15547.78 Sq. Ft.	4.64 Cu. Yd.	2225.98 Cu. Yd.	2221.35 Cu. Yd.<Fill>



Elevations Table			
Number	Color	Minimum	Maximum
1	■	-16.00	-12.00
2	■	-12.00	-8.00
3	■	-8.00	-4.00
4	■	-4.00	0.00
5	■	0.00	4.00

Cut/Fill Summary

Name	Cut Factor	Fill Factor	2d Area	Cut	Fill	Net
Surface - Mitigation below 732.00	1.00	1.00	15700.00 Sq. Ft.	3790.22 Cu. Yd.	0.00 Cu. Yd.	3790.22 Cu. Yd.<Cut>
Totals			15700.00 Sq. Ft.	3790.22 Cu. Yd.	0.00 Cu. Yd.	3790.22 Cu. Yd.<Cut>



DESIGNED: AEA
DRAWN: MWA
REVIEWED: AEA
THESE PLANS AND DESIGNS ARE COPYRIGHT PROTECTED AND MAY NOT BE USED IN WHOLE OR IN PART WITHOUT THE WRITTEN CONSENT OF PINNACLE ENGINEERING GROUP, LLC

ONSITE CIVIL ENGINEERING INFRASTRUCTURE PLANS

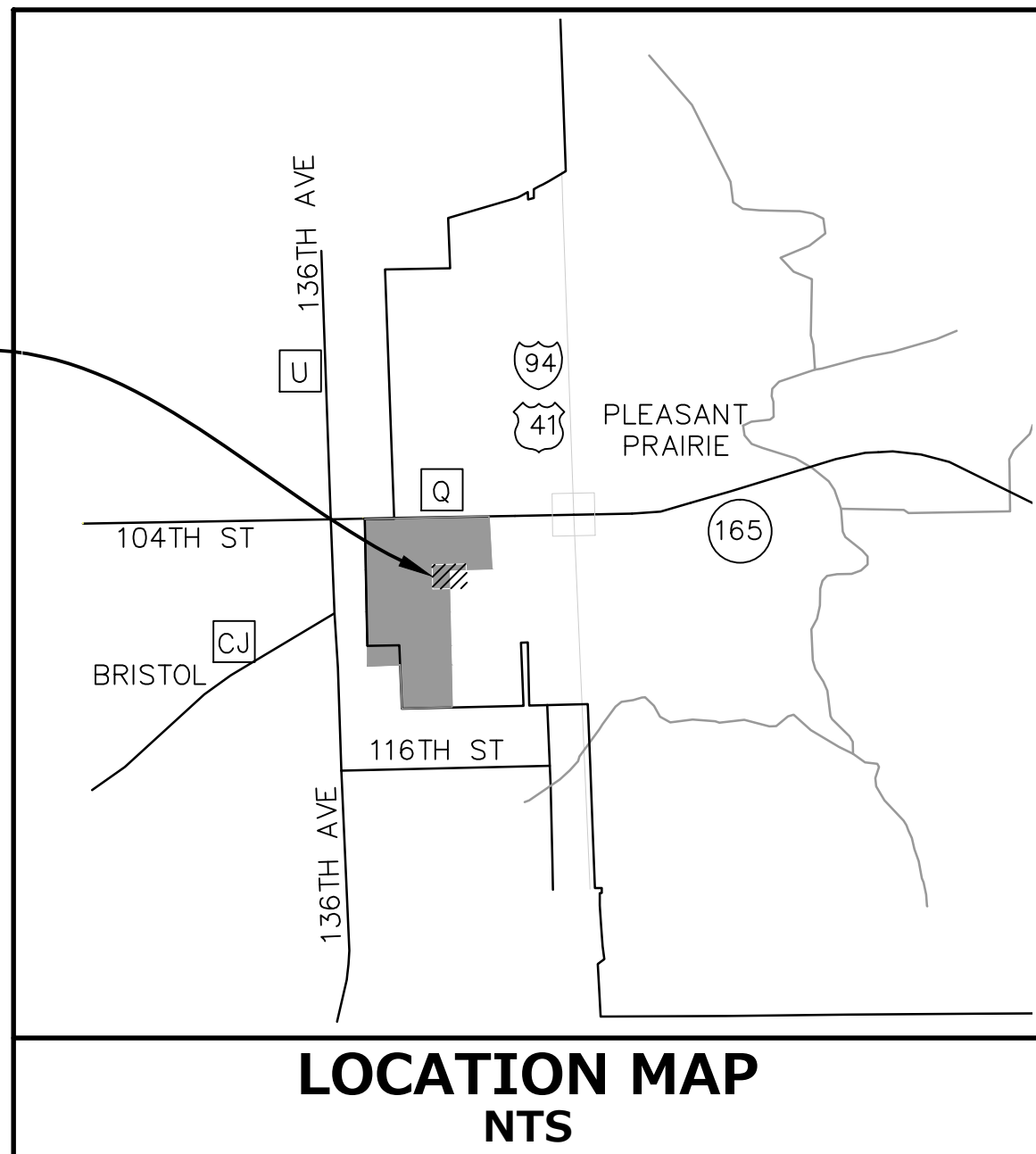
FOR

CONFERENCE CENTER AT ULINE CORPORATE CAMPUS

PLEASANT PRAIRIE, WISCONSIN

PLANS PREPARED FOR
ROUTE 165, LLC
12575 ULIN DRIVE
PLEASANT PRAIRIE, WI 53158

LEGEND		EXISTING	PROPOSED
SANITARY MANHOLE		⊙	⊙
STORM MANHOLE		⊙	⊙
CATCH BASIN		□	■
INLET		○	●
PRECAST FLARED END SECTION		△	▲
CONCRETE HEADWALL		∩	∩
VALVE VAULT		⊗	⊗
VALVE BOX		⊕	⊕
FIRE HYDRANT		⊙	⊙
BUFFALO BOX		⊙	⊙
CLEANOUT		⊙	⊙
SANITARY SEWER		—	—
FORCE MAIN		—	—
STORM SEWER		—	—
WATER MAIN		—	—
UTILITY CROSSING		—	—
GRANULAR TRENCH BACKFILL		▨	▨
LIGHTING		⊙	⊙
ELECTRICAL CABLE		—	—
ELECTRICAL TRANSFORMER OR PEDESTAL		⊙	⊙
POWER POLE		⊙	⊙
POWER POLE WITH LIGHT		⊙	⊙
GUY WIRE		—	—
STREET SIGN		⊙	⊙
GAS MAIN		—	—
TELEPHONE LINE		—	—
CONTOUR		—	—
SPOT ELEVATION		⊙	⊙
WETLANDS		▨	▨
FLOODWAY		—	—
FLOODPLAIN		—	—
HIGH WATER LEVEL (HWL)		—	—
NORMAL WATER LEVEL (NWL)		—	—
DIRECTION OF SURFACE FLOW		→	→
DITCH OR SWALE		—	—
DIVERSION SWALE		—	—
OVERFLOW RELIEF ROUTING		→	→
TREE WITH TRUNK SIZE		⊙	⊙
SOIL BORING		⊙	⊙
TOPSOIL PROBE		⊙	⊙
FENCE LINE, TEMPORARY SILT		—	—
FENCE LINE, WIRE		—	—
FENCE LINE, CHAIN LINK OR IRON		—	—
FENCE LINE, WOOD OR PLASTIC		—	—
CONCRETE SIDEWALK		—	—
CURB AND GUTTER		—	—
DEPRESSED CURB		—	—
REVERSE PITCH CURB & GUTTER		—	—
EASEMENT LINE		—	—



INDEX OF SHEETS	
C-1	COVER SHEET
C-2	EXISTING CONDITIONS
C-3	GRADING, PAVING & EROSION CONTROL PLAN
C-4	UTILITY PLAN
C-5	CONSTRUCTION DETAILS

PROJECT TEAM CONTACTS	
CIVIL ENGINEER: ADAM ARTZ, P.E. PINNACLE ENGINEERING GROUP 15850 BLUEMOUND ROAD, SUITE 210 BROOKFIELD, WI 53005 (262) 754-8888 MATT CAREY, P.E. PINNACLE ENGINEERING GROUP 15850 BLUEMOUND ROAD, SUITE 210 BROOKFIELD, WI 53005 (262) 754-8888	LANDSCAPING: VANCE BARNES, P.L.A. DAVID J. FRANK LANDSCAPE CONTRACTING N120 W213450 FRIESTADT ROAD GERMANTOWN, WI 53022 (262) 255-4888
APPLICANT: RANDY COPENHARVE ROUTE 165, LLC 12575 ULIN DRIVE PLEASANT PRAIRIE, WI 53158 (262) 612-4200	ARCHITECT: NSA 333 EAST ERIE STREET MILWAUKEE, WI 53202 (414) 278-7734

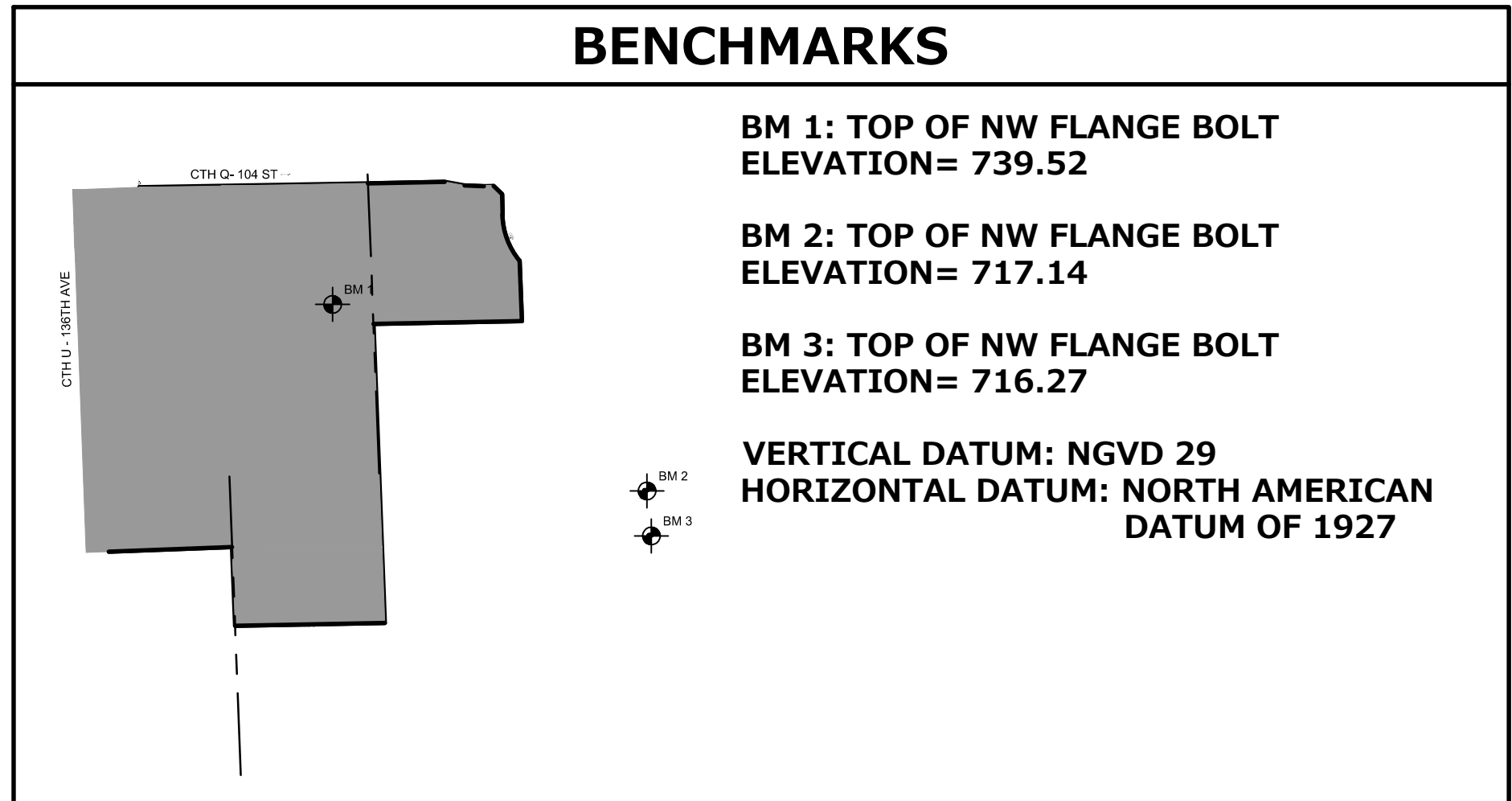
- ### GENERAL NOTES
- THE INTENTION OF THE PLANS AND SPECIFICATIONS IS TO SET FORTH PERFORMANCE AND CONSTRUCTION MATERIAL STANDARDS FOR THE PROPER EXECUTION OF WORK. ALL WORKS CONTAINED WITHIN THE PLANS AND SPECIFICATIONS SHALL BE COMPLETED IN ACCORDANCE WITH ALL REQUIREMENTS FROM LOCAL, STATE, FEDERAL OR OTHER GOVERNING AGENCY'S LAWS, REGULATIONS, JURISDICTIONAL ORDINANCES/CODES/RULES/ETC., AND THE OWNER'S DIRECTION.
 - MULTIPLE GEOTECHNICAL EXPLORATION REPORTS HAVE BEEN PREPARED FOR THIS PROJECT. MOST RECENTLY BY PROFESSIONAL SERVICE INDUSTRIES (GEOTECHNICAL CONSULTANT) (DATED JUNE 04, 2014) AND TERRACON (DATED JUNE 07, 2010) (APRIL 08, 2009) (MAY 09, 2008). FOR THIS PROJECT SITE, THE DATE ON SUB-SURFACE SOIL CONDITIONS IS NOT INTENDED AS REPRESENTATION OR WARRANTY OF THE CONTINUITY OF SUCH CONDITIONS BETWEEN BORINGS OR INDICATED SAMPLING LOCATIONS. IT SHALL BE EXPRESSLY UNDERSTOOD THAT THE OWNER WILL NOT BE RESPONSIBLE FOR ANY INTERPRETATIONS OR CONCLUSIONS DRAWN THERE FROM BY THE CONTRACTOR. DATE IS MADE AVAILABLE FOR THE CONVENIENCE OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR PERFORMING ANY ADDITIONAL SOILS INVESTIGATIONS THEY FEEL IS NECESSARY FOR THE PROPER EVALUATION OF THE SITE FOR PURPOSES OF PLANNING, BIDDING OR CONSTRUCTING THE PROJECT AT NO ADDITIONAL COST TO THE OWNER.
 - THE CONTRACTOR IS RESPONSIBLE TO REVIEW AND UNDERSTAND ALL COMPONENTS OF THE PLANS AND SPECIFICATIONS, INCLUDING FIELD VERIFYING SOIL CONDITIONS, PRIOR TO SUBMISSION OF A BID PROPOSAL.
 - THE CONTRACTOR SHALL PROMPTLY REPORT ANY ERRORS OR AMBIGUITIES LEARNED AS PART OF THEIR REVIEW OF PLANS, SPECIFICATIONS, REPORTS AND FIELD INVESTIGATIONS.
 - THE CONTRACTOR IS SOLELY RESPONSIBLE FOR THE COMPUTATION OF QUANTITIES AND WORK REQUIRED TO COMPLETE THIS PROJECT. THE CONTRACTOR'S BID SHALL BE BASED ON ITS OWN COMPUTATIONS AND IN NO SUCH INSTANCE RELY ON THE ENGINEER'S ESTIMATE.
 - QUESTIONS/CLARIFICATIONS WILL BE INTERPRETED BY ENGINEER/OWNER PRIOR TO THE AWARD OF CONTRACT. ENGINEER/OWNER WILL SUBMIT OFFICIAL RESPONSES IN WRITING. INTERPRETATIONS PRESENTED IN OFFICIAL RESPONSES SHALL BE BINDING ON ALL PARTIES ASSOCIATED WITH THE CONTRACT. IN NO WAY SHALL WORD-OF-MOUTH DIALOG CONSTITUTE AN OFFICIAL RESPONSE.
 - PRIOR TO START OF WORK, CONTRACTOR SHALL BE COMPLETELY FAMILIAR WITH ALL CONDITIONS OF THE SITE, AND SHALL ACCOUNT FOR CONDITIONS THAT AFFECT OR MAY AFFECT CONSTRUCTION INCLUDING, BUT NOT LIMITED TO, LIMITATIONS OF WORK ACCESS, SPACE LIMITATIONS, OVERHEAD OBSTRUCTIONS, TRAFFIC PATTERNS, LOCAL REQUIREMENTS, ADJACENT ACTIVITIES, ETC. FAILURE TO CONSIDER SITE CONDITIONS SHALL NOT BE CAUSE FOR CLAIM OF JOB EXTRAS.
 - COMMENCEMENT OF CONSTRUCTION SHALL EXPLICITLY CONFIRM THAT THE CONTRACTOR HAS REVIEWED THE PLANS AND SPECIFICATIONS IN ENTIRETY AND CERTIFIES THAT THEIR SUBMITTED BID PROPOSAL CONTAINS PROVISIONS TO COMPLETE THE PROJECT, WITH THE EXCEPTION OF UNFORESEEN FIELD CONDITIONS. ALL APPLICABLE PERMITS HAVE BEEN OBTAINED, AND CONTRACTOR UNDERSTANDS ALL OF THE REQUIREMENTS OF THE PROJECT.
 - SHOULD ANY DISCREPANCIES OR CONFLICTS IN THE PLANS OR SPECIFICATIONS BE DISCOVERED AFTER THE AWARD OF CONTRACT, ENGINEER SHALL BE NOTIFIED IN WRITING IMMEDIATELY AND CONSTRUCTION OF ITEMS AFFECTED BY THE DISCREPANCIES/CONFLICTS SHALL NOT COMMENCE, OR CONTINUE, UNTIL A WRITTEN RESPONSE FROM ENGINEER/OWNER IS DISTRIBUTED. IN THE EVENT OF A CONFLICT BETWEEN REFERENCED CODES, STANDARDS, SPECIFICATIONS AND PLANS, THE ONE ESTABLISHING THE MOST STRINGENT REQUIREMENTS SHALL BE FOLLOWED.
 - THE CONTRACTOR SHALL, AT ITS OWN EXPENSE, OBTAIN ALL NECESSARY PERMITS AND LICENSES TO COMPLETE THE PROJECT. OBTAINING PERMITS, OR DELAYS, IS NOT CAUSE FOR DELAY OF THE CONTRACT OR SCHEDULE. CONTRACTOR SHALL COMPLY WITH ALL PERMIT REQUIREMENTS.
 - THE CONTRACTOR SHALL NOTIFY ALL INTERESTED GOVERNING AGENCIES, UTILITY COMPANIES AFFECTED BY THIS CONSTRUCTION PROJECT, AND DIGGER'S HOTLINE IN ADVANCE OF CONSTRUCTION TO COMPLY WITH ALL JURISDICTIONAL ORDINANCES/CODES/RULES/ETC., PERMIT STIPULATIONS, AND OTHER APPLICABLE STANDARDS.
 - SAFETY IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE TO INITIATE, INSTITUTE, ENFORCE, MAINTAIN, AND SUPERVISE ALL SAFETY PRECAUTIONS AND JOB SITE SAFETY PROGRAMS IN CONNECTION WITH THE WORK.
 - CONTRACTOR SHALL KEEP THE JOBSITE CLEAN AND ORDERLY AT ALL TIMES. ALL LOCATIONS OF THE SITE SHALL BE KEPT IN A WORKING MANNER SUCH THAT DEBRIS IS REMOVED CONTINUOUSLY AND ALL RESPECTIVE CONTRACTORS OPERATE UNDER GENERAL "GOOD HOUSEKEEPING".
 - THE CONTRACTOR SHALL INDEMNIFY THE OWNER, ENGINEER, AND THEIR AGENTS FROM ALL LIABILITY INVOLVED WITH THE CONSTRUCTION, INSTALLATION, AND TESTING OF THE WORK ON THIS PROJECT.
 - PRIOR TO CONSTRUCTION A PRE-CONSTRUCTION MEETING MUST BE HELD AT THE VILLAGE OFFICES. THE PRE-CONSTRUCTION MEETING SHALL BE SCHEDULED AND MODERATED BY THE DESIGN ENGINEER OF RECORD.

- ### REQUIRED SUBMITTALS FOR APPROVAL
- HOT MIX ASPHALT - MIX DESIGN
 - CONCRETE PAVEMENTS (EXTERIOR) - MIX DESIGN & JOINT PLAN
 - PAVEMENT STONE BASE COURSE - GRADATION
 - PIPE BEDDING & TRENCH BACKFILL - GRADATION
 - MANHOLE BACKFILL - GRADATION
 - PAVEMENT MARKING PAINT
 - CHEMICAL SOIL STABILIZATION MIX DESIGN (IF APPLICABLE)

- ### REQUIRED SUBMITTALS FOR RECORDS
- WATER MAIN PIPES, VALVES, HYDRANTS & FITTINGS
 - SANITARY SEWER PIPE & FITTINGS
 - STORM SEWER PIPE, STRUCTURES, & FITTINGS
 - TRACER WIRE
 - UNDERGROUND UTILITY LOCATION RECORD DRAWINGS
 - ADA SIGN & TRUNCATED DOMES, PAINT & STENCILS (IF APPLICABLE)

ABBREVIATIONS

BL	BASE LINE	PT	POINT OF TANGENCY
C	LONG CHORD OF CURVE	PVI	POINT OF VERTICAL INTERSECTION
C & G	CURB AND GUTTER	R	RADIUS
CB	CATCH BASIN	ROW	RIGHT-OF-WAY
CL	CENTERLINE	SAN	SANITARY SEWER
D	DEGREE OF CURVE	ST	STORM SEWER
EP	EDGE OF PAVEMENT	T	TANGENCY OF CURVE
FF	FINISHED FLOOR	TB	TOP OF BANK
FG	FINISHED GRADE	TC	TOP OF CURB
FL	FLOW LINE	TF	TOP OF FOUNDATION
FP	FLOODPLAIN	TP	TOP OF PIPE
FR	FRAME	TS	TOP OF SIDEWALK
FW	FLOODWAY	TW	TOP OF WALK
HWL	HIGH WATER LEVEL	WM	WATER MAIN
INV	INVERT	Δ	INTERSECTION ANGLE
L	LENGTH OF CURVE		
MH	MANHOLE		
NWL	NORMAL WATER LEVEL		
PC	POINT OF CURVATURE		



FOR REVIEW

Toll Free (800) 242-8511
Milwaukee Area (414) 259-1181
Hearing Impaired TDD (800) 545-2289
www.DiggersHotline.com

EXPIRATION DATE: JULY 31, 2016

PINNACLE ENGINEERING GROUP, LLC
ENGINEER'S LIMITATION

PINNACLE ENGINEERING GROUP, LLC AND THEIR CONSULTANTS DO NOT WARRANT OR GUARANTEE THE ACCURACY AND COMPLETENESS OF THE DELIVERABLES HEREIN BEYOND A REASONABLE DILIGENCE. IF ANY MISTAKES, OMISSIONS, OR DISCREPANCIES ARE FOUND TO EXIST WITHIN THE DELIVERABLES, THE ENGINEER SHALL BE PROMPTLY NOTIFIED PRIOR TO BID SO THAT HE MAY HAVE THE OPPORTUNITY TO TAKE WHATEVER STEPS NECESSARY TO RESOLVE THEM. FAILURE TO PROMPTLY NOTIFY THE ENGINEER OF SUCH CONDITIONS SHALL ABSOLVE THE ENGINEER FROM ANY RESPONSIBILITY FOR THE CONSEQUENCES OF SUCH FAILURE. ACTIONS TAKEN WITHOUT THE KNOWLEDGE AND CONSENT TO THE ENGINEER, OR IN CONTRADICTION TO THE ENGINEER'S DELIVERABLES OR RECOMMENDATIONS, SHALL BECOME THE RESPONSIBILITY NOT OF THE ENGINEER BUT OF THE PARTIES RESPONSIBLE FOR TAKING SUCH ACTION.

FURTHERMORE, PINNACLE ENGINEERING GROUP, LLC IS NOT RESPONSIBLE FOR CONSTRUCTION SAFETY OR THE MEANS AND METHODS OF CONSTRUCTION.

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PINNACLE ENGINEERING GROUP
ENGINEERING | NATURAL RESOURCES | SURVEYING

WISCONSIN OFFICE:
15850 W. BLUEMOUND ROAD
BROOKFIELD, WI 53005
(262) 754-8888
CHICAGO | MILWAUKEE | NATIONWIDE

ULINE CORP. CAMPUS CONF. CENTER
PLEASANT PRAIRIE

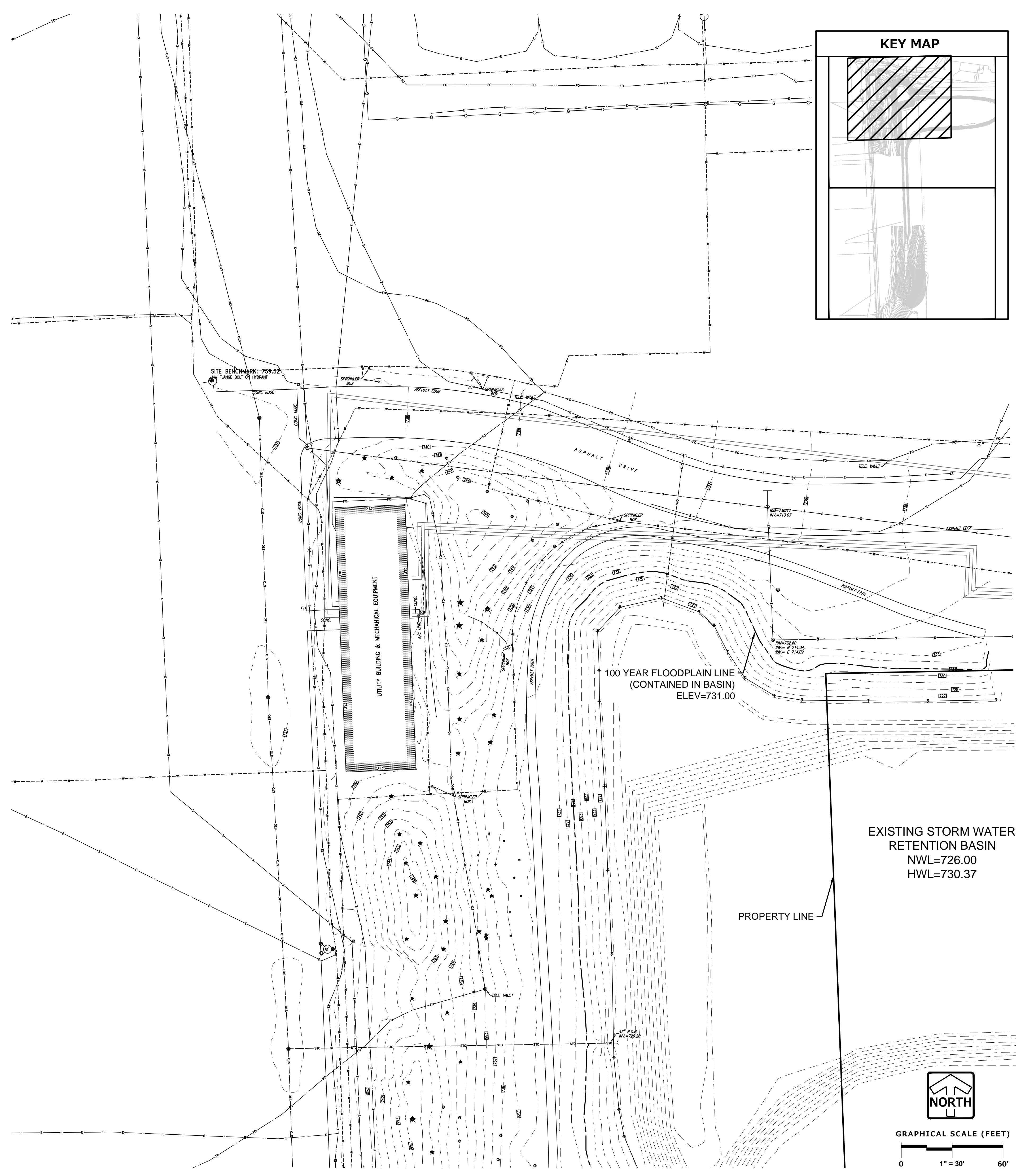
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REG. JOB No. 119,008
REG. No. AEA
START DATE 05/06/15
SCALE

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FOR REVIEW
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 DRAFTER: MEA
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EXISTING CONDITIONS SURVEY

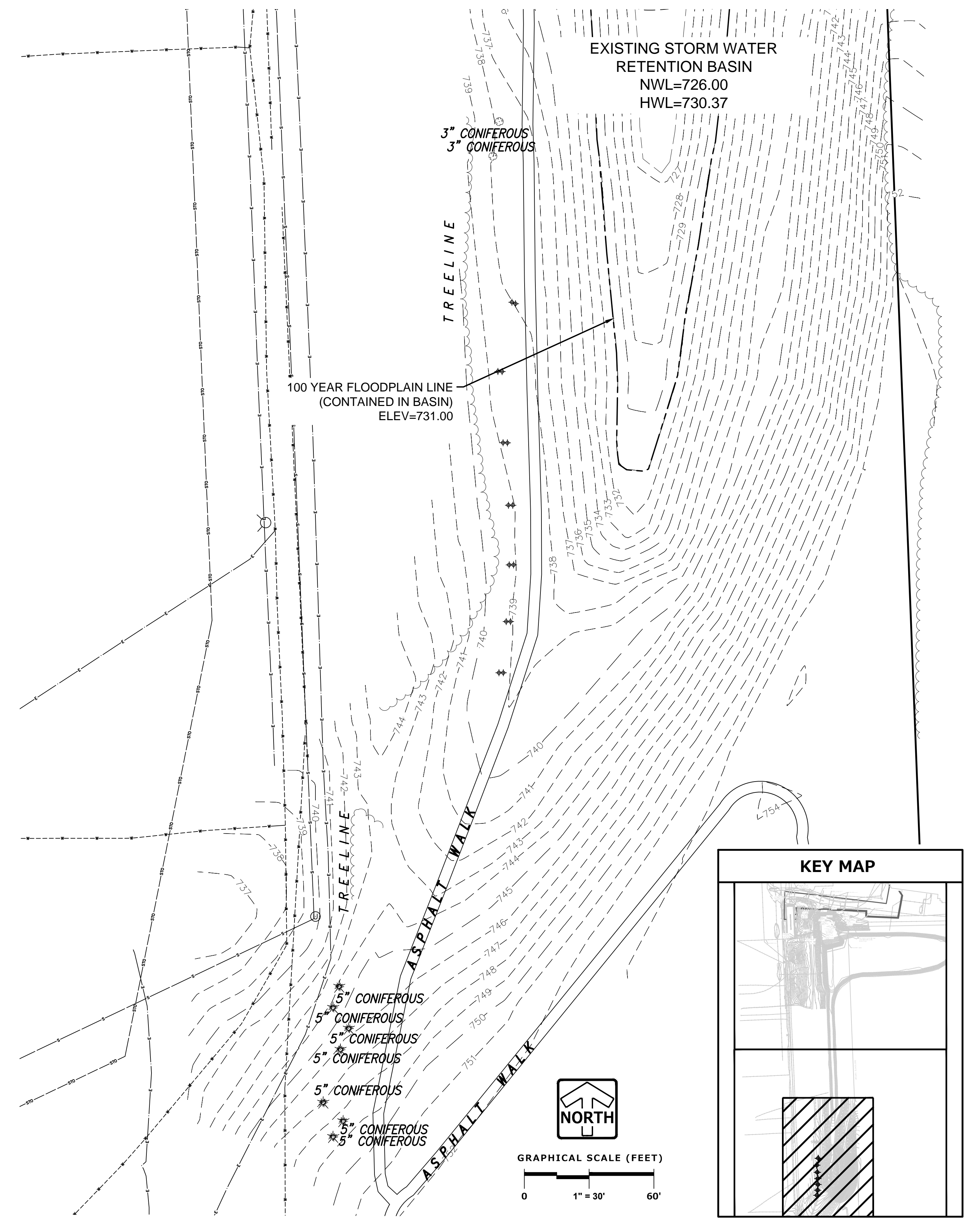
EXISTING CONDITIONS SURVEY PROVIDED BY CHARUT LAND SURVEYS IN 2015. ALTHOUGH PINNACLE ENGINEERING GROUP HAS NO REASON TO BELIEVE THE SURVEY IS INACCURATE, REG MAKES NO WARRANTIES ON EXISTING INFORMATION CONTAINED WITHIN THESE PLANS IS ALL-INCLUSIVE OR ACCURATE. CONTRACTOR SHALL UNDERTAKE NECESSARY EFFORTS TO VERIFY THE EXISTING CONDITIONS PRIOR TO THE START OF MATERIAL PROCUREMENT AND CONSTRUCTION EFFORTS/ACTIVITIES.

CONTRACTOR RESPONSIBILITY:

"THE INFORMATION SHOWN ON THIS DRAWING CONCERNING TYPE AND LOCATION OF UNDERGROUND UTILITIES IS NOT GUARANTEED TO BE ACCURATE OR ALL-INCLUSIVE. THE CONTRACTOR IS RESPONSIBLE FOR MAKING HIS OWN DETERMINATIONS AS TO THE TYPE AND LOCATION OF UNDERGROUND UTILITIES AS MAY BE NECESSARY TO AVOID DAMAGE THERETO. IF ADDITIONAL UTILITIES ARE KNOWN TO EXIST IN THE PROPERTY, THE OWNER WILL PROVIDE EXISTING PLANS OF OTHER UTILITIES SERVING THE SITE AND THE BUILDING THAT OTHERWISE CANNOT BE LOCATED BY A VISUAL OBSERVATION OF THE PROPERTY OR OF WHICH THE SURVEYOR WOULD HAVE NO KNOWLEDGE."

LEGEND

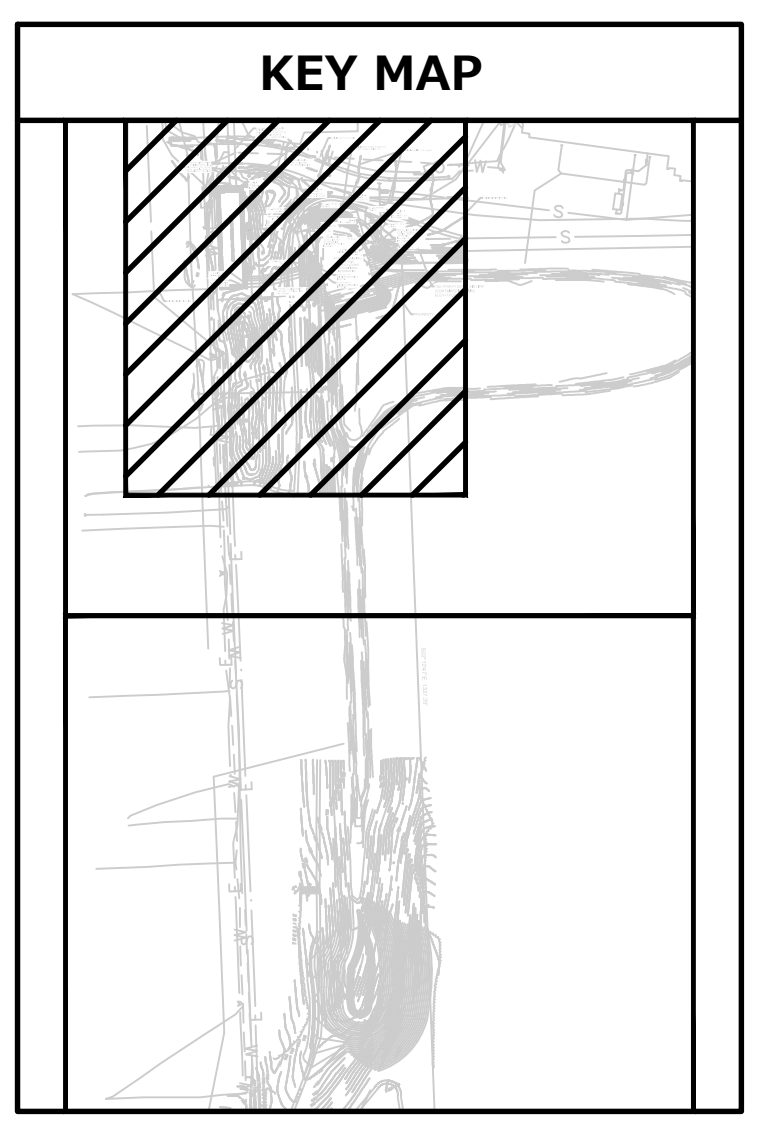
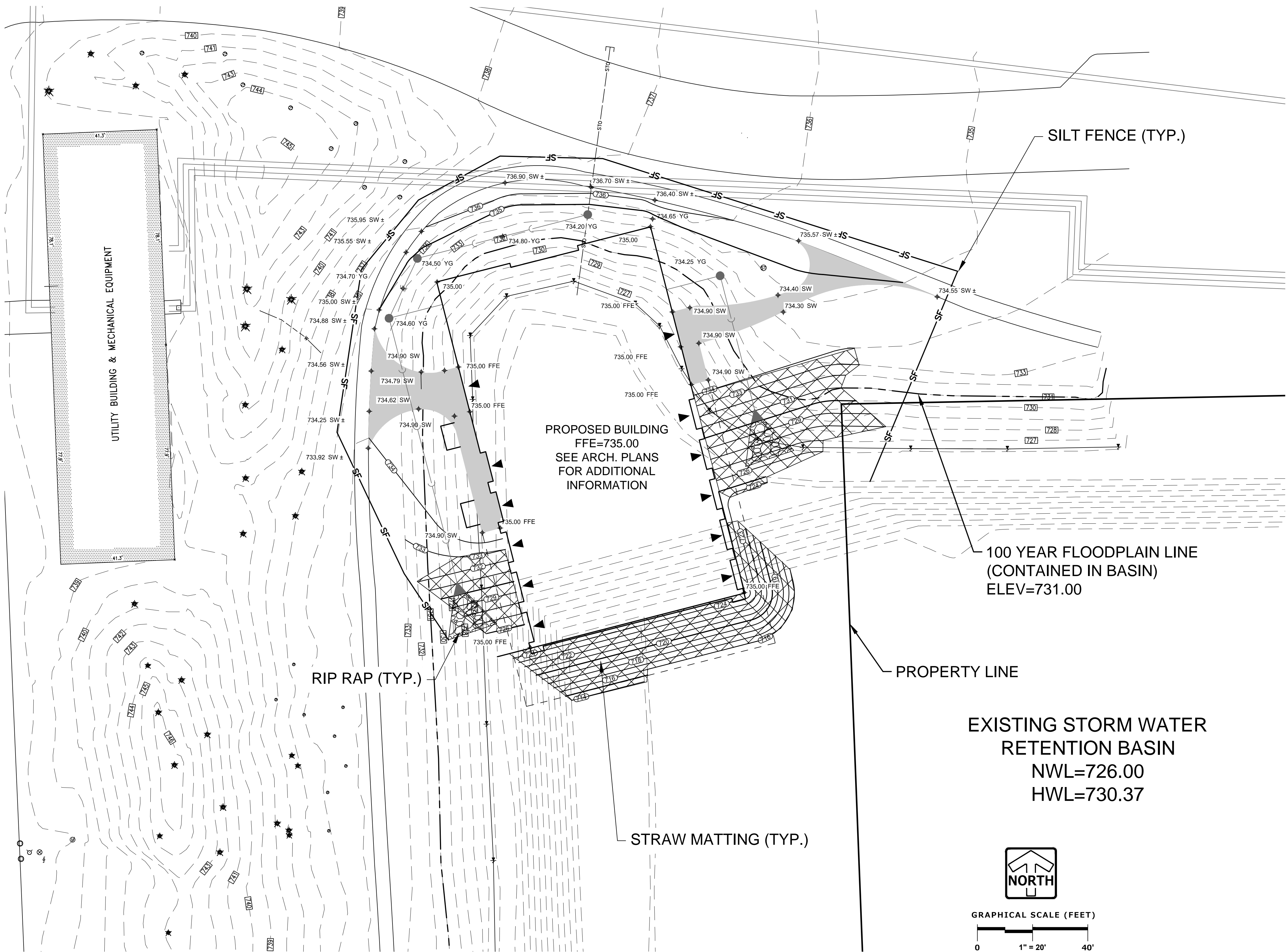
SANITARY MANHOLE	⊙	LIGHTING	⚡
STORM MANHOLE	⊗	ELECTRICAL CABLE	— — — — — — — — — —
CATCH BASIN	⊖	ELECTRICAL TRANSFORMER OR PEDESTAL	⊠
INLET	□	POWER POLE	⊙— — — — — — — — — —
PRECAST FLARED END SECTION	△	POWER POLE WITH LIGHT	⊙— — — — — — — — — —
CONCRETE HEADWALL	∩	GUY WIRE	— — — — — — — — — —
VALVE VAULT	⊗	STREET SIGN	⊙
VALVE BOX	⊙	GAS MAIN	— — — — — — — — — —
FIRE HYDRANT	⊙	TELEPHONE LINE	— — — — — — — — — —
BUFFALO BOX	⊙	CONTOUR	~
CLEANOUT	⊖	OVERFLOW RELIEF ROUTING	⚡
SANITARY SEWER	— — — — — — — — — —	TREE TRUNKS	⊙
FORCE MAIN	— — — — — — — — — —	CONCRETE SIDEWALK	— — — — — — — — — —
STORM SEWER	— — — — — — — — — —	EASEMENT LINE	— — — — — — — — — —
WATER MAIN	— — — — — — — — — —		



REVISIONS

NO.	DESCRIPTION	DATE
1	WDRS NOI SUBMITTAL	05-04-15

DESIGNED: ABA
DRAWN: ABA
CHECKED: ABA
REVIEWED: ABA
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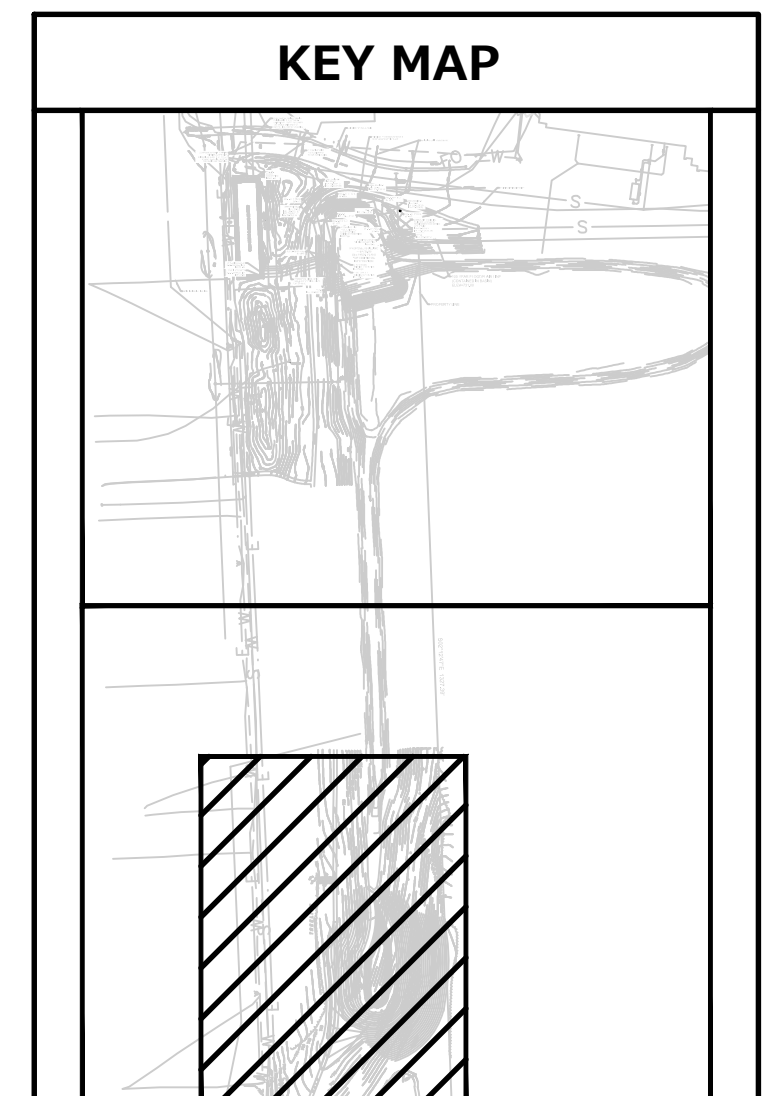
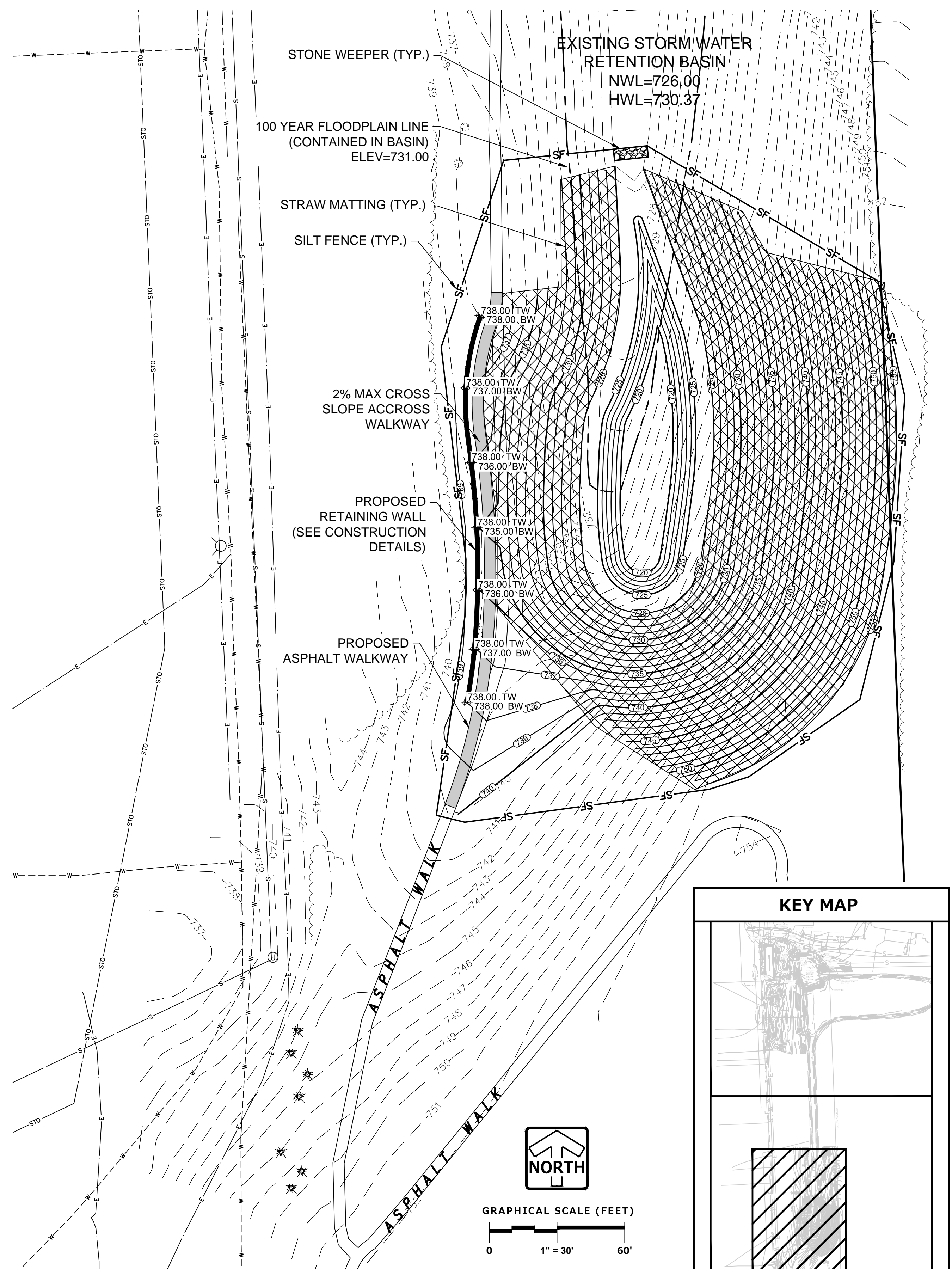


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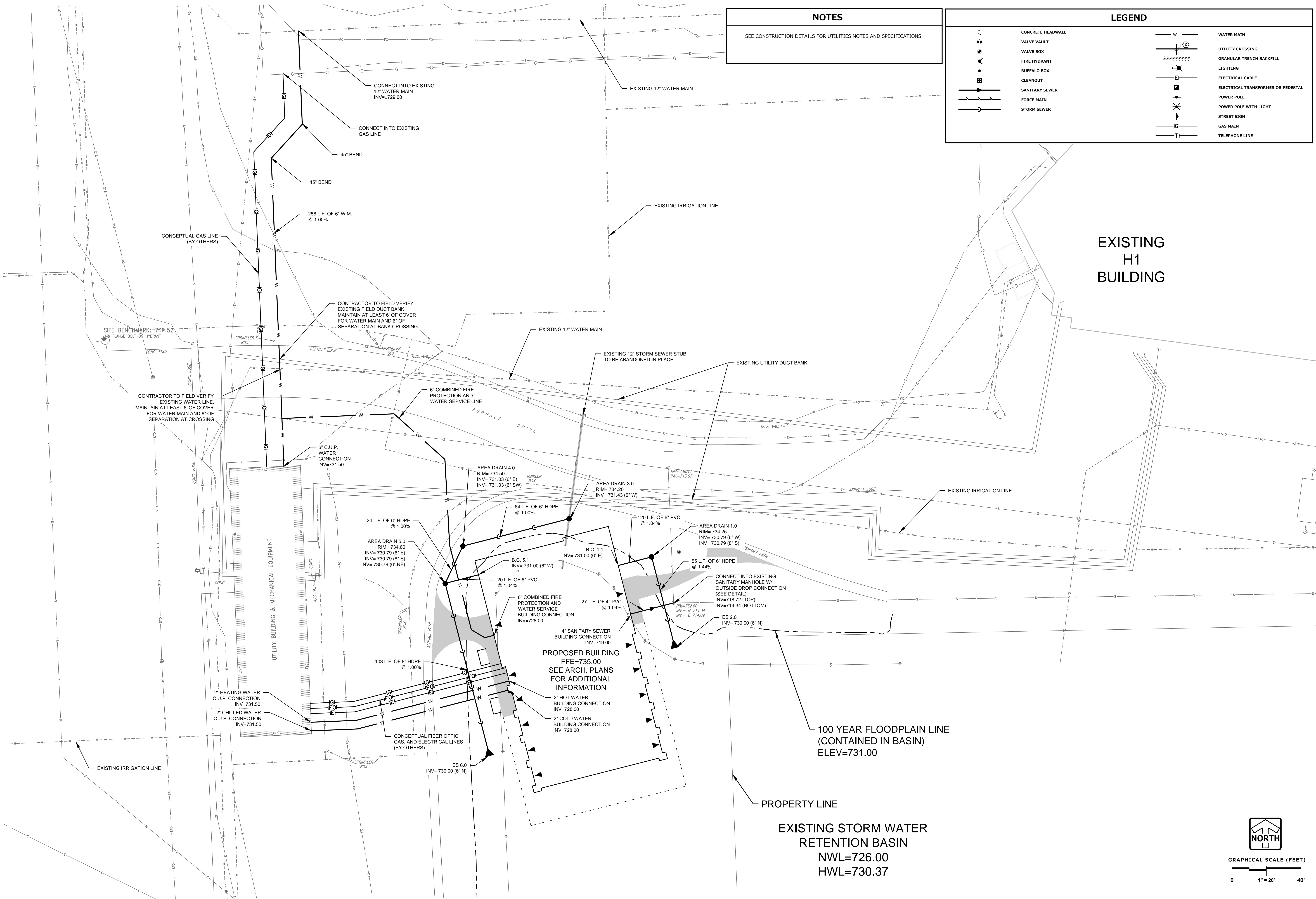
SEE CONSTRUCTION DETAILS FOR PAVING, GRADING, EROSION CONTROL NOTES AND SPECIFICATIONS.

LEGEND

CONTOUR		749	FLOODPLAIN	
SPOT ELEVATION		x 750.00	HIGH WATER LEVEL (HWL)	
WETLANDS			NORMAL WATER LEVEL (NWL)	
FLOODWAY			DIRECTION OF SURFACE FLOW	
MANDOOR			DITCH OR SWALE	
			DIVERSION SWALE	
			OVERFLOW RELIEF ROUTING	
ASPHALT PATH			FENCE LINE, TEMPORARY SILT	
			EROSION CONTROL BLANKET	
			(NORTH AMERICAN GREEN S75 OR EQUAL)	



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NOTES
SEE CONSTRUCTION DETAILS FOR UTILITIES NOTES AND SPECIFICATIONS.

LEGEND	
	CONCRETE HEADWALL
	VALVE VAULT
	FIRE HYDRANT
	BUFFALO BOX
	CLEANOUT
	SANITARY SEWER
	FORCE MAIN
	STORM SEWER
	WATER MAIN
	UTILITY CROSSING
	GRANULAR TRENCH BACKFILL
	LIGHTING
	ELECTRICAL CABLE
	ELECTRICAL TRANSFORMER OR PEDESTAL
	POWER POLE
	POWER POLE WITH LIGHT
	STREET SIGN
	GAS MAIN
	TELEPHONE LINE

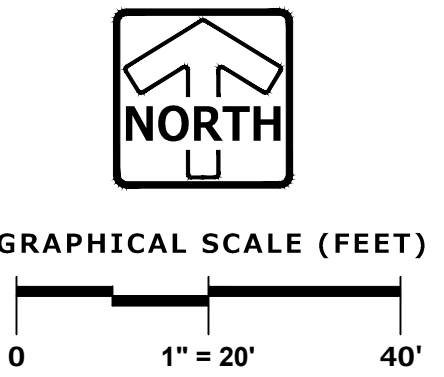
EXISTING H1 BUILDING

PROPOSED BUILDING
FFE=735.00
SEE ARCH. PLANS FOR ADDITIONAL INFORMATION

100 YEAR FLOODPLAIN LINE
(CONTAINED IN BASIN)
ELEV=731.00

PROPERTY LINE

EXISTING STORM WATER RETENTION BASIN
NWL=726.00
HWL=730.37



REVISIONS	
1	WDRS NOI SUBMITTAL 05-04-15

REG. JOB No. 119.00B
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 REG. No. 119.00B
 START DATE 05/06/15
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 FOR REVIEW
 UTILITY PLAN

GENERAL SPECIFICATIONS FOR CONSTRUCTION ACTIVITIES

1. THE PROPOSED IMPROVEMENTS SHALL BE CONSTRUCTED ACCORDING TO THE WISCONSIN D.O.T. STANDARD SPECIFICATIONS FOR HIGHWAY AND STRUCTURE CONSTRUCTION, LATEST EDITION, THE STANDARD SPECIFICATIONS FOR SEWER & WATER IN WISCONSIN, AND WISCONSIN ADMINISTRATIVE CODE, §§ 360.03(3) AND 360.03(4).
2. THE CONTRACTOR SHALL OBTAIN ALL PERMITS REQUIRED FOR EXECUTION OF THE WORK. THE CONTRACTOR SHALL CONDUCT HIS WORK ACCORDING TO THE REQUIREMENTS OF THE PERMITS.
3. THE CONTRACTOR SHALL NOTIFY THE OWNER AND THE MUNICIPALITY FORTY-EIGHT (48) HOURS PRIOR TO THE START OF CONSTRUCTION.
4. THE MUNICIPALITY SHALL HAVE THE RIGHT TO INSPECT, APPROVE, AND REJECT THE CONSTRUCTION OF THE PUBLIC PORTIONS OF THE WORK. THE OWNER SHALL HAVE THE RIGHT TO INSPECT, APPROVE, AND REJECT THE CONSTRUCTION OF ALL PRIVATE PORTIONS OF THE WORK.
5. THE CONTRACTOR SHALL INDEMNIFY THE OWNER, THE ENGINEER, AND THE MUNICIPALITY, THEIR AGENTS, ETC. FROM ALL LIABILITY INVOLVED WITH THE CONSTRUCTION, INSTALLATION, AND TESTING OF THE WORK ON THIS PROJECT.
6. THE CONTRACTOR SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
7. THE CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFYING ALL UTILITY INFORMATION SHOWN ON THE PLANS PRIOR TO THE START OF CONSTRUCTION. THE CONTRACTOR SHALL CALL DIGGER'S HOTLINE AT 1-800-242-8511 TO NOTIFY THE UTILITIES OF HIS INTENTIONS, AND TO REQUEST FIELD STAKING OF EXISTING UTILITIES.
8. SILT FENCE AND OTHER EROSION CONTROL FACILITIES MUST BE INSTALLED PRIOR TO CONSTRUCTION OR ANY OTHER LAND DISTURBING ACTIVITY. FOLLOW THE SEQUENCE OF CONSTRUCTION ON THE EROSION CONTROL PLAN FOR MORE DETAILS. INSPECTIONS SHALL BE MADE WEEKLY OR AFTER EVERY RAINFALL OF 0.2" OR MORE. REPAIRS SHALL BE MADE IMMEDIATELY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING ALL EROSION CONTROL FACILITIES ONCE THE THREAT OF EROSION HAS PASSED WITH THE APPROVAL OF THE GOVERNING AGENCY.
9. ANY ADJACENT PROPERTIES OR ROAD RIGHT-OF-WAYS WHICH ARE DAMAGED DURING CONSTRUCTION MUST BE RESTORED BY THE CONTRACTOR.
10. TRASH AND DEBRIS SHALL BE NOT BE ALLOWED TO ACCUMULATE ON THIS SITE AND THE SITE SHALL BE CLEAN UPON COMPLETION OF WORK.
11. THE OWNER SHALL HAVE THE RIGHT TO HAVE ALL MATERIALS USED IN CONSTRUCTION TESTED FOR COMPLIANCE WITH THESE SPECIFICATIONS.

SPECIFICATIONS FOR GRADING & EROSION CONTROL

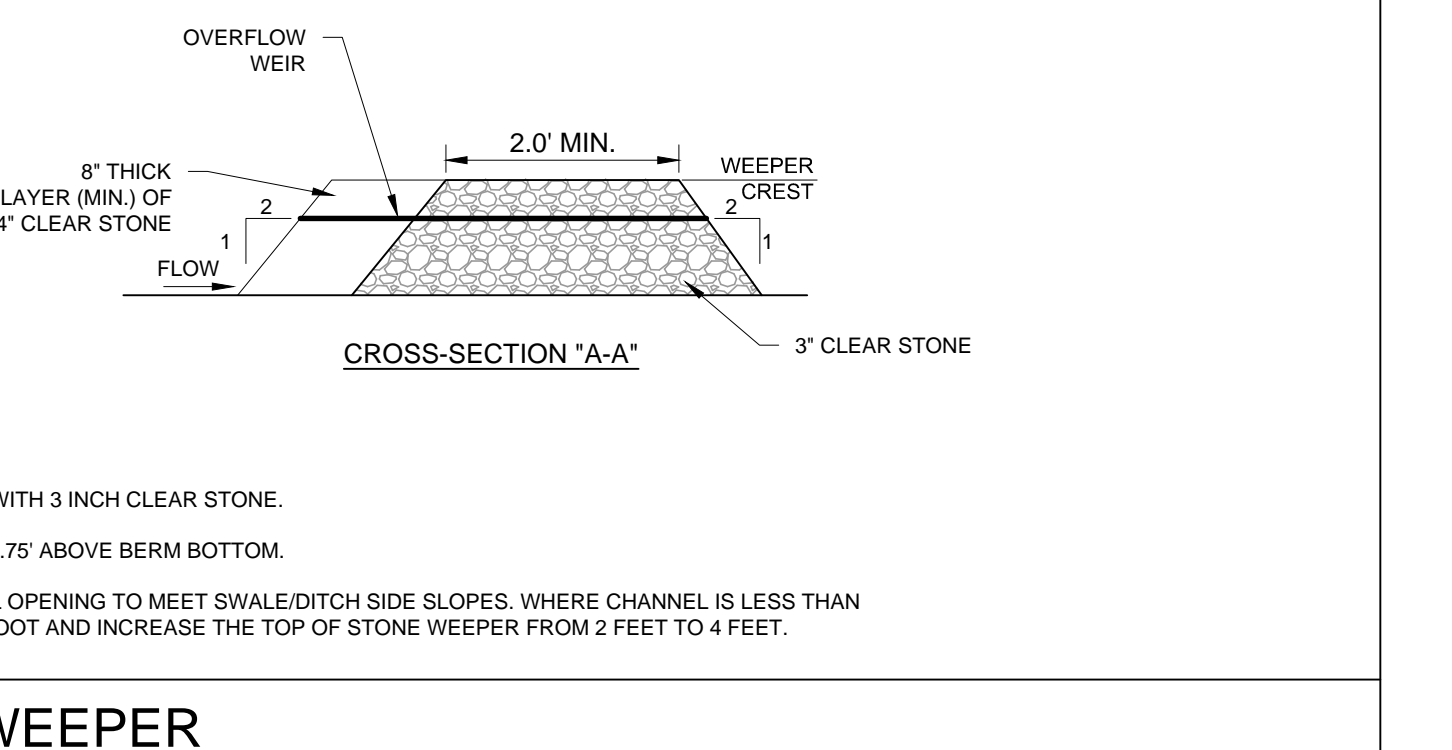
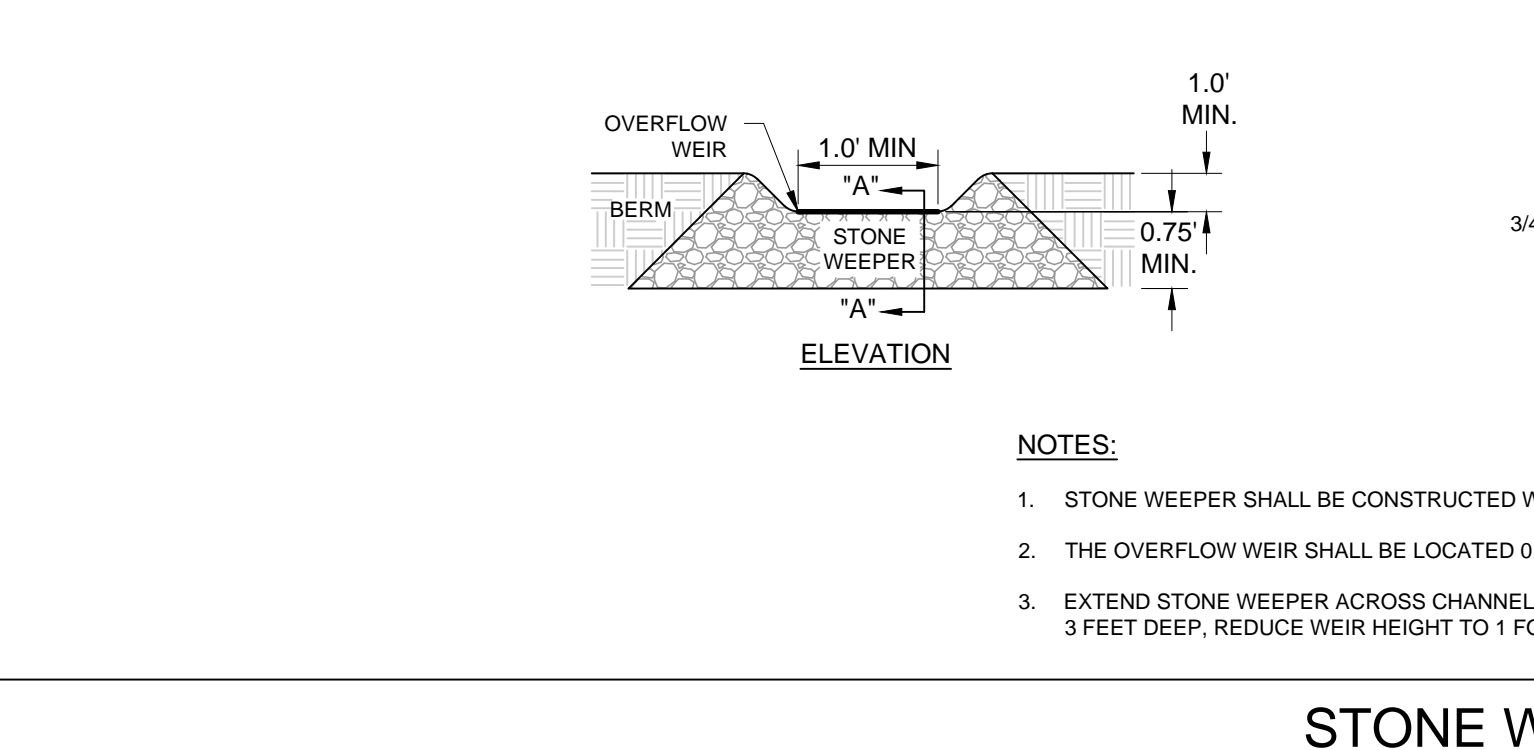
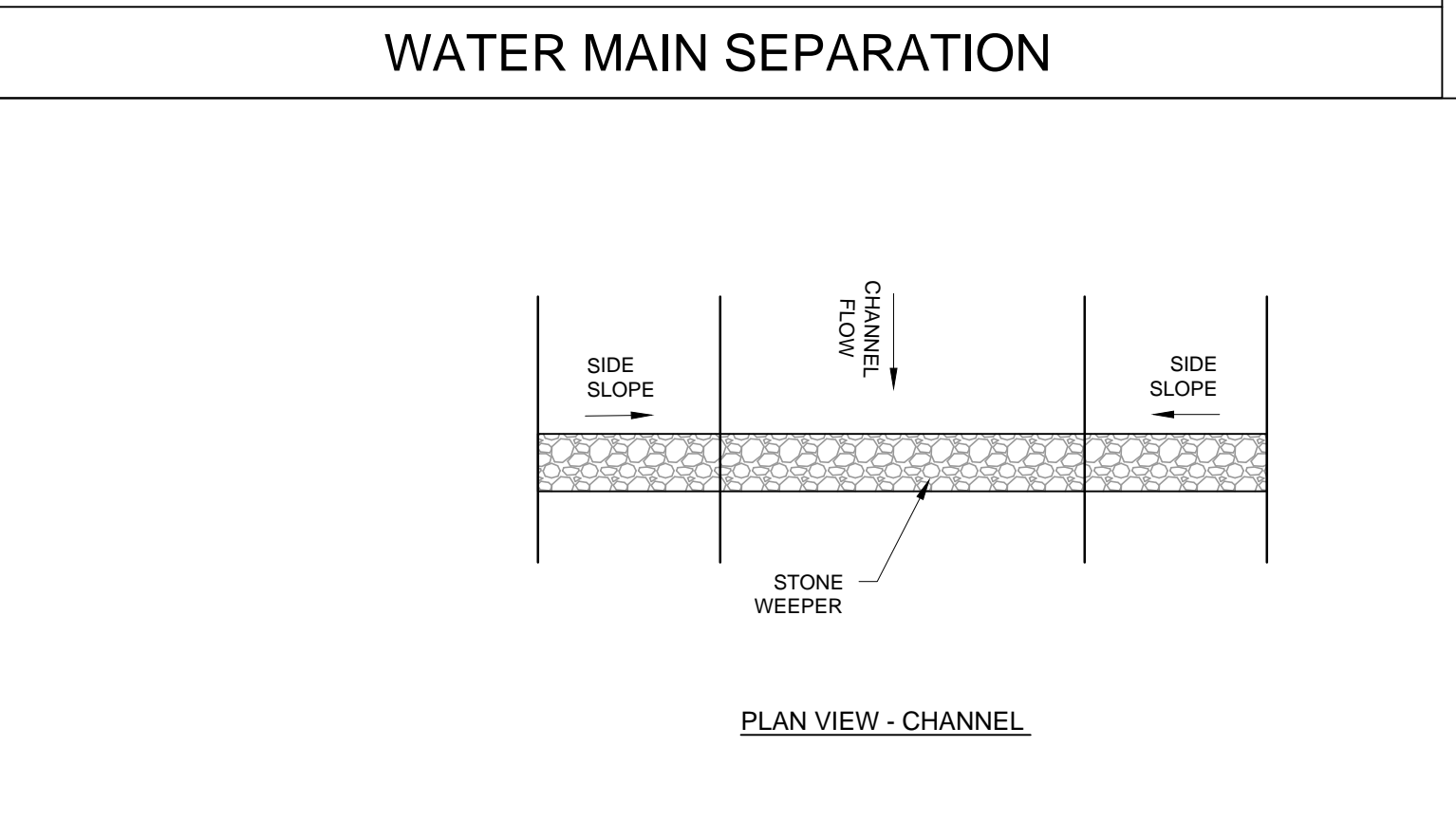
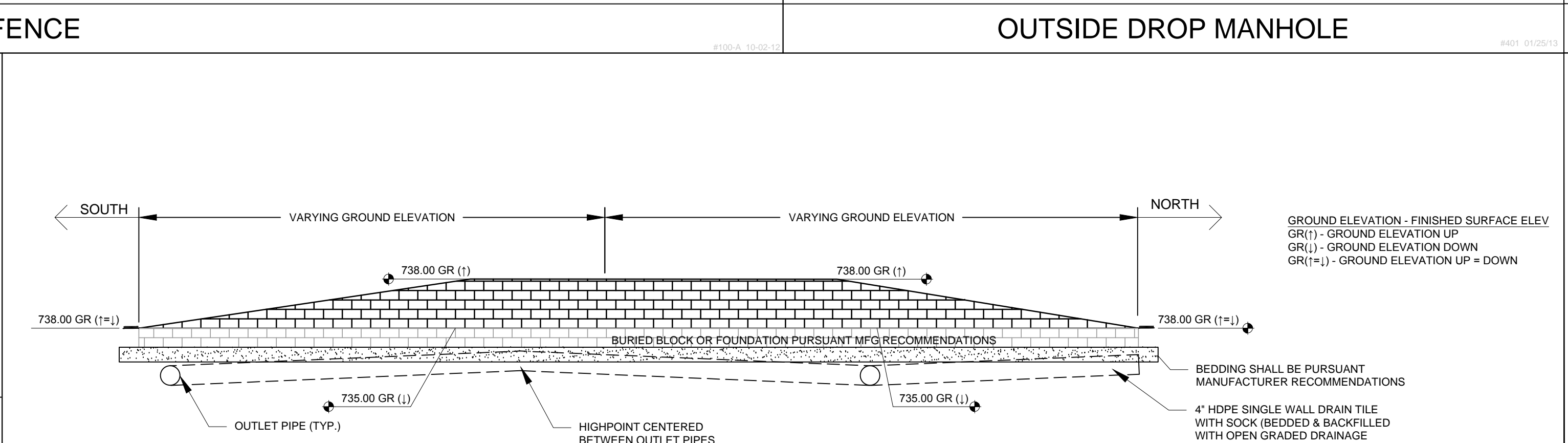
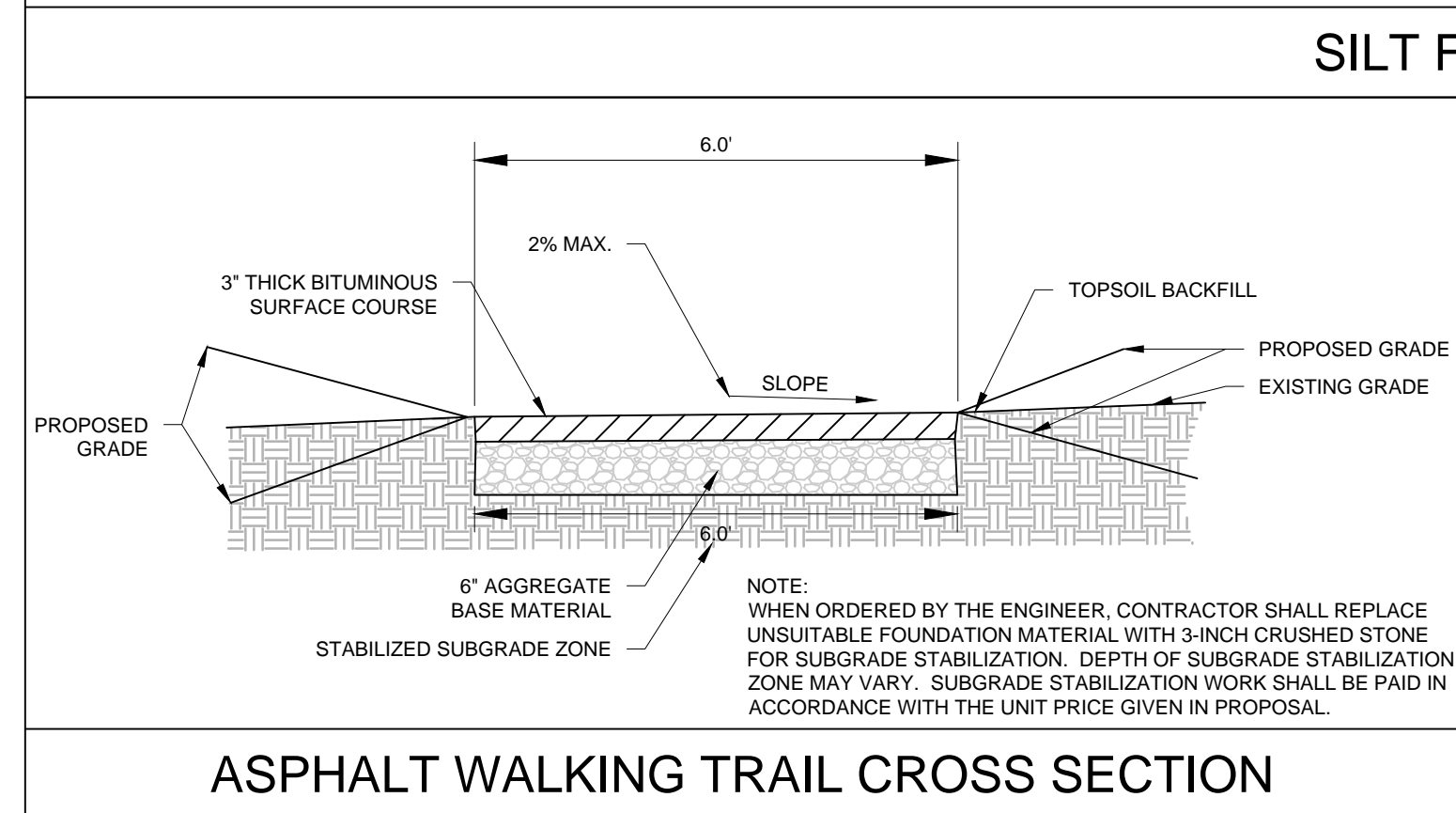
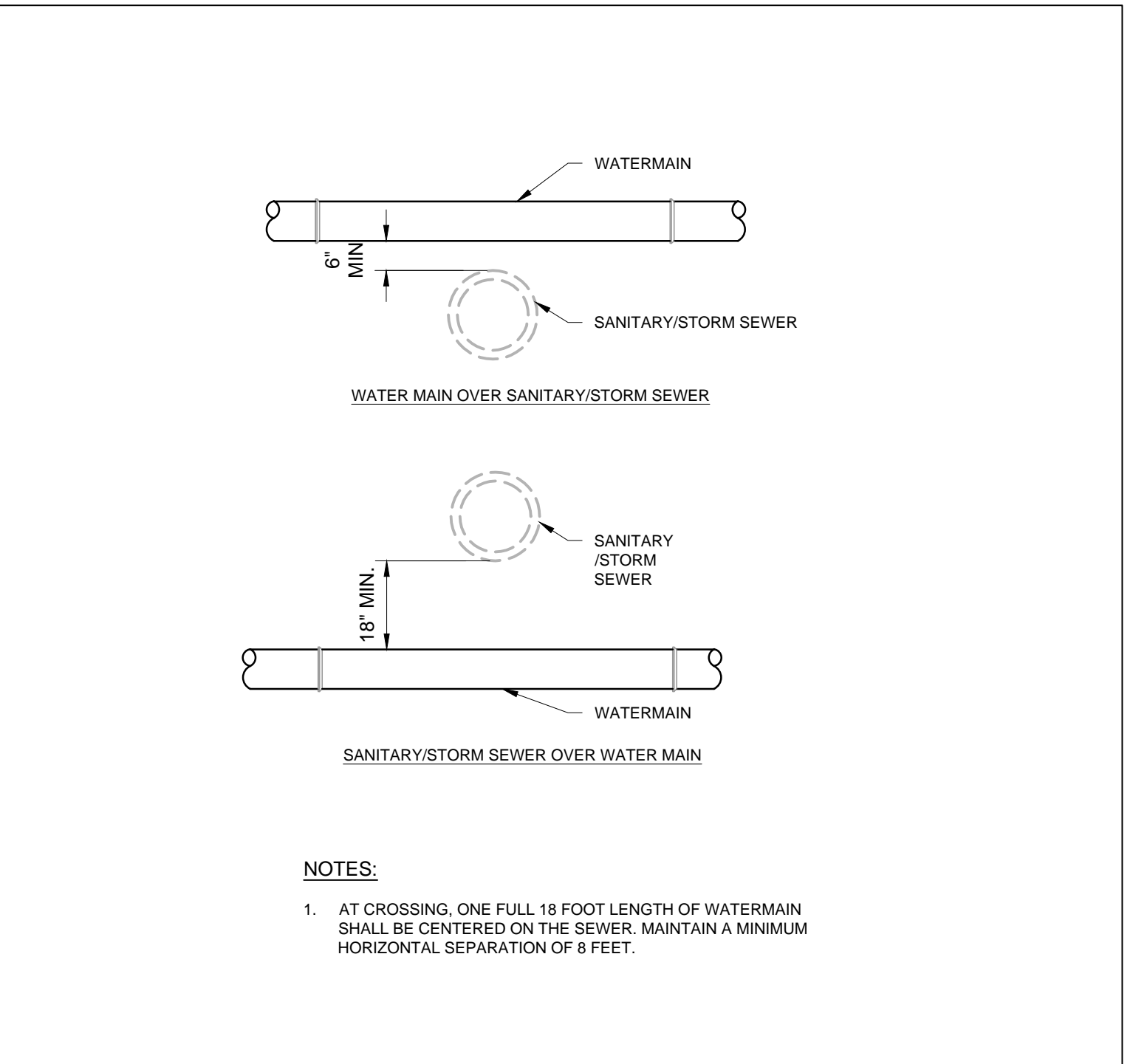
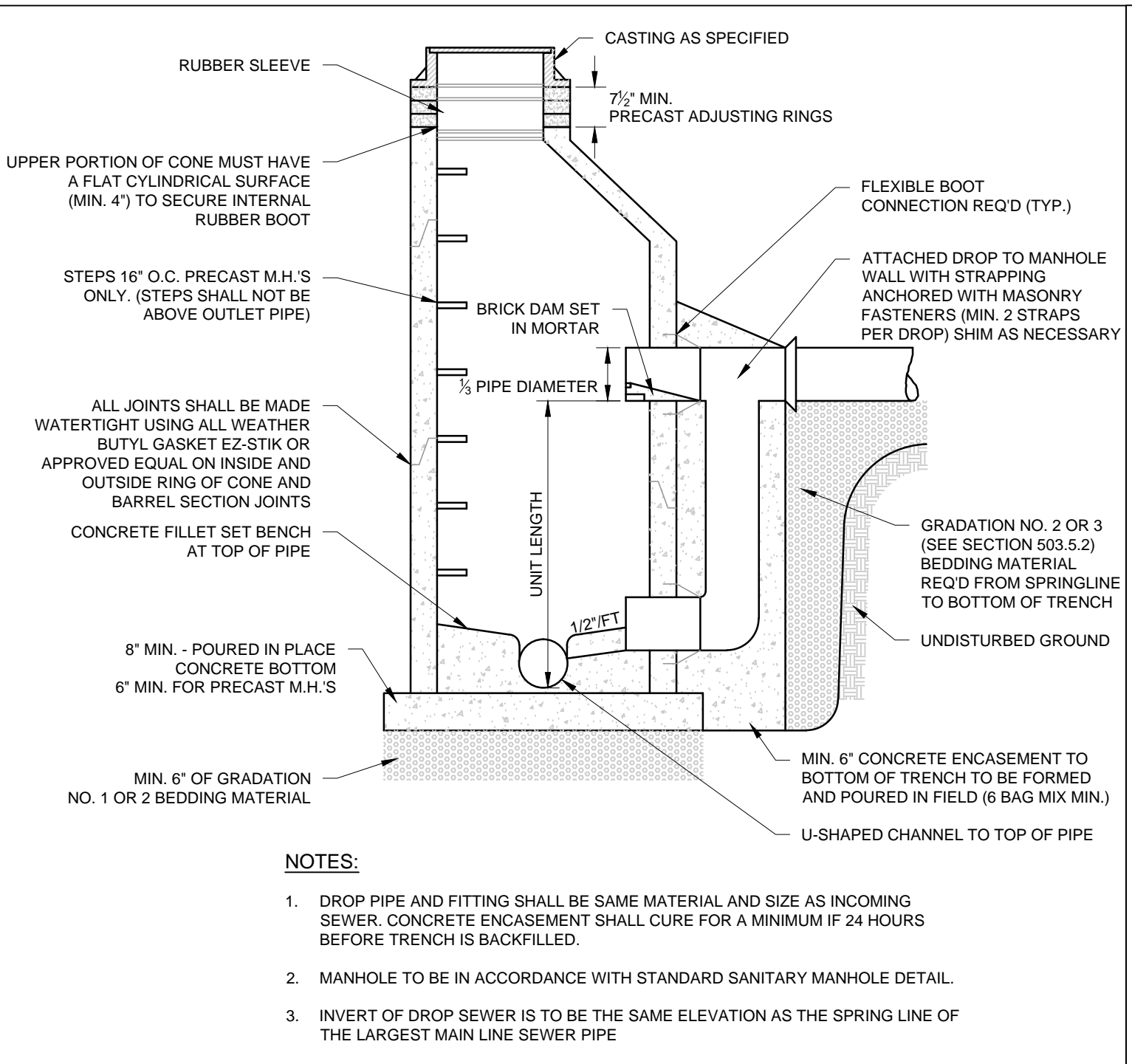
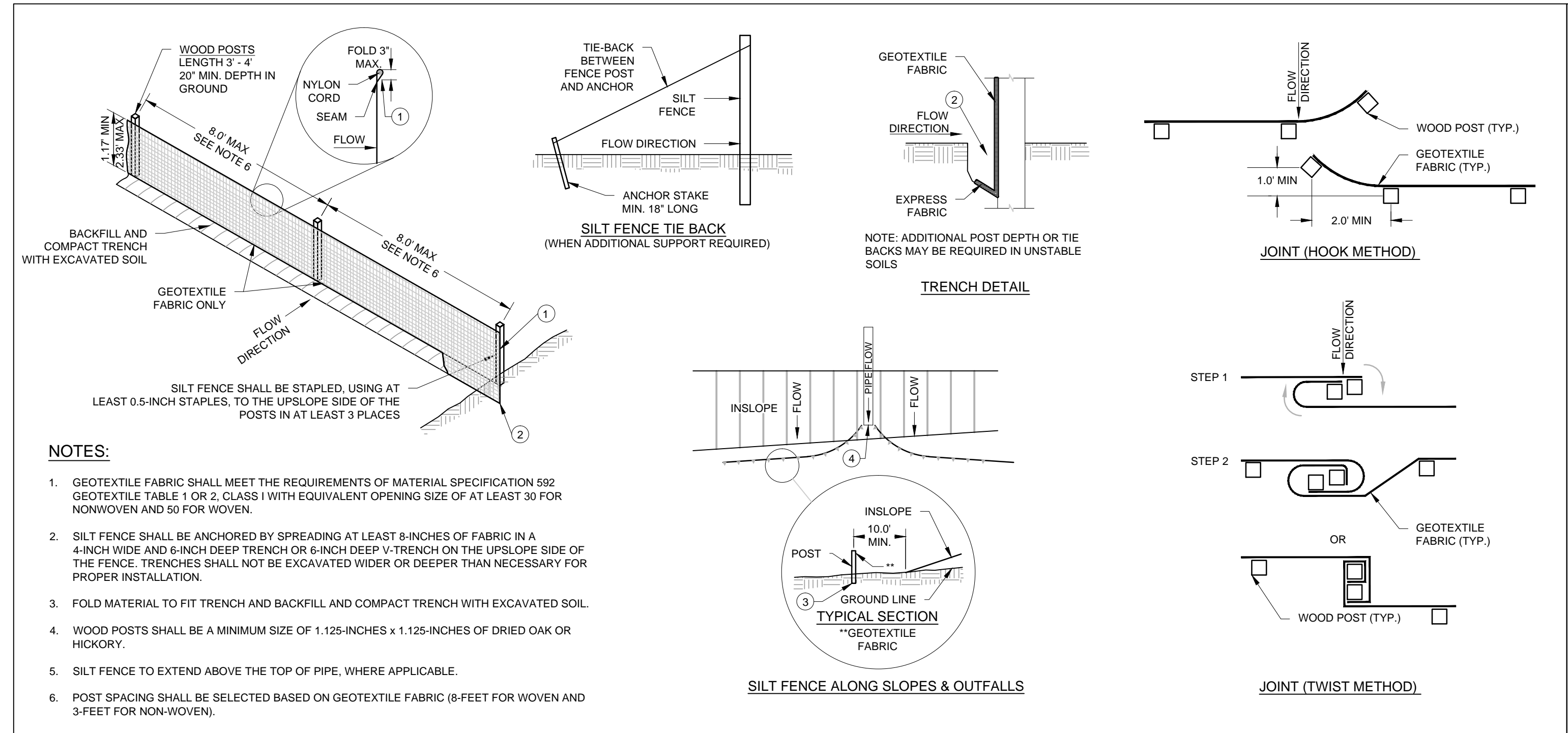
1. THE CONTRACTOR SHALL ASSUME SOLE RESPONSIBILITY FOR THE COMPUTATIONS OF ALL GRADING AND FOR ACTUAL LAND BALANCE, INCLUDING UTILITY TRENCH SPOIL. THE CONTRACTOR SHALL IMPORT OR EXPORT MATERIAL AS NECESSARY TO COMPLETE THE PROJECT. CONTRACTOR SHALL NOTIFY OWNER OF THE NEED TO IMPORT OR Haul OFF SOIL. ON-SITE LOCATIONS SUITABLE FOR BORROW OR FILL MAY BE PRESENT. COORDINATE WITH OWNER.
2. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING SOIL CONDITIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION. A GEOTECHNICAL REPORT MAY BE AVAILABLE FROM THE OWNER.
3. SITE SHALL BE CLEARED TO THE LIMITS SHOWN ON THE PLANS. REMOVE VEGETATION FROM THE SITE. BURNING IS NOT PERMITTED. PROTECT TREES AND OTHER FEATURES FROM DAMAGE WITH FENCING. STOCKPILES SHALL NOT BE LOCATED CLOSER THAN 25' TO A DRAINAGE STRUCTURE OR FEATURE AND SHALL BE SURROUNDED WITH SILT FENCE.
4. THE GEOTECHNICAL ENGINEER IS RESPONSIBLE FOR VERIFYING COMPACTION AND FILL PLACEMENT IN THE FIELD. THE GEOTECHNICAL ENGINEER MAY SUPERCEDE THESE SPECIFICATIONS IF THERE IS GOOD CAUSE TO DO SO. AN EXPLANATION MUST BE SUBMITTED TO THE ENGINEER IN WRITING BEFORE ANY DEVIATIONS ARE MADE.
5. IF NO GEOTECHNICAL RECOMMENDATION IS AVAILABLE, THEN THE FOLLOWING SPECIFICATIONS SHALL APPLY. ALL FILL SHALL BE CONSIDERED STRUCTURAL FILL AND SHALL BE PLACED IN ACCORDANCE WITH THE FOLLOWING: THE COMPACTED FILL SUBGRADE SHALL CONSIST OF AND SHALL BE UNDERLAIN BY SUITABLE BEARING MATERIALS, FREE OF ALL ORGANIC, FROZEN OR OTHER DELETERIOUS MATERIAL AND INSPECTED AND APPROVED BY THE RESIDENT GEOTECHNICAL ENGINEER. PREPARATION OF THE SUBGRADE AFTER STRIPPING, SHALL CONSIST OF PROOF-ROLLING TO DETECT UNSTABLE AREAS THAT MIGHT BE UNDERCUT, AND COMPACTING THE SCARIFIED SURFACE TO THE SAME MINIMUM DENSITY INDICATED BELOW. THE COMPACTED FILL MATERIALS SHALL BE FREE OF ANY DELETERIOUS, ORGANIC OR FROZEN MATTER AND SHALL HAVE A MAXIMUM LIQUID LIMIT (ASTM D-293) AND PLASTICITY INDEX (ASTM D-294) OF 30 AND 10 RESPECTIVELY. UNLESS SPECIFICALLY TESTED AND FOUND TO HAVE LOW EXPANSIVE PROPERTIES AND APPROVED BY AN EXPERIENCED SOILS ENGINEER, THE TOP TWELVE (12") INCHES OF COMPACTED FILL SHOULD HAVE A MAXIMUM THREE (3") INCH PARTICLE DIAMETER AND ALL UNDERLYING COMPACTED FILL A MAXIMUM SIX (6") INCH PARTICLE DIAMETER UNLESS SPECIFICALLY APPROVED BY AN EXPERIENCED SOILS ENGINEER. ALL FILL MATERIAL MUST BE TESTED AND APPROVED UNDER THE DIRECTION AND SUPERVISION OF AN EXPERIENCED SOILS ENGINEER PRIOR TO PLACEMENT. IF THE FILL IS TO PROVIDE NON-FROST SUSCEPTIBLE CHARACTERISTICS, IT MUST BE CLASSIFIED AS A CLEAN OW, GP, SW, OR SP PER UNITED SOIL CLASSIFICATION SYSTEM (ASTM D-2487). FOR STRUCTURAL FILL THE DENSITY OF THE STRUCTURAL COMPACTED FILL AND SCARIFIED SUBGRADE AND GRADES SHALL NOT BE LESS THAN 95 PERCENT OF THE MAXIMUM DRY DENSITY AS DETERMINED BY THE STANDARD PROCTOR (ASTM D-998) WITH THE EXCEPTION TO THE TOP 12 INCHES OF PAVEMENT SUBGRADE WHICH SHALL A MINIMUM IN-SITU DENSITY OF 100 PERCENT OF THE MAXIMUM DRY DENSITY, OR 5 PERCENT HIGHER THAN UNDERLYING FILL MATERIALS. THE MOISTURE CONTENT OF COHESIVE SOIL SHALL NOT VARY BY MORE THAN 1 TO +3 PERCENT AND GRANULAR SOIL 3 PERCENT OF OPTIMUM WHEN PLACED AND COMPACTED OR RECOMPACTED UNLESS SPECIFICALLY APPROVED BY THE SOILS ENGINEER TAKING INTO CONSIDERATION THE TYPE OF MATERIALS AND COMPACTION EQUIPMENT BEING USED. THE COMPACTION EQUIPMENT SHOULD CONSIST OF SUITABLE MECHANICAL EQUIPMENT SPECIFICALLY DESIGNED FOR SOIL COMPACTION. BULLDOZERS OR SIMILAR TRACKED VEHICLES ARE TYPICALLY NOT SUITABLE FOR COMPACTION. MATERIAL THAT IS TOO WET TO PERMIT PROPER COMPACTION MAY BE SPREAD ON THE FILL AND PERMITTED TO DRY, DISCING, HARROWING OR PULVERIZING MAY BE NECESSARY TO REDUCE THE MOISTURE CONTENT TO A SATISFACTORY VALUE. AFTER WHICH IT SHALL BE COMPACTED. THE FINISHED SUBGRADE AREAS OF THE SITE SHALL BE COMPACTED TO 100 PERCENT OF THE STANDARD PROCTOR (ASTM D-998) MAXIMUM DENSITY.
6. NO FILL SHALL BE PLACED ON A WET OR SOFT SUBGRADE. THE SUBGRADE SHALL BE PROOF-ROLLED AND INSPECTED BY THE GEOTECHNICAL ENGINEER BEFORE ANY MATERIAL IS PLACED.
7. SUBGRADE TOLERANCES ARE +/- 1" FOR LANDSCAPE AREAS AND +/- 1/2" FOR ALL PAVEMENT AND BUILDING AREAS.
8. TOPSOIL SHALL BE FREE OF DELETERIOUS MATERIALS, ROOTS, OLD VEGETATION, ROCKS OVER 2" DIAMETER AND SHALL NOT BE EXCESSIVELY CLAYEY IN NATURE. NO CLUMPS LARGER THAN 4" ARE ACCEPTABLE. TOPSOIL MAY BE AMENDED AS NEEDED WITH SAND OR COMPOST TO BE LOOSE WHEN SPREAD.
9. THE CONTRACTOR SHALL MAINTAIN SITE DRAINAGE THROUGHOUT CONSTRUCTION. THIS MAY INCLUDE THE EXCAVATION OF TEMPORARY DITCHES OR PUMPING TO ALLEVIATE WATER PONDING. ANY DEWATERING SHALL NOT GO DIRECTLY TO STREAMS, CREEKS, WETLANDS OR OTHER ENVIRONMENTALLY SENSITIVE AREAS WITHOUT BEING TREATED FIRST. A DIRT BAG OR OTHER DEWATERING TREATMENT DEVICE MAY BE USED TO CAPTURE SEDIMENT FROM THE PUMPED WATER.
10. CONTRACTOR IS ADVISED THAT ALL MUD AND DEBRIS MUST NOT BE DEPOSITED ONTO THE ADJACENT ROADWAYS PER THE REQUIREMENT OF THE MUNICIPALITY OR OTHER APPROPRIATE GOVERNMENT AGENCIES. IN THE EVENT THIS OCCURS, THE ROADWAYS SHALL BE POWER SWEEP IMMEDIATELY AND ALL SEDIMENT REMOVED FROM DOWNSTREAM FACILITIES.

SPECIFICATIONS FOR PRIVATE UTILITIES

1. BEFORE PROCEEDING WITH ANY UTILITY CONSTRUCTION, THE CONTRACTOR SHALL EXCAVATE EACH EXISTING LATERAL OR POINT OF CONNECTION AND VERIFY THE LOCATION AND ELEVATION OF ALL UTILITIES. IF ANY EXISTING UTILITIES ARE NOT AS SHOWN ON THE DRAWINGS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER IMMEDIATELY FOR POSSIBLE REDESIGN.
2. ALL CONNECTIONS TO EXISTING PIPES AND MANHOLES SHALL BE CORED CONNECTIONS. CONNECTIONS TO WATERMAIN SHALL BE WET TAPED WITH A STAINLESS STEEL TAPPING SLEEVE.
3. PROPOSED SANITARY SEWER AND INTERNALLY CONNECTED STORM SEWER SHOWN ON THIS PLAN SHALL TERMINATE AT A POINT FIVE (5) FEET FROM THE EXTERIOR BUILDING WALL. THE EXACT LOCATION OF ALL DOWN SPOUTS CONNECTIONS SHALL BE PER THE ARCHITECTURAL PLANS.
4. CONTRACTOR SHALL NOT SHUT OFF WATER OR PLUG SANITARY SEWER IN MUNICIPAL LINES WITHOUT PRIOR APPROVAL.
5. MATERIALS FOR STORM SEWER SHALL BE AS FOLLOWS: STORM SEWER PIPE 48" OR LESS SHALL BE HIGH DENSITY POLYETHYLENE (HDPE) CORRUGATED PIPE WITH AN INTEGRALLY FORMED SMOOTH WATERWAY SUCH AS ADS N-12. FOR PIPE 10" OR LESS IN DIAMETER, PVC, ASTM D-3034, SDR-26, MAY ALSO BE USED. WHERE SPECIFICALLY REQUIRED, REINFORCED CONCRETE PIPE (RCP), ASTM C-76, CLASS III OR HIGHER, MAY BE USED. TRENCH SECTION SHALL BE CLASS "B" FOR PVC AND HDPE AND CLASS "C" FOR CONCRETE. MANHOLES, INLETS AND CATCH BASINS SHALL BE PRE CAST REINFORCED CONCRETE, ASTM C-478. CASTINGS SHALL BE HEAVY DUTY CAST IRON. AREA DRAINS SHALL BE ADS NYLOPLAST OR EQUIVALENT AND SHALL BE A MINIMUM OF 24" IN DIAMETER. CONNECTIONS TO EXISTING PIPES SHALL BE MADE WITH INSERT WYE OR EQUIVALENT.
6. MATERIALS FOR SANITARY SEWER SHALL BE AS FOLLOWS: SANITARY SEWER SHALL BE PVC, ASTM D-3034, SDR-35 WITH RUBBER GASKETED JOINTS, CONFORMING TO ASTM D-3212. TRENCH SECTIONS SHALL BE CLASS "B" BEDDING. CRUSHED STONE CHIPS SHALL BE USED FOR BEDDING MATERIAL. CONNECTIONS SHALL BE MADE WITH AN INSERT WYE OR EQUIVALENT. A MINIMUM OF 6" OF COVER IS REQUIRED FOR ALL SANITARY SEWER.
7. MATERIALS FOR WATER SERVICES AND PRIVATE HYDRANTS SHALL BE AS FOLLOWS: WATER SERVICES SHALL BE PVC, HDPE, OR DI AS ALLOWED BY MUNICIPAL CODE. PVC SHALL BE AWWA C-900. DI SHALL BE AWWA C151. CLASS 52 (OR AS REQUIRED BY LOCAL CODE). TRENCH SECTIONS SHALL BE CLASS "B" BEDDING. CRUSHED STONE CHIPS SHALL BE USED FOR BEDDING MATERIAL. CONNECTION SHALL BE MADE WITH A WET TAP. CORPORATE STOP AND VALVE BOX PER MUNICIPAL STANDARDS. A MINIMUM OF 6" COVER IS REQUIRED FOR ALL WATERMAIN. VALVES SHALL BE NONRISING STEM, RESILIENT SEATED GATE VALVES COMPLYING WITH AWWA C509 WITH A THREE PIECE CAST IRON VALVE BOX. INSTALL THRUST BLOCKS AT ALL BENDS AND TEES. DISINFECT ALL NEW LINES AND OBTAIN SAFE WATER SAMPLE PRIOR TO USE.
8. EXTREME CAUTION MUST BE FOLLOWED REGARDING THE COMPACTION OF ALL UTILITY TRENCHES. MECHANICALLY COMPACTED GRANULAR BACKFILL IS REQUIRED UNDER & WITHIN 5 FEET OF ALL PAVEMENT INCLUDING SIDEWALKS. FLOODING OF BACKFILL MATERIAL IS NOT ALLOWED.
9. TRACER WIRE (NO. 8 SINGLE STRAND COPPER) AND WARNING TAPE SHALL BE INSTALLED ON ALL UTILITIES IN ACCORDANCE WITH THE LOCAL AND STATE CODES. TRACER WIRE SHALL TERMINATE IN A VALVECO TERMINAL BOX AT EACH END.
10. MANHOLE TESTING ON SANITARY LINES AND PRESSURE TESTING ON WATERMAIN MAY BE REQUIRED BY THE OWNER OR MUNICIPALITY.
11. UPON COMPLETION OF FINAL PAVING OPERATIONS, THE UTILITY CONTRACTOR SHALL ADJUST ALL MANHOLE AND INLET RIMS AND VALVE BOXES TO FINISHED GRADE.

SPECIFICATIONS FOR PAVING

1. AGGREGATES USED IN THE CRUSHED STONE BASE SHALL CONFORM TO THE GRADATION REQUIREMENTS SECTIONS 301.2 AND 305.2.2 OF THE STANDARD SPECIFICATIONS. THICKNESS SHALL BE PER THE DETAIL ON THE PLANS. BASE SHALL BE 1/2 INCH DIAMETER UNLESS NOTED OTHERWISE.
2. SUBGRADE SHALL BE PROOFROLLED AND APPROVED BY A GEOTECHNICAL ENGINEER PRIOR TO PLACEMENT OF STONE BASE. EXCAVATE UNSUITABLE AREAS AND REPLACE WITH BREAKER RUN STONE AND RECOMPACT. REFER TO THE GEOTECHNICAL REPORT FOR ADDITIONAL SPECIFICATIONS.
3. EXISTING PAVEMENT SHALL BE SAWCUT IN NEAT STRAIGHT LINES TO FULL DEPTH AT ANY POINT WHERE EXISTING PAVEMENT IS REMOVED. CURB AND WALK SHALL BE REMOVED TO THE NEAREST JOINT. REMOVED PAVEMENT SHALL BE REPLACED WITH THE SAME SECTION AS EXISTING. MUNICIPAL STANDARDS MAY REQUIRE ADDITIONAL WORK.
4. ASPHALT FOR PARKING AREAS AND THE PRIVATE ROAD SHALL BE PER THE DETAILS MATERIALS AND PLACEMENT SHALL CONFORM TO THE DOT STANDARD SPECIFICATIONS, SECTION 450 AND 460. TYPE E-0.3 IS REQUIRED UNLESS NOTED OTHERWISE.
5. A COMMERCIAL GRADE MIX MAY BE SUBSTITUTED ONLY WITH APPROVAL FROM THE OWNER.
6. CONCRETE FOR CURB, DRIVEWAY, WALKS AND NON-FLOOR SLABS SHALL CONFORM TO SECTION 415 OF THE STANDARD SPECIFICATIONS, GRADE A, ASTM C-94, 6 BAG MIX, WITH A MINIMUM 28 DAY COMPRESSIVE STRENGTH OF 3,500 PSI. JOINTING SHALL BE PER SECTION 415.3.7 OF THE STANDARD SPECIFICATIONS WITH CONSTRUCTION JOINTS HAVING A MAXIMUM SPACING OF 10'. EXPANSION JOINTS SHALL BE PROVIDED EVERY 50'. CONCRETE SHALL BE FINISHED PER SECTION 415.3.8 WITH A MEDIUM BROOM TEXTURE. A CURING MEMBRANE IN CONFORMANCE WITH SECTION 415.3.12 IS REQUIRED.



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REVIEWED: ABA
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FOR REVIEW
CONSTRUCTION DETAILS







MEMORADUM

Office of the Village Engineer
Matthew J. Fineour, P.E.

TO: Village Board

FROM: Matthew Fineour, P.E., Village Engineer

SUBJ: Vidhya Corp. VIII, Inc.
Settlement Agreement #3

DATE: June 9, 2015

Background

In 2011, it was reported that petroleum was being discharged from the BP gas station's on-site storm sewer system to the STH 165 ditch line. The petroleum discharge is an illicit storm water discharge which violates Village Ordinances. Following several reports, observations, and citations, the Village entered into a settlement agreement with Vidhya Corp VIII, Inc. on November 19, 2012 as a means to address the illicit discharges. As the project progressed, the settlement agreement was amended twice; first, on April 1st, 2013 and again on February 3, 2014 in order to modify requirements.

The BP property has a prior record of contamination that was investigated from 2000 to 2004 with a WDNR case closure in 2006. With the new releases in 2011, the WDNR was notified and the WDNR opened a new contamination site case. The property owner must meet WDNR criteria and approval for site case closure, however, there is no specified timeframe set by the WDNR to accomplish this. WDNR does not oversee the property owner's project in this regard.

The settlement agreement contains remedial and reporting requirements to bring the property into compliance with Village Ordinances. Without going into detail of each specific requirement, the actions taken to address the illicit discharge include the following broad scope items:

- A. A site investigation to determine the source and extent of the contamination.
- B. Remedial plans and actions to: a) remove contaminated soils from the ditch line by the storm sewer discharge location, and b) prevent site contamination from further contributing to illicit discharges.
- C. Sampling, inspections, and reporting to monitor the site and the remedial treatment system.

Site Status

Over the past several years, Vidhya and their consultants have taken actions in order to comply with the settlement agreement and have made progress in addressing illicit discharges. The following broad scope items have been completed to date:

- A treatment system has been designed and installed to extract and treat contaminated groundwater that contains petroleum product. The groundwater is an identified contributing source for the illicit discharge due to the high groundwater elevations and interaction with existing storm sewer trenches. The treatment system is currently operational with on-going sampling and monitoring requirements.
- Contaminated soils were removed from the STH 165 ditch line in 2014.
- The site investigation continued in 2014 with additional soil and groundwater investigation(s) which is documented in a report prepared by Fehr Graham, Vidhya's environmental consultant. The report was submitted and reviewed by the WDNR and Village. The site investigation is not finished and requires on-going sampling and monitoring of the site.

Over the past year, there was one known illicit discharge that was reported during a routine Village inspection on September 4, 2014 in which a petroleum sheen was noticed at the storm sewer outfall. Village staff notified BP representatives of the illicit discharge. In review of the situation, the illicit discharge would appear to have been caused due to the treatment system having been non-operational for a period of time which was not caught by BP representatives, causing the groundwater elevation to rise above the settlement agreement height of 8-feet below ground level. Once the system was turned back on subsequent inspections showed no further illicit discharges.

Settlement Agreement Amendment No. 3

In accordance with the existing settlement agreement, Vidhya Corp VII is currently sampling the groundwater coming into and out of the treatment system on a monthly basis. Based on the testing results, Vidhya Corp. is requesting the sampling frequency to be reduced to a quarterly basis.

An amendment to the settlement agreement (#3) has been drafted to modify the sampling and analytical testing requirements and to provide requirements for the on-going site investigation based on the findings of the site investigation report and the WDNR's review comments. The drafted agreement as brought forth is acceptable to Village Staff. Village staff is accepting of the reduced sampling frequency with the terms of the original settlement agreement for independent sampling by the Village unchanged as stated in amendment #3 Section B (1).

Vidhya Corp. has indicated opposition to this particular provision in the agreement. Although progress has been made, due to the level of overall project performance being exhibited, Village staff does not recommend reducing the terms of the original settlement agreement for Village oversight. If this term is not acceptable to Vidhya Corp, it is then recommended that the Village continue on the terms of the original settlement agreement and existing amendments 1 and 2.

AMENDMENT #3 TO SETTLEMENT AGREEMENT

THIS AMENDMENT #3 to SETTLEMENT AGREEMENT is entered into this ____ day of _____, 2015 between the Village of Pleasant Prairie, Wisconsin (referred to as the "Village") and VIDHYA Corp VIII, Inc. (referred to as "Vidhya").

RECITALS

- A. VIDHYA Corp, VIII, Inc. ("Vidhya") is the current owner of a property located at 10477 120th Avenue, Pleasant Prairie Wisconsin (the "Property") with the following Tax Parcel Number: 92-4-122-302-0130. The Property contains the BP Amoco gasoline station #3789 and convenience store (the "Station").
- B. Vidhya received a Conditional Use Permit granted by the Village Plan Commission on July 16, 2012, 2012 and said approval included the requirement that Vidhya and the Village parties to enter into a **Settlement Agreement** to address remedial actions and activities for stopping illicit discharges of petroleum-based contamination from the Property. The **Settlement Agreement** was executed by the parties on November 19, 2012. Subsequently an **Amendment #1 to the Settlement Agreement** executed on April 1, 2013 was also approved subject to the May 28, 2013 Village Plan Commission amended Conditional Use Permit approval. Subsequently an **Amendment #2 to the Settlement Agreement** was entered into between Vidhya and the Village on February 3, 2014.
- C. On March 30, 2015, Vidhya submitted a written request to amend the Conditional Use Permit and modify the referenced **Settlement Agreement, Amendment #1 to Settlement Agreement** and **Amendment #2 to Settlement Agreement**.
- D. **Amendment #3 to Settlement Agreement** addresses Vidhya's request for modified testing requirements and to follow the recommendation and approval of the Wisconsin Department of Natural Resources for testing.
- E. The Village's Consultant, Drake Environmental Consulting Group ("Drake Environmental") and the Village Engineer have evaluated the request for modifications and in doing so reviewed all relevant materials provided to them by Vidhya and its consultants.

Now, therefore, the parties agree to amend the Settlement Agreement, **Amendment #1 to Settlement Agreement** and **Amendment #2 to Settlement Agreement** as follows:

A. Supplemental Site Investigation Plan.

A Comprehensive Site Investigation Plan, dated December 19, 2014 was prepared by Fehr Graham Engineering and Environmental, Inc. The report was reviewed by the WDNR and Village. Comments from the WDNR (including a May 4, 2015 email from Shanna Laube-Anderson) are included in Exhibit A and incorporated into this Amendment. Vidhya agrees based on the report findings and the WDNR's technical review to complete the following:

1. Provide quarterly monitoring of all nine site monitoring wells for laboratory analysis of PVOC+N to show a stable or decreasing contamination trend. Sampling shall include obtaining groundwater level measurements from all existing monitoring wells for evaluation of groundwater flow and treatment system influence and collection of extraction well depth measurements. The WDNR in its May 4, 2015 authorized Vidhya to "proceed with its plan to collect quarterly groundwater samples until a stable or decreasing plume is established, however long that may take."
2. If the WDNR indicates further definition of the extent of contamination is necessary, Vidhya will provide a revised plan acceptable to the Village.
3. A summary report shall be provided to the Village and its consultant for the quarterly site investigation monitoring results. The report shall be submitted no later than 45-days after the sampling event. The Village and its consultant shall be provided copies of the laboratory analytical reports for each quarterly sampling event within 10 business days of report issuance by laboratory (submittal of such laboratory analytical reports via email to the Village and its consultant within the 10 business day period will meet this requirement).
4. Vidhya shall notify the Village and its consultant of their intent to submit for WDNR case closure for the site investigation at least 5 business days prior to the request and shall provide the Village and its consultant with a copy of the case closure request report concurrently with its submittal of such a report to the WDNR.
5. The Supplemental Site Investigation Plan requirement shall be considered by the Village to have been completed upon Vidhya receiving WDNR conditional closure for the site subject to continuing obligations for the site.

B. Sampling & Analytical Testing – Active Zeolite & Carbon Treatment System Operation and Quarterly Groundwater Sampling

1. The Village agrees that reduced quarterly testing of active treatment system effluent may be permitted based on its review of current site conditions so long as the laboratory analytical results for the effluent samples remain below their respective WPDES Permit standards.

Such reduced sampling shall require Vidhya to continue to permit the Village to conduct monthly effluent sampling per the terms of the settlement agreement and the Village shall also be permitted to obtain routine water level information from existing and/or future extraction wells and/or any existing or future monitoring wells.

2. A quarterly report for the carbon treatment system operation shall be provided to the Village and its consultant within 45 days of each quarterly sampling event and shall contain the following elements at a minimum:
 - a. Analytical test results for each sampling event and sampling point along with a copy of a signed discharge monitoring report for the periods covered by the report.
 - b. Summary of gallons treated / discharged on a monthly basis based on actual readings obtained during treatment system inspection events.
 - c. A summary of the water level recordings of the extraction wells and any existing monitoring wells obtained during treatment system inspection events.
 - d. A summary of any high level alarm activations or any incidents where the water level is less than 8-feet below ground elevation. Vidhya shall immediately notify the Village (within 24 hours) of any incidents where it is discovered that the water level is less than 8-feet below ground elevation and take immediate action to rectify such a condition. Vidhya shall ensure that proper and sufficient training is provided to its staff in order to allow its staff to understand and implement proper response procedures in conjunction with any future treatment system alarm notifications. Such training shall be conducted every three months or more in order to ensure staff is adequately trained. In the event that staff does not demonstrate sufficient understanding of the system alarms, the Village may require Vidhya to install a remote alarm notification system which shall serve to immediately provide Vidhya's consultants and Village with notice that the treatment system alarm has been activated.

- e. A summary of the Zeolite and Carbon filter media status, including monitoring method, life expectancy of each filter, percent utilized, and expected change out date(s).
 - f. A summary of the Treatment System operations, system condition, and site inspections performed during the reporting period for each of the listed items below. Inspections shall be completed by a qualified individual for the task identified.
 - i. Inspection of the Treatment System and measurement of the water level recordings of the extraction wells no less than twice per month by an environmental professional with the experience and knowledge of operating and maintaining the system and obtaining water level recordings. If free product is observed within the extraction wells, the Village and the WDNR shall be immediately notified of such a discovery.
 - ii. Inspection of the site ditch line located at the treatment system discharge on a weekly basis and after each rain event of 0.5-inches or more in 24-hours. The quarterly report shall confirm that such inspections were completed and recorded in an inspection log. Copies of the required inspection logs shall be provided to the Village within 5 business days, if requested.
 - iii. Inspection of the storm sewer catch basin with oil absorbent boom no less than twice per month. The condition and change out dates of the oil absorbent boom(s) and storm drain baffle absorbents shall be recorded and reported to the Village.
 - iv. The system flow rate and measured water level elevation data from the extraction wells (in tabulated form) shall be provided to the Village and its consultant on a monthly basis.
 - v. Vidhya shall be responsible for performing routine monitoring of the site groundwater/ surface water conditions as necessary to prevent illicit discharges to the ditch.
3. Any change in the operation of the Treatment System, including planned maintenance activities or other activities which may affect the system function or groundwater elevations shall require prior written notification to the Village with a description and plan of such activities.

- 4. Vidhya shall agree to comply with all applicable Chapter 292, Wisconsin Statutes requirements and Chapter NR 700 to 754 Wisconsin Administrative Code requirements & regulations with regard to its continued operation of the active treatment system and to provide the WDNR, the Village and its consultant with all forms and reports required by State Statutes and WDNR regulatory requirements.

- C. Vidhya acknowledges the Village has not unreasonably delayed the issuance of any permits and Vidhya acknowledges that the Village has not breached any term or provision of the Agreement and has not caused any delays to Vidhya's performance under the Agreement.

- D. Except as specifically provided otherwise in this Amendment, Vidhya shall comply with each and every provision of the Agreement and all prior amendments.

- E. Vidhya shall comply with all other conditions as set forth in the adopted and amended Conditional Use permits for the property.

- F. Notices and information to be provided to Village Engineer Matt Fineour and DJ Burns at Drake Environmental in this Amendment shall be deemed delivered upon the email or facsimile transmission of such notices or information to both Village Engineer Matt Fineour and DJ Burns at Drake Environmental.

IN WITNESS WHEREOF, the parties have entered into this **Amendment #3 to Settlement Agreement** on the date first written above.

VIDHYA Corp VIII, Inc.

The Village of Pleasant Prairie

By: _____

By: _____

John P. Steinbrink, Village
President

Date: _____

Date: _____

ATTEST:

By: _____

Jane M. Romanowski, Village Clerk

From: [Matt Dahlem](mailto:Matt.Dahlem@dnr.wisconsin.gov)
To: [Matt Fineour](mailto:Matt.Fineour@dnr.wisconsin.gov)
Cc: djburns@drakecg.com
Subject: FW: VIDHYA - BP Pleasant Prairie
Date: Monday, May 04, 2015 3:16:03 PM
Importance: High

Matt,

Below is the response from WDNR regarding Fehr Grahams Site Investigation Report from December 2014.

MATT DAHLEM, P.G. I Senior Project Hydrogeologist
Fehr Graham - Engineering & Environmental

1237 Pilgrim Road
Plymouth, WI 53073
P: 920.892.2444
F: 920.892.2620
www.fehr-graham.com

-----Original Message-----

From: Laube-Anderson, Shanna L - DNR [<mailto:Shanna.LaubeAnderson@wisconsin.gov>]
Sent: Monday, May 04, 2015 2:42 PM
To: Matt Dahlem
Subject: VIDHYA - BP Pleasant Prairie

Matt,

Due to several phone calls with other interested parties I have now completed a full review of the Site Investigation Report.

As we have previously discussed, the extent and degree of soil contamination has been defined sufficiently. The lead remaining in the ditch appears to be all within the same range which indicates that it is most likely from general traffic deposits over the years of leaded gas usage. The PVOC's appear to have been defined and remediated.

Groundwater contamination has also been defined and the goal is that proposed quarterly monitoring events will indicate a stable or decreasing plume. Due to the presence of the ditch, very shallow groundwater and running of the dewatering system for the storm water and tank basin water control at the BP property groundwater flow direction is not clear cut. With MW-102, on the Culver's property having Benzene between the ES and the PAL, the goal is that after additional rounds of sampling that would drop even lower. MES-MW-6, to the north toward the ditch, is above the ES but the well east of that one along the ditch, MW-103, is below the ES and PAL. This indicates that the extent of groundwater is sufficiently defined. If future groundwater sampling indicates an increase we will revisit the proposed plan at that time. Based upon the presence of fiber optic, sewer lines and electric lines in the ditch I would assume getting access to install a well in the ditch would be difficult if not impossible and going to the other side of Hwy 165 would be a waste of money as the assumption is groundwater drainage in the area is to the wetland to the east therefore not migrating across the highway to the north.

Vapor sampling was performed at the BP building and does not indicate a vapor concern and soil data to the east of the BP building as well as groundwater samples do not indicate a need for further evaluation of vapors on or off-site.

The Department understands that the dewatering/groundwater pump and treat system will need to continue running and I will discuss the need to allow the system to continue operating with the Southeast Region Closure Committee once groundwater sampling indicates we are almost ready for

closure review. They may agree that turning the system off could cause more harm to the BP and its neighbors than any good data we would recover from that exercise. Just let me know when you think you are about ready to take the last round prior to closure and I can discuss this with the Committee and then if they determine that the system needs to be shut down and the monitoring wells sampled during the shutdown we can work through the details on that process.

I spoke with Lisa Wood, the attorney for Culvers, this morning. Her client is concerned about his property value due to the presence of petroleum contaminated soils along the property boundary. I explained what would happen at closure, that right now due to no ES exceedence on their property, MES-MW-2 looks to be on the BP side of the property line, they would get continuing obligation for soil and put on GIS. If that well MW-2 is in fact on Culver's property then a Groundwater GIS would also be placed on the Culver's property, which means that if a plan to install a drinking water well was ever considered then special casing would be necessary. I explained that the Soil GIS means, based upon the soil data we have, that the property owner would need to keep the landscaped area and the parking lot in good condition, which they most likely would do anyway if they wanted to keep their business going. I think she understood that since they are complying with the code and that in our eyes the extent and degree is defined the Department is not going to require additional sampling of soil. She was hoping that the Department would require the BP owner to do additional soil sampling on the Culver's property. I told her that her client can do additional sampling if they so desired and we would review it in combination with the rest of the data we have for this site. She also inquired about the Off-site Exemption Letter and I told her that they could apply for that and as long as the Culver's property was never a gas station or had tanks I did not see any reason why they would not get approved for that letter.

Now that I have completed a full review of the recently submitted report and have had several discussions with yourself and others about this site, you may proceed with the proposed plan to collect quarterly groundwater samples until a stable or decreasing plume is established, however long that may take. If something shows up unexpectedly please feel free to call me and we can discuss how to proceed.

Hope this helps address the concerns about this project. Please forward to all interested parties. Thank You.

Shanna Laube-Anderson
Hydrogeologist

We are committed to service excellence.
Visit our survey at <http://dnr.wi.gov/customersurvey> to evaluate how I did.

Shanna L. Laube-Anderson
Hydrogeologist - Remediation and Redevelopment Wisconsin Department of Natural Resources
9531 Rayne Rd
Sturtevant, WI 53177
Phone: 262-884-2341
Shanna.Laubeanderson@wisconsin.gov

dnr.wi.gov



Office of the Director of Public Works
John Steinbrink Jr., P.E.

To: Michael Pollocoff
From: John Steinbrink Jr.
Subject: Municipal Solid Waste Disposal Award of Contract
Date: June 9, 2015

On April 10, 2015, a request for proposal for Municipal Solid Waste Disposal was issued to VendorNet where it was sent to 111 waste management companies. It was also advertised in the Kenosha News Legal Section for two weeks. In addition, bid packets were sent to 7 landfill companies.

On Thursday, May 7, 2015, at 10:00 a.m., bids were received for Municipal Solid Waste Disposal. Two bids were received.

On Monday, May 18, 2015, it was recommended and approved by the Village Board of Trustees to reject all bids in order to foster true competitive bidding which enables the Village to keep costs low and garner the most qualified companies to do the work.

On May 19, 2015, a request for proposal for Municipal Solid Waste Disposal was re-issued to VendorNet where it was sent to 111 waste management companies. It was also advertised in the Kenosha News Legal Section for two weeks. In addition, bid packets were sent to 3 local landfill companies.

On Thursday, June 4, 2015, at 9:00 a.m., three bids were received for Municipal Solid Waste Disposal.

The proposals were evaluated based on annual tipping fee cost and travel cost to the landfill. Please see the attached bid analysis sheet.

Based on the attached analysis, Advanced Disposal offers the lowest cost for landfilling (tipping). Advanced Disposal has been under contract for the Village's solid waste disposal since 2009. The Solid Waste Department's tipping fee budget for 2015 is \$290,000. It is estimated the tipping fees under this contract will be \$282,750.

I recommend the contract be awarded to Advanced Disposal for solid waste disposal.

Solid Waste Landfill Analysis

3-Jun-15

Advanced Disposal

Tipping costs	Year 1	Year 2	Year 3
Tons /yr	6,500	6,600	6,700
Tipping fee	\$ 43.50	\$ 44.60	\$ 45.75
Tipping cost/yr	\$ 282,750	\$ 294,360	\$ 306,525

Transportation costs	Year 1	Year 2	Year 3
Village Labor	\$ 39.48	\$ 40.66	\$ 41.88
Village Fleet	\$ 40.40	\$ 41.61	\$ 42.86
Costs per hour	\$ 79.88	\$ 82.28	\$ 84.74

Transportation Time	Year 1	Year 2	Year 3
Hours/ trip	0.75	0.75	0.75
Trips/year	910	910	910
Hours per year	682.5	682.5	682.5

Trip Cost	Year 1	Year 2	Year 3
Cost per year	\$ 54,518.10	\$ 56,153.64	\$ 57,838.25

Total Annual Costs	\$ 337,268.10	\$ 350,513.64	\$ 364,363.25
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Estimated Three Year Costs \$ 1,052,145.00

Kestral Hawk

Tipping costs	Year 1	Year 2	Year 3
Tons /yr	6,500	6,600	6,700
Tipping fee	\$ 41.25	\$ 41.66	\$ 42.08
Tipping cost/yr	\$ 268,125	\$ 274,973	\$ 281,930

Transportation costs	Year 1	Year 2	Year 3
Village Labor	\$ 39.48	\$ 40.66	\$ 41.88
Village Fleet	\$ 40.40	\$ 41.61	\$ 42.86
Costs per hour	\$ 79.88	\$ 82.28	\$ 84.74

Transportation Time	Year 1	Year 2	Year 3
Hours/ trip	1.25	1.25	1.25
Trips/year	910	910	910
Hours per year	1137.5	1137.5	1137.5

Trip Cost	Year 1	Year 2	Year 3
Cost per year	\$ 90,863.50	\$ 93,589.41	\$ 96,397.09

Total Annual Costs	\$ 358,988.50	\$ 368,561.91	\$ 378,327.22
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Estimated Three Year Costs \$ 1,105,877.63

Waste Management

Tipping costs	Year 1	Year 2	Year 3
Tons /yr	6,500	6,600	6,700
Tipping fee	\$ 43.60	\$ 44.60	\$ 45.60
Tipping cost/yr	\$ 283,400	\$ 294,360	\$ 305,520

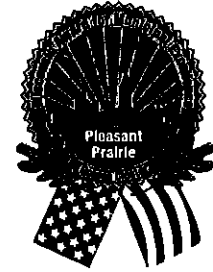
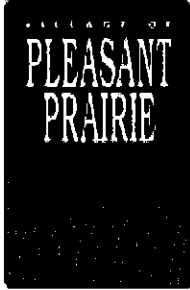
Transportation costs	Year 1	Year 2	Year 3
Village Labor	\$ 39.48	\$ 40.66	\$ 41.88
Village Fleet	\$ 40.40	\$ 41.61	\$ 42.86
Costs per hour	\$ 79.88	\$ 82.28	\$ 84.74

Transportation Time	Year 1	Year 2	Year 3
Hours/ trip	1.1	1.1	1.1
Trips/year	910	910	910
Hours per year	1001	1001	1001

Trip Cost	Year 1	Year 2	Year 3
Cost per year	\$ 79,959.88	\$ 82,358.68	\$ 84,829.44

Total Annual Costs	\$ 363,359.88	\$ 376,718.68	\$ 390,349.44
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Estimated Three Year Costs \$ 1,130,427.99



**REQUEST FOR PROPOSALS
RFP #15-SW-01-RB**

VILLAGE OF PLEASANT PRAIRIE

Municipal Solid Waste Disposal

Rebid May 19, 2015

ISSUED BY:

**Village of Pleasant Prairie, Wisconsin
Department of Public Works
8600 Green Bay Rd
Pleasant Prairie, WI 53158**

REQUEST FOR PROPOSALS

Notice is hereby given that proposals will be received by the Village of Pleasant Prairie ("Village"), Wisconsin for:

Municipal Solid Waste Disposal

Rebid RFP #15-SW-01-RB

Proposals shall be delivered or mailed to: John Steinbrink Jr. P.E., Director of Public Works, Village of Pleasant Prairie, 8600 Green Bay Rd, Pleasant Prairie, WI, 53158.

Proposals will be accepted until:

Date: Thursday June 4, 2015

Time: 9:00 A.M. (CST)

Proposals submitted after the above-noted due date and time will be rejected. Respondents accept all risks of late delivery of mailed submittals regardless of fault.

The Village reserves the right to reject any and all submittals and to waive irregularities and informalities in the submittal and evaluation process. This RFP does not obligate the Village to pay any costs incurred by respondents in the preparation and submission of their statement of qualifications. Furthermore, the RFP does not obligate the Village to accept or contract for any expressed or implied services.

It is the policy of the Village of Pleasant Prairie to assure that no person shall, on the grounds of race, color, religion, national origin or sex, as provided by Title VI of the Civil Rights Act of 1964, be excluded from participation in, be denied the benefits of, or be otherwise discriminated against under any of its federally funded programs and activities.

The Village is committed to a program of equal employment opportunity regardless of race, color, creed, sex, age, nationality, disability, or sexual orientation. The successful consultant must comply with the Village of Pleasant Prairie's equal opportunity requirements.



John Steinbrink Jr, P.E.
Director of Public Works

May 19, 2015

Date

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1) PURPOSE & INTENT

This solicitation involves **Municipal Solid Waste Disposal** for the Village of Pleasant Prairie. The Village is seeking proposals for land filling (tipping) fees associated with the disposal of residential municipal solid waste.

The Village of Pleasant Prairie will distribute Request for Proposal requests. A Village of Pleasant Prairie Evaluation and Selection Committee will evaluate the RFP's submitted and establish a short list. Those firms selected on the short list will be ranked. The Village of Pleasant Prairie will then discuss approach, project schedule and resources with the highest ranked firm. Negotiations of fee's, terms and conditions will follow. In the event that the negotiations with the highest ranked firm are unsuccessful, negotiations will then proceed to the second highest ranked firm.

2) SCOPE OF SERVICES/PROJECT REQUIREMENTS

A) DEFINITIONS

- (1) "VILLAGE" means the Village of Pleasant Prairie.
- (2) "COUNTY" means Kenosha County.
- (3) "PROJECT" means **Municipal Solid Waste Disposal**

B) GENERAL

- (1) The Services under this contract shall consist of all tasks necessary or incidental to accomplish the PROJECT.
- (2) The Respondent shall furnish all services and labor necessary to conduct and complete the services, and shall furnish all materials, equipment, supplies, and incidentals other than those designated in writing as to be furnished by the VILLAGE.
- (3) The services under this contract shall be performed in accordance with generally accepted standards.
- (4) The services shall comply with the applicable State and Federal laws and regulations consistent with the scope of project.

C) REQUIRMENTS - This project will require the selected firm to provide the following:

Municipal Solid Waste Disposal as outlined in this document

3) GENERAL INSTRUCTIONS AND FORMAT

The evaluation and selection of a consultant will be based on the information submitted in the request for proposal plus references and any required interviews/presentations. Consultants shall respond clearly and completely to all requirements. Failure to respond to any of the requirements in the RFP may be the basis for rejecting a submittal. The submitted qualifications shall be typed and submitted on 8.5" x 11" paper and bound securely. Tables/Graphs/Charts and other exhibits may be submitted on 11" x 17" paper, properly folded to an 8.5" x 11" size and bound securely within the document. There is a 12 total page limit to the responses.

4) MUNICIPAL SOLID WASTE DISPOSAL VOLUMES

The Village currently has 20,004 residents and is serviced by in-house collection vehicles with fully automated collection. The average annual residential solid waste and recyclables collected over the past 4 years is illustrated in the table below.

	2011	2012	2013	2014
Recycling (tons)	2,144	2,122	2,111	2,117
Garbage (tons)	5,951	6,009	6,213	6,445

As of January 1, 2015 there are 7,348 solid waste accounts in the Village of Pleasant Prairie.

5) SUBMITTAL PROCESS & REQUIREMENTS

Qualifications are sought from firms with recognized expertise in the **Municipal Solid Waste Disposal**.

Proposals shall include the following:

- A) Firm name, address, telephone number and contact person. A two-page statement of interest and qualifications for this project.
- B) Brief history of the firm.
- C) A brief (maximum four-page) project understanding description.
- D) Discussion of firm's specific abilities and expertise to provide the required services.
- E) Key personnel proposed as project team members.
- F) Clearly identify sub-consultants, if proposed, with similar information.
- G) Examples of specific knowledge, expertise and project experience related to this type of project.
- H) References of other owners for which the firm has provided similar services.
Reference information **must** include:
 - (1) Name of owner.
 - (2) Project name.
 - (3) Brief description of firm's involvement.
 - (4) Contact person.
 - (5) Project/Contact Address.
 - (6) Project/Contact Telephone number.
 - (7) Firm's key personnel assigned to the referenced project.
- I) Contract Details
 - (1) List of accepted materials.
 - (2) Hours of operation accepting delivery of Solid Waste Material.
 - (3) Location of landfill.

- (4) Distance from 8600 Green Bay Road, Pleasant Prairie, WI.
- (5) Any limitations for acceptance of delivery– this would include holidays, weekends, weather, volume or weight, etc.
- (6) Tipping fee for material.
- (7) Any other additional applicable fees or taxes.
- (8) Insurance requirements, if any.
- (9) Period of contract (minimum of 3 years).
- (10) Options for renewal of three (3) additional years.
- (11) Years of landfill life.
- (12) Limitations or weight restrictions to or at the transfer/landfill.
- (13) Any licenses requirements for interstate trucking, if applicable.
- (14) Types and frequency of reports submitted to Village of Pleasant Prairie.
- (15) Billing frequency and other pertinent financial information.
- (16) Requirements of the Village such any public notifications.
- (17) Liabilities, if any, including landfill closure cost.

Firms must submit the following items:

- One original (clearly labeled as such) PLUS three copies of all materials required for acceptance of their qualifications (**Proposal Items A through I**).

On or before 9:00 a.m. on Thursday, June 4, 2015, via U.S. Mail, UPS, Fed Ex, DHL, Airborne, etc. to:

Village of Pleasant Prairie
Attn: John Steinbrink Jr., P.E.
Director of Public Works
8600 Green Bay Rd.
Pleasant Prairie, WI 53158

The Village of Pleasant Prairie does not accept facsimile or email submitted proposals. A firm, if it so chooses, may hand-deliver its proposal package on or before the date and time listed above. All submittals must be date-stamped by the Village of Pleasant Prairie. Submittals received after 9:00 a.m. on Thursday, June 4, 2015, will not be accepted.

The complete proposal package shall be plainly marked as shown below. The **envelope** shall contain the proposal transmittal letter and the firm's technical and qualification proposal (original plus three copies).

All submittals must be packaged, sealed, and clearly labeled to show the following information on the outside of the package:

Firm's Name and Address
RFP Title – **"Village of Pleasant Prairie – RFP#15-SW-01-RB
Municipal Solid Waste Disposal Rebid**
RFP Due Date of Thursday, June 4, 2015

*It is not the intent of this RFP to solicit an **overly long response**, but it is important that the firm's experience/expertise is adequately described. It will, for example, be much more useful to address abilities and expertise directly comparable to this project than to include an exhaustive list of all projects completed by the firm. Village staff will review the submitted proposals. The selected firm will meet with the Village to prepare a contractual agreement between the Village and the firm after the final selection is complete.*

6) SCHEDULE

Listed below are estimated dates and times of actions related to this RFP. In the event that the Village finds it necessary to change any of the specific dates and times, it will do so by issuing amendments to this RFP. Failure by the Village to issue amendments to this schedule will not invalidate this selection process.

RFP SCHEDULE:

EVENT	DATE
RFP Release	May 19, 2015
Questions in writing (if any) Due	May 28, 2015
Rebid RFP Responses Due	Thursday, June 4, 2015
Internal Village Review of RFP Responses	June 4, 2015
Scope and Contract Negotiation	June 5, 2015
Contract Recommendation to the Village Board	June 8, 2015
Village Board Approval	June 15, 2015
Contract begins	August 3, 2015

7) EVALUATION COMMITTEE, SELECTION & AWARD PROCESS

A) Evaluation committee. The Village RFP Evaluation & Selection Committee will consist of:

- John Steinbrink, Jr., Public Works Director
- Joe Middleton, Streets Foreman
- Jesse Houle, Village Construction Manager

The evaluation committee members have been selected because of their special expertise and knowledge of the service(s) and/or product(s) that are the subject of this RFP.

B) The selection committee will evaluate the proposals utilizing the proposal evaluation criteria (except compensation). The Village, because of time constraints and depending upon the thoroughness of the proposals, may at its sole option award a contract based upon the initial proposal submittal. Do not assume there will be an opportunity for submittal of additional information. Submit your proposal as if it were your "best and final offer."

C) If the Village intends to hold interviews, the proposers to be interviewed will be contacted to schedule an interview. The interview will further evaluate the proposer in the following categories: (1) Project Manager/Key Personnel; (2) Overall Qualification of Firm/Team; and (3) Project Approach. The point values or ratings for these categories are subject to change based upon the interview.

D) Final evaluation:

Upon completion of any interviews/presentations by the respondents, the Evaluation and Selection Committee will review its evaluations and make any necessary ranking adjustments based on the information obtained in the interview/presentation, possible reference checks, and any other pertinent respondent information.

E) Right to reject qualifications and negotiate contract terms:

The Village reserves the right to reject any and all submittals. The Village reserves the right to negotiate the terms of the contract, including the award amount, with the selected consultant prior to entering into a contract.

F) Award of contract:

The Village reserves the right to make an award without further discussion of the submittals. The firm selected as the apparent successful firm will be expected to enter into a contract with the Village. The foregoing should not be interpreted to prohibit either party from proposing additional contract terms and conditions during the negotiations of the final contract. If the selected firm fails to sign the contract within ten (10) business days of delivery of the final contract, the Village may elect to negotiate a contract with the next-highest ranked firm. The Village shall not be bound, or in any way obligated, until both parties have executed a contract. No party may incur any chargeable costs prior to the execution of the final contract. Following consultant selection, the successful consultant shall prepare a proposal and scope of work for review by the Village. Once the Village and consultant have reached an agreement on the scope of services, a final contract will be prepared by the Village.

8) MISCELLANEOUS

A) Questions

Questions regarding this RFP may be directed to John Steinbrink Jr. P.E., Director of Public Works, via e-mail at jsteinbrink@plprairiewi.com. Unauthorized contact regarding this RFP with other Village employees may result in disqualification from consideration in the proposal. Any oral communications will be considered unofficial and non-binding on the Village, unless it is followed by a written statement from the Village.

B) Rejection of Submittals

The Village reserves the right to reject any and all submittals and to waive irregularities and informalities in the submittal and evaluation process. This RFP does not obligate the Village to pay any costs incurred by respondents in the preparation and submission of their qualifications. Furthermore, this RFP does not obligate the Village to accept or contract for any expressed or implied services.

C) Withdrawal of Qualifications

Consultants may withdraw a submittal, in writing, at any time up to the proposal due date and time. The written withdrawal notice must be received by the Village Director of Public Works. The notice must be signed by an authorized representative of the consultant.

D) Incurring Costs

The Village is not liable for any costs incurred by consultants in responding to this RFP.

E) Proprietary Proposal Material

Any proprietary information revealed in the submittal should be clearly identified as such by the respondent.

F) Terms

There is no expressed or implied obligation of the Village to reimburse firms for any costs incurred in preparing submittals in response to this request. The Village reserves the right to reject any and all submittals and to modify the scope of services. The Village further reserves the right to retain all submittals and to use any idea in a submittal regardless of whether that submittal is selected.

G) Signatures

RFPs shall be signed by one of the legally authorized officers of the submitting firm/corporation. If awarded the contract, the contract shall also be executed by said officer.

H) Contract Negotiation

The Village reserves the right to negotiate all elements of the submittals, proposals, terms and conditions, and/or scope of work as part of the contract negotiation process prior to any formal authorization of the contract by the Village.

I) Equal Opportunity Employment

The successful consultant(s) must comply with the Village equal opportunity requirements. The Village is committed to a program of equal employment opportunity regardless of race, color, creed, sex, age, nationality or disability.

J) Title VI

It is the Village's policy to assure that no person shall, on the grounds of race, color, religion, sex or national origin as provided by Title VI of the Civil Rights Act of 1964, be excluded from participation in, be denied the benefits of, or be otherwise discriminated against under any of its federally funded programs and activities.

K) Insurance Requirements

The selected firm shall maintain insurance that is sufficient to protect the firm's business against all applicable risks. Standard requirements may be negotiated if it is in the best interest of the Village.

L) Non-Endorsement

As a result of the selection of a firm to supply products and/or services to the Village, firm agrees to make no reference to the Village in any literature, promotional material, brochures, sales presentation or the like without the express written consent of the Village.

M) Non-Collusion

Submittal and signature of a statement of qualifications swears that the document is genuine and not a sham or collusive, and not made in the interest of any person not named, and that the consultant has not induced or solicited others to submit a sham offer, or to refrain from proposing.

N) Compliance with Laws & Regulations

In addition to nondiscrimination and affirmative action compliance requirements previously listed, the consultant or consultants ultimately awarded a contract shall comply with Federal, State and local laws, statutes and ordinances, and industry standards relative to the execution of the work. This requirement includes, but is not limited to, protection of public and employee safety and health; environmental protection; waste reduction and recycling; the protection of natural resources; permits; fees; taxes; and similar subjects.

O) Public Records

Under Wisconsin state law, the documents (including but not limited to written, printed, graphic, electronic, photographic or voice mail materials and/or transcriptions, recordings or reproductions thereof) submitted in response to this RFP (the "documents") become a public record upon submission to the Village, subject to mandatory disclosure upon request by any person, unless the documents are exempted from public disclosure by a specific provision of law. If the Village receives a request for inspection or copying of any such documents it will promptly notify the person submitting the documents to the Village (by U.S. mail and by fax if the person has provided a fax number) and upon the written request of such person, received by the Village within five (5) days of the mailing of such notice, will postpone disclosure of the documents for a reasonable period of time as permitted by law to enable such person to seek a court order prohibiting or conditioning the release of the documents. The Village assumes no contractual obligation to enforce any exemption. The Village does not accept any responsibility for agreements, contracts or purchase orders issued by other public agencies to the consultant. Each public agency accepts responsibility for compliance with any additional or varying laws and regulations governing purchase by or on behalf of the public agency. The Village accepts no responsibility for the performance of the consultant in providing goods and/or services to other public agencies, nor any responsibility for the payment price to the consultant for other public agency purchases.

P) Clarification and/or Revisions of this RFP:

Revisions to this RFP will only be made by an official written amendment issued by the Village. In order to be eligible to receive amendments to this RFP, all respondents are responsible to notify the Village of its official contact person, address and email address. All amendments/clarifications will be forwarded to the respondents of record.

**VILLAGE OF PLEASANT PRAIRIE
RESPONDENT'S PROPOSAL
SIGNATURE PAGE AND LEGAL STATUS**

THE UNDERSIGNED CERTIFY THAT THEY ARE AN OFFICIAL LEGALLY AUTHORIZED TO BIND THEIR FIRM AND TO ENTER INTO A CONTRACT SHOULD THE VILLAGE ACCEPT THIS PROPOSAL.

PROPOSAL BY: _____
(Name of Firm)

LEGAL STATUS OF RESPONDENT: (Please check the appropriate box)

CORPORATION _____ STATE OF INCORPORATION _____

PARTNERSHIP _____ LIST NAMES _____

DBA _____ EXPLAIN _____

SIGNATURE OF RESPONDENT: _____ TITLE: _____

ADDRESS: _____ CITY: _____

ZIP: _____ TEL: _____

Signed this _____ Day of _____ 20____.

CONTRACTOR'S AFFIDAVIT

STATE OF WISCONSIN)

(SS.

_____ VILLAGE)

_____, an office of

to-wit: I, _____, being first duly sworn in oath, says:

That _____, has no contractual relationships whatsoever, either direct or indirect, with any officer, agent, or employee of the

Owner: _____; nor has any officer, agent, or employee of the Owner any financial interest, either direct or indirect, present or prospective, absolute or conditional in the contract to which this bond pertains, as this affiant well knows.

Dated at _____ this _____ day of _____, 20__

Affiant _____

Title _____

Subscribed and sworn to before me this _____ day of _____, 20__

Notary Public, State of Wisconsin

My Commission Expires: _____

PROPOSAL FOR:

Village of Pleasant Prairie

Attn: John Steinbrink, Jr.

8600 Green Bay Rd.

Pleasant Prairie, WI 53158

Submitted By:

Advanced Disposal Services Zion Landfill, Inc.

701 Green Bay Rd.

Zion, IL 60099

Contact: James Lewis

Phone: 847-599-5910

A) Statement of interest and qualifications

Advanced Disposal Services (ADS) is interested in continuing to provide disposal services for the Village of Pleasant Prairie. Advanced Disposal Services, Zion Landfill, Inc. (Zion) became the owner of the Zion Landfill in 2012 by way of a stock purchase agreement with Veolia. Zion has been the outlet for the Village's municipal solid waste since 2009. Zion's management team has not changed since the initial contract was signed with the Village. Jim Lewis, the general manager, has worked for the landfill since 1995. John Hagopian, the operations manager, has been with the landfill since 1986. The administrative staff, responsible for the prompt, accurate, billing of customers, has not changed since 2009. With this team in place and a recent expansion which extends the projected life of the landfill to 2028, Zion is prepared to serve the disposal needs of the Village for many years.

B) History of the firm

See Attachment 1

C) Project description

The successful bidder is responsible for the weighing, disposal and billing of approximately 6,500 tons of garbage per year for a minimum of three years. The garbage will be collected and transported by the Village.

D) Abilities and expertise to provide the required services

Zion is a modern composite-lined landfill that sits on top of a twenty foot layer of clay. These combine to maximize the environmental security of the site. The site is managed by Jim Lewis, a Civil Engineer and licensed PE with twenty three years in the business. Mr. Lewis is supported by a staff of long-time employees dedicated to the safe and efficient burial of garbage.

In an attempt to further improve the efficiency of our customers, Zion is installing an automated scale. Once operational, this scale will allow select customers (including the Village) to minimize wait times by using the new scale.

E) Key personnel

Landfill:

Jim Lewis - General Manager

John Hagopian - Operations Manager

Kelly Cunningham, Patty Shebesta – Billing

F) Sub-consultants

None

G) Expertise or experience relative to this work

The team in place at the landfill has demonstrated their ability to provide the required services over the last six years.

H) References

1. Name of Owner: AbbVie

Description of Involvement: Zion provided disposal services for AbbVie and formerly Abbott Labs

Contact Person: William Lewis

Address: 1401 Sheridan Rd D731 Bldg M4, North Chicago, IL 60064

Phone: (office) 1 847-937-6096 (cell) 1 847-772-0728

2. Name of Owner: TKG Environmental Services Group LLC

Description of Involvement: Zion provides disposal services for TKG

Contact Person: Rich Katz

Address: 3804 Hawthorn Court, Waukegan, IL 60087

Phone: (847) 505-1402

I) Contract details

1. List of accepted materials:

Municipal solid waste, construction and demolition debris, special waste (with proper profile and manifest)

2. Hours of Operation:

Zion M-F 6:30-3:30, Sat: 7:00-11:00

3. Location of Landfill:

701 Green Bay Rd. Zion, IL 60099

4. Distance from 8600 Green Bay Rd, Pleasant Prairie, WI:

4.9 miles

5. Limitations for acceptance of delivery:

a. Extreme weather including snow/ice and or wind

b. Zion observes New Years Day, Memorial Day, 4th of July, Labor Day, Thanksgiving Day, and Christmas. When these Holidays occur on Saturday or Sunday they may be observed on the following Monday.

6. Tipping fee for material

Year 1 - \$43.50/ton

Year 2 - \$44.60/ton

Year 3 - \$45.75/ton

Year 4 - \$46.90/ton

Year 5 - \$48.10/ton

Year 6 - \$49.30/ton

Rates assume a six year agreement and payment by check within 30 days of invoice.

7. Any additional applicable fees or taxes
These rates include all current fees and taxes. The current state fee is \$2.22/ton. The current County fee is \$1.27/ton. Changes to these fees will be passed on to the Village.
8. Insurance requirements
See Attachment 2
9. Period of contract
Zion is proposing 6 years
10. Options for renewal of (3) additional years
Zion is open to multiple options beyond the initial contract term.
11. Years of landfill life
12.5 years
12. Limitations or weight restrictions to or at the landfill
There are no weight restrictions at Zion.
13. Any licenses requirements for interstate trucking, if applicable
There are no license requirements at Zion.
14. Types and frequency of reports submitted to the Village
Invoices will provide the ticket detail for each load the Village brings to Zion. Reports will continue to be provided when requested.
15. Billing frequency and other pertinent financial information.
Invoices are normally sent weekly.
16. Requirements of the Village such as any public notification.
Zion has no public notification requirement pertaining to this business opportunity. Village employees must follow ADS safety policies while on site.
17. Liabilities, if any, including landfill closure cost.
The Village would have no liability for landfill closure cost. Liability for all other matters would depend on the incident at hand.

VILLAGE OF PLEASANT PRAIRIE
RESPONDENT'S PROPOSAL
SIGNATURE PAGE AND LEGAL STATUS

THE UNDERSIGNED CERTIFY THAT THEY ARE AN OFFICIAL LEGALLY AUTHORIZED TO BIND THEIR FIRM AND TO ENTER INTO A CONTRACT SHOULD THE VILLAGE ACCEPT THIS PROPOSAL.

PROPOSAL BY: Advanced Disposal Services Zion Landfill, Inc.
(Name of Firm)

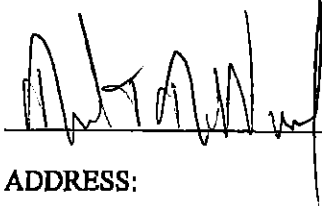
LEGAL STATUS OF RESPONDENT: (Please check the appropriate box)

CORPORATION STATE OF INCORPORATION Illinois

PARTNERSHIP LIST NAMES _____

DBA EXPLAIN _____

SIGNATURE OF RESPONDENT:



TITLE:

Regional Vice President

ADDRESS:

701 Geen Bay Road

CITY:

Zion, IL.

ZIP: 60099

TEL: 847-623-3870

Signed this 21 Day of May 2015.

CONTRACTOR'S AFFIDAVIT

STATE OF WISCONSIN)

(SS.

VILLAGE)

Advanced Disposal Services
Zion Landfill, Inc., an office of

Advanced Disposal Services Zion Landfill, Inc.

to-wit: I, Daniel T. Dewaard, being first duly sworn in oath, says:

Advanced Disposal Services
That Zion Landfill, Inc. has no contractual relationships whatsoever, either direct or indirect, with any officer, agent, or employee of the

Advanced Disposal Services
Owner: Zion Landfill, Inc.; nor has any officer, agent, or employee of the Owner any financial interest, either direct or indirect, present or prospective, absolute or conditional in the contract to which this bond pertains, as this affiant well knows.

Dated at Advanced Disposal this 21 day of May, 2015

Affiant [Signature] Daniel DeWard

Title Regional Vice President

Subscribed and sworn to before me this 21 day of May, 2015

[Signature]
Notary Public, State of ~~Wisconsin~~ Illinois

My Commission Expires: 1/12/17

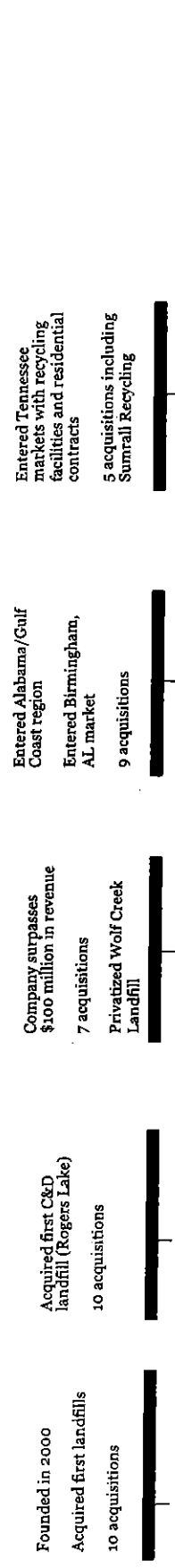


Attachment 1



Advanced Disposal

KEY MILESTONES



2001 Founded in 2000
Acquired first landfills
10 acquisitions

2002 12 acquisitions
Eagle Point Landfill opened

2003 Acquired first C&D landfill (Rogers Lake)
10 acquisitions

2004 3 acquisitions

2005 Company surpasses \$100 million in revenue
7 acquisitions
Privatized Wolf Creek Landfill

2006 Purchased by Highstar
3 acquisitions

2007 Entered Alabama/Gulf Coast region
Entered Birmingham, AL market
9 acquisitions

2008 Surpasses over 30K commercial and industrial customers
7 acquisitions

2009 Entered Tennessee markets with recycling facilities and residential contracts
5 acquisitions including Sumrall Recycling

2010 18 acquisitions
Entered North Carolina and South Carolina markets with awarded residential contracts and acquisition of Southland Sanitation
Turkey Trot Landfill opened

2011 Completed refinace
11 acquisitions including Guardian Waste Group
Asset Swap with Republic Services

2012 3 acquisitions including Interstate Waste Services and Veolia ES Solid Waste
Tripled company size and expanded footprint to 20 states

2013 17 acquisitions including G&C Waste Services
DOJ Divestitures in NY and GA per Veolia acquisition
Other remaining divestitures in NY and NJ

2014 4 acquisitions as of August
Change of Leadership Team
Charlie Appleby retires and Richard Burke assumes CEO role

Since its founding, Advanced Disposal has:

- Grown to 91 collection operations, 42 landfills, 73 transfer stations and 23 recycling facilities in 17 states and the Bahamas
- Grown its customer base through city, county & subscription contracts, representing more than 2.7 million residential customers and more than 202,000 commercial & industrial and construction & demolition customers
- Grown its team of employees to more than 5,400
- Completed 177 acquisitions and development projects
- Become the largest privately-owned environmental services company in the United States

Attachment 2

ARTICLE I. CONTRACTOR'S INDEMNITY AND INSURANCE. Contractor agrees to defend, hold harmless and unconditionally indemnify Company, and all of its Affiliates (defined below), and all of their respective officers, directors and employees, against and for all liabilities, costs, expenses (including attorney's fees and expenses of investigation), claims and damages which Company may at any time suffer or sustain or become liable for by reason of any accidents, damages or injuries (including injuries resulting in death) either to the persons or property or both, of Contractor or Company or employees of either party, or to any other parties, in any manner caused by or resulting from Contractor's breach of this Agreement or acts or failures to act by Contractor or its employees or agents in the performance of this Agreement; provided, however, that such indemnification and hold harmless shall not apply to claims for loss, damage, injury or death to the extent caused by the negligence of Company.

Without limiting any of the other obligations or liabilities of the Contractor for work performed on the premises or property of Company pursuant to this Agreement, Contractor will at all times specified below carry and continuously maintain at its own expense, or cause to be carried and continuously maintained, at least the minimum insurance coverage set below and with insurance companies qualified to do business in the State of _____ and with an A.M. Best rating of no less than A X.

Coverage	Limits
Worker's Compensation	Statutory Limits
Employer's Liability	\$1,000,000 Accident – Each Employee \$1,000,000 Disease – Policy Limits \$1,000,000 Disease – Each Employee
Commercial General Liability including Products/Completed Operations	\$1,000,000 Each Occurrence \$2,000,000 Aggregate
Automobile Liability including Owned, Leased, Non-Owned and Hired Vehicles	\$1,000,000 Each Accident
Umbrella Liability	\$2,000,000 Each Occurrence
Contractor's Pollution Legal Liability	\$1,000,000 Each Occurrence \$2,000,000 Aggregate
Professional Liability	\$2,000,000 Each Claim

Insurance shall be endorsed to provide that the Company, if any, shall be included as additional insured with the understanding in each case that any obligation imposed upon the insured (including the liability to pay premiums) shall be the sole obligation of Contractor or its Subcontractors and not that of Company. Inasmuch as such insurance is written to cover more than one insured, all terms, conditions, insuring agreements and endorsements, with the exceptions of limits of liability, shall operate in the same manner as if there were a separate policy covering each insured.

With respect to all insurance maintained by Contractor hereunder, (a) the interests of Company shall not be invalidated by any action or inaction of any other person, and Company shall be insured regardless of any breach or violation by the Contractor or any other person of any warranties, declarations or conditions contained in such policies; (b) all deductibles or self-insured retentions shall be in amounts acceptable to Company but shall be paid by Contractor; (c) the insurers thereunder shall waive all rights of subrogation against Company and its subsidiaries, any right of set-off and counterclaim and any other right to deduction whether by attachment or otherwise; (e) such insurance shall be primary without right of contribution of any other insurance or self-insurance carried by Company with respect to their interest in the Work; and (e) if such insurance is cancelled by the insurer for any reason whatsoever (including nonpayment of premium) or any substantial change is made in the coverage that affects the interests of Company, such insurance shall nonetheless remain effective for thirty (30) days (except for nonpayment of premium which shall be ten (10) days) receipt of written notice to Company sent by registered mail from such insurer of such cancellation or change.

Contractor shall furnish Company prior to the date that Contractor signs this Agreement, and on any coverage renewal date, approved certificates of all insurance required hereby executed by each insurer or by an authorized representative of each insurer where it is not practical for such insurer to execute the certificate itself. Such certification shall evidence that the insurance required is in effect, identify the underwriting carrier, the type of insurance, the insurance limits (including applicable deductibles) and the policy term, and shall specifically list the special provisions enumerated for such insurance required by this section. The insurance certificates shall provide that not less than 30 days advance written notice will be given to Company prior to any cancellation of the coverage. Upon request, Contractor shall furnish Company with copies of all insurance policies, binders, endorsements, cover notes and other evidence of such insurance obtained by Contractor.

Contractor shall include in each subcontract it enters into provisions not less favorable to Company than those contained in this Section; except that in those provisions in each subcontract, "Contractor" will be substituted for "Company" and "subcontractor" will be substituted for "Contractor". All insurance procured by Contractor and evidenced in the referenced certificates of insurance above shall be kept valid to the extent relevant for the duration of Contractor's performance of its obligations under this Agreement.

Republic Services – Kestrel Hawk Landfill
Proposal in Response to
“Village of Pleasant Prairie – RFP#15-SW-01-Municipal Solid Waste Disposal”
RB

This proposal is being submitted by Republic Services Kestrel Hawk Landfill located in Racine, Wisconsin. Kestrel Hawk Landfill is owned and operated by Republic Services of Wisconsin, LLP, which is a wholly owned subsidiary of Republic Services, Inc. Contact information for Kestrel Hawk is as follows:

Kestrel Hawk Landfill
1989 Oakes Road
Racine, WI 53406

Contacts: Mike Williams, Division Manager
(262) 884-7081

Jodi Acheson, Manufacturing and Env. Services Rep.
(262) 215-1774

History of the Firm:

Kestrel Hawk Landfill started out as the original City of Racine landfill in 1963. It was then sold to Land Reclamation who operated the landfill until the early 1990s when it was sold to Sanifill. Sanifill obtained the expansion permits for the current site in 1994. Sanifill then sold the site to USA Waste. USA Waste merged with Waste Management in 1998 and as a result of that merger, Kestrel Hawk was divested to Republic Services on November 11, 1998. Republic Services has owned and operated Kestrel Hawk Landfill since then. In October of 2010, Republic Services entered into an agreement with Veolia (now Advanced Waste) to divest collection operations in southern Wisconsin (known as Green Valley Disposal and located in Waunakee, Delavan and Racine) along with Mallard Ridge Landfill in Delavan, Wisconsin. Kestrel Hawk Landfill was not sold to Advanced and now operates as the only Republic Services location in southern Wisconsin.

About Republic Services:

Every Republic Services division is guided by five essential core values – to be Respectful, Responsible, Reliable, Resourceful, and Relentless in all we do, every day. Our highly passionate, professional team is reminded of these principles every time they see the five R's joined together to form the Republic Services Star. It's what makes us who we are, reminding us to keep our customers at the heart of it all.

Republic Services, Inc. was incorporated in 1996 with a “can do” spirit, driving its dramatic growth and acquisitions through the years, welcoming other organizations that share its values and fiduciary discipline. Today, Republic Services, Inc. is the second largest provider of services in the domestic non-hazardous solid waste industry, as measured by revenue as well as a Fortune 500 company, publicly traded on the New York Stock Exchange (NYSE:RSG).

Through our subsidiaries, we provide non-hazardous solid waste and recycling services for commercial, industrial, municipal and residential customers. Our customers come first as we strive to safely and sustainably provide reliable service through 340 collection operations, 197 transfer stations, 190 active solid waste landfills and 61 recycling centers and 73 landfill gas and renewable energy projects across 39 states and Puerto Rico. Republic Services, Inc. is a holding company and all operations are conducted by its subsidiaries.

Project Understanding, Specific Abilities, and Expertise to Provide the Required Services:

It is our understanding that this RFP is for disposal of municipal solid waste. Kestrel Hawk Landfill is licensed in the State of Wisconsin (license #572) to accept municipal solid waste. Kestrel Hawk does not accept recycling materials for processing or transfer. Kestrel Hawk does not provide any collection services. Kestrel Hawk acts solely as a landfill and accepts solid waste and permitted, approved special wastes. The permit for the current area of operations was issued by the Wisconsin Department of Natural Resources on August 30, 1996. Kestrel Hawk operates in compliance with all federal, state and permit requirements. The WDNR inspects Kestrel Hawk on a routine basis and there have been no issues or violations identified since 2008 when a minor issue with our gas flare occurred.

There are 9 total employees that work at Kestrel Hawk, one Division Manager (who also oversees another landfill but is at Kestrel Hawk 3-4 days per week), one supervisor, one heavy equipment mechanic, three heavy equipment operators, one administrative assistant, one scale operator and one laborer. Further details on the key personnel will be provided in the next section of this proposal.

Kestrel Hawk accepted an average of 735 tons per day in 2014. The additional tonnage received from Pleasant Prairie would be no extra burden on our operations and could easily be handled. The operations at Kestrel Hawk are extremely customer friendly and are extremely conducive to minimizing customer time on site. In 2013 we constructed Phase 5 of Kestrel Hawk which is located directly adjacent to the entrance to the site and next to the scale. Trucks that enter Kestrel Hawk travel only 200 feet on concrete until they reach the scale. Only 100 feet after exiting the scale, trucks turn left and into our active area. At some landfills, trucks have to travel long distances to reach the active area, thus adding time to routes. Kestrel Hawk uses auto fluff for daily cover. Auto fluff is the shredded remnants of car seats and other non-metallic parts of scrapped cars. The advantage of using auto fluff is that trucks do not get stuck on the fluff when it rains. Kestrel Hawk rarely, if ever, has to pull trucks that get stuck. Landfills that rely on soil for daily cover often find it difficult to keep trucks moving efficiently during adverse weather.

In addition to the 5 R's referenced earlier, Republic Services has 5 priorities that we place next to each point of our star logo. The priority that sits atop the top point of the star is Safety and Compliance. There is nothing more important at Republic than safety and compliance. Here at Kestrel Hawk, our employees attend two safety meetings per month. One is part of our "Focus 6" program where we focus on the 6 aspects of our operations that cause the most potential for incidents or accidents. Those are Backing, Roll Overs, Rear End Collisions, Intersections and Push, Pull, Lift Mechanics. The other is related to OSHA compliance topics such as lockout/tagout, bloodborne pathogens, confined space and hazard communication. Our safety culture here at Kestrel Hawk is engrained in our day to day operations. Our commitment to safety is not just for our employees, it is for our customers as well. Our heavy equipment operators politely remind all of our customers to wear their proper PPE when on site and to follow all safety rules. The safety of our employees while on our site is of the utmost importance to us here at Kestrel Hawk.

Key Personnel Proposed as Project Team Members:

Mike Williams, Division Manager

Mike serves as a Division Manager for Republic Services and has responsibility over Kestrel Hawk Landfill in Racine, Wisconsin and Upper Rock Island County Landfill in East Moline, Illinois. Mike spends 3 to 4 days a week at Kestrel Hawk and 1 to 2 days a week at Upper Rock. Mike has 27 years of experience with landfill operations, design, compliance and monitoring. Mike spent 13 years with Browning-Ferris Industries. As part of that 13 year career with BFI, Mike was the District Vice President for Mallard Lake Landfill and the Zion Landfill. Mike was instrumental in securing the expansion of the Zion Landfill located in Zion Illinois. Prior to joining Republic Services in 2006, Mike spent 6 years at Ernst & Young providing audit support for landfills across the country. Mike has physically been on over 70 landfills and has observed many different operational styles and his expertise greatly enhances Kestrel Hawk's customer experience. Mike is a Certified Facility Manager in the State of Wisconsin.

Steve Mack, Operations Supervisor

Steve has been at Kestrel Hawk for 14 years. He has served as a mechanic for our collections operations, a driver, an operator, a heavy equipment mechanic and now as a supervisor. There is no part of solid waste collection operations and disposal that Steve is not familiar with. He understands what drivers deal with on a daily basis and also understands what the landfill needs to do to provide outstanding customer service to those drivers. Steve is also a Certified Facility Manager in the State of Wisconsin.

Jodi Acheson, Manufacturing and Environmental Services Representative

Jodi has been with Republic since 1999 and primarily serves as a sales representative. She has an extensive knowledge of Special Waste permitting. Jodi has worked with many different municipalities over the years to provide all of their solid and special waste disposal needs.

Tom Marquis and Steve Nelson, Heavy Equipment Operators

Tom and Steve have been operating equipment at Kestrel Hawk for over 10 years. They have the utmost concern for customer service and safety. They are both Certified Site Operators in the State of Wisconsin.

Related Project Experience:

City of Racine

In 2006, Kestrel Hawk entered into an exclusive agreement with the City of Racine to accept their municipal solid waste. The City of Racine runs its own collection operations and delivers waste to the landfill. Kestrel Hawk also accepts the street sweepings from City of Racine, although that is a permitted special waste a different rate than the MSW. Kestrel Hawk takes an average of 2,000 tons per month from the City of Racine.

Contact Person: Mark Yehlen, Commissioner of Public Works
730 Washington Avenue, Room 303
Racine, WI 53403
(262) 636-9121

Five Star Services

Five Star is a company that offers equipment rental along with dumpster services. They take the majority of their dumpsters to Kestrel Hawk for disposal. Kestrel Hawk works closely with Five Star and allows them to store several empty containers on site for added convenience.

Contact Person: Jeff Knepper
Five Star Services
2740 Wisconsin Street
Sturtevant, WI 53177
(262) 554-7973

ASDA Enterprises, Inc.

ASDA is an independent full service waste collection company that delivers on average about 500 tons of waste per month into Kestrel Hawk. They operate Rear End Loaders, Front End Loaders and Roll Off trucks.

Contact Person: Andy Naber
33904 White Oak Drive
Burlington, WI
(262) 539-2086

Contract Details:

1. List of accepted materials:

It is our understanding that the scope of this proposal is for municipal solid waste disposal. As such, we can accept all municipal solid waste which by regulation cannot include the following:

- Hazardous Waste
- Lead Acid Batteries
- Waste Oil
- Yard Waste
- Boiler Waste
- Bimetal Containers
- Appliances
- Electronics
- Tires
- Bulk recyclables

2. Hours of Operation: Monday through Friday 7:00 to 4:00 PM. Exceptions can be made to open on Saturdays after holidays if needed.

3. Location of Landfill: Kestrel Hawk is located at 1989 Oakes Road, Racine, WI 53406.

4. Distance from 8600 Green Bay Road, Pleasant Prairie, WI: Kestrel Hawk is located 13 miles (straight up Green Bay Road) from 8600 Green Bay Road.
5. Limitations on Acceptance: Kestrel Hawk is not open on Saturdays, but is allowed to be if needed. Host agreements preclude us from operating on Sundays. On two occasions over the last 5 years we have had to shut down early due to uncontrollable high winds. We do not have any volume restrictions.
6. Tipping Fee for Material: Kestrel Hawk is offering an initial rate of \$41.25 per ton for MSW delivered to the site by Pleasant Prairie. This rate includes the \$13/ton State of Wisconsin tax. This rate would be in effect from August 1, 2015 through October 1, 2015. Beginning October 2, 2015 (and each October 2nd through the duration of the contract) the rate would increase by the annual percentage increase, if any, in the Consumer Price Index published by the Bureau of Labor Statistics of the United States Department of Labor, All Items, for urban wage earners and clerical workers 1982-1984=100 during the 12 previous month (End of September to End of September). If there is no increase, the rate would remain the same. Bid is contingent upon parties negotiating a mutually acceptable agreement.
7. Any Other Additional Applicable Fees or Taxes: Republic Services adds a \$3.25 fee on each invoice.

8. Insurance Requirements:

Workers' Compensation

Coverage A	Statutory
Coverage B-Employers Liability	\$1,000,000 each Bodily Injury by Accident
	\$1,000,000 policy limit Bodily Injury by Disease
	\$1,000,000 each occurrence Bodily Injury by Disease
	This coverage is required if Contractor employs persons on either a full or part-time basis to perform services

Automobile Liability

Bodily Injury/Property Damage	\$2,000,000
Combined Single Limit	Coverage is to apply to all owned, non-owned, hired and leased vehicles (including trailers)

Commercial General Liability

Bodily Injury/Property Damage	\$2,000,000 each occurrence
Combined Single Limit	\$2,000,000 general aggregate

The insurance above must be endorsed to include the Company as an Additional Insured on all policies except Workers Compensation and Professional Liability and must also provide a Waiver of Subrogation on Workers Compensation. Any combination of primary and excess/umbrella policies may be utilized to satisfy the required limits of liability.

The Certificate Holder must be made out to our proper legal entity name as follows:
Republic Services, Inc. and all of its subsidiaries

9. **Period of Contract:** Republic Services is proposing a 3 year contract. If selected, it is understood that a negotiated service agreement will be negotiated and signed and will be based on the above mentioned initial rate and annual increases.
10. **Options for Renewal of 3 Additional Years:** Republic Services would be open to entering into another 3 year contract at the conclusion of this contract if both parties mutually agree at that time.
11. **Years of Landfill Life:** Current estimates show that Kestrel Hawk landfill has about 9 years of remaining site life.
12. **Limitations or Weight Restrictions at the Landfill:** Kestrel Hawk has no tonnage restrictions. Weight restrictions are based on applicable DOT regulations and should be followed by the hauler. In addition, if Republic Services is the successful bidder and a service agreement is negotiated, Republic Services will be asking for an exclusive agreement where all of Pleasant Prairie's MSW waste is delivered to Kestrel Hawk.
13. **Any License Requirements for Interstate Trucking:** Republic Services will not be providing any trucking services.
14. **Types and Frequency of Reports Submitted to the Village of Pleasant Prairie:** Republic Services can provide detailed tonnage reports at any time the Village requests through our easy to use TRUX gate management system.
15. **Billing Frequency:** Republic Services sends out invoices twice per month, on the 15th and end of month.
16. **Requirements of the Village such as any Public Notifications:** None known at this time, other than Republic's previously mentioned requirement that we receive all of Pleasant Prairie's MSW under the agreement.
17. **Liabilities, if any, including Landfill Closure Cost:** Current closure cost bond for Kestrel Hawk is \$6,808,800. All required financial assurance mechanisms for Kestrel Hawk are up to date with the State of Wisconsin.

CONTRACTOR'S AFFIDAVIT

STATE OF WISCONSIN)

(SS.

_____ VILLAGE)

Ray Carter an office of
Republic Services of Wisconsin Limited Partnership

to-wit: I, Ray Carter, being first duly sworn in oath, says:

That Republic Services of WI, L.P. has no contractual relationships whatsoever, either direct or indirect, with any officer, agent, or employee of the

Owner: Village of Pleasant Prairie, nor has any officer, agent, or employee of the Owner any financial interest, either direct or indirect, present or prospective, absolute or conditional in the contract to which this bond pertains, as this affiant well knows.

Dated at 1:30 PM this 3^e day of June, 2015

Affiant [Signature]

Title General Manager

Subscribed and sworn to before me this 3 day of June, 2015

Catherine P. Long
Notary Public, State of Wisconsin



My Commission Expires: 9-17-15

VILLAGE OF PLEASANT PRAIRIE
RESPONDENT'S PROPOSAL
SIGNATURE PAGE AND LEGAL STATUS

THE UNDERSIGNED CERTIFY THAT THEY ARE AN OFFICIAL LEGALLY AUTHORIZED TO BIND THEIR FIRM AND TO ENTER INTO A CONTRACT SHOULD THE VILLAGE ACCEPT THIS PROPOSAL.

PROPOSAL BY: Republic Services of Wisconsin Limited Partnership
(Name of Firm)

LEGAL STATUS OF RESPONDENT: (Please check the appropriate box)

CORPORATION _____ STATE OF INCORPORATION Delaware
PARTNERSHIP LIST NAMES General Partner:
Republic Services of Wisconsin GP, LLC
Limited Partner:
Republic Services of Wisconsin, LP, LLC
DBA EXPLAIN Kestrel Hawk Recycling + Disposal

SIGNATURE OF RESPONDENT:

TITLE:

[Signature]

General Manager

ADDRESS:

CITY:

1989 Oaks Road

Racine

ZIP:

53406

TEL:

(262) 884-7081

Signed this 3rd Day of June 2015.

COPY

**Waste Management of Wisconsin, Inc.
Pheasant Run Recycling and Disposal Facility
19414 60th Street
Bristol, WI 53104**

RFP #15-SW-01-RB

for the

Village of Pleasant Prairie

Municipal Solid Waste Disposal

June 4, 2015

Waste Management of Wisconsin, Inc.
Pheasant Run Recycling and Disposal Facility
19414 60th Street
Bristol, WI 53104
262-857-7956
Peggy Slind, Account Manager, 262-250-8707, pslind@wm.com

Pheasant Run Recycling and Disposal Facility (RDF) is a 951 acre facility in Bristol, Wisconsin. This solid waste complex includes a landfill gas-to-energy facility generating enough power for approximately 9,000 homes. Pheasant Run RDF has 4.5 years of constructed site life, and 71 years of permitted site life. Municipal, commercial, and industrial solid wastes are accepted at the landfill. No hazardous waste is accepted.

Pheasant Run RDF accepts approximately 650 tons of waste per day for disposal.

Pheasant Run RDF is in compliance with state, federal and local permits and regulations and has an excellent operational and regulatory compliance history.

Waste Management of Wisconsin, Inc. has implemented a computer database system at Pheasant Run RDF called CYCLE. This system is specially designed to monitor environmental regulatory compliance. CYCLE systematically tracks all the regulatory requirements associated with the operation of the Pheasant Run RDF. Sources include Federal Laws, State Regulations, Company Directives, Permit Conditions, Local Agreements and site Plans. The system is an internal compliance program that will work as an electronic reminder to help the facility meet deadlines and maintain 100% environmental compliance. Internal company environmental audits are conducted on a regular basis to ensure environmental compliance.

Key site personnel include:

Steve Meyer, District Manager, 33 years with Waste Management
Daniel Otzelberger, Engineer, 28 years with Waste Management
Lucy Fox, Senior Landfill Operations Specialist, 32 years with Waste Management
Peggy Slind, Account Manager, 33 years with Waste Management

Waste Management's operations include 264 active solid waste facilities and 310 transfer stations. Pheasant Run RDF has the capacity and capabilities to provide municipal solid waste disposal for the Village of Pleasant Prairie.

Pheasant Run RDF is permitted to accept municipal solid waste, construction and demolition waste and non-hazardous special waste. The hours of operations are currently 7:00 am – 3:00 pm, closed on Saturday and Sunday. The facility is also closed on New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, and Christmas Day. The facility is open the Saturday following a holiday.

Pheasant Run RDF is 11.6 miles from 8600 Green Bay Road, Pleasant Prairie, WI. Trucks should not arrive via Highway K (60th Street) East of the landfill entrance, or Highway N (38th Street).

Waste Management provides disposal and recycling services to the following references:

City of Sheboygan	Village of Shorewood
David Biebel – Director of Public Works	Leeann Butschlick – Director of Public Works
2026 New Jersey Avenue	3801 N Morris Blvd
Sheboygan, WI 53081	Shorewood, WI 53211
920-459-3366	414-847-2653
Tony Knoeck, Waste Management Representative	Tony Knoeck, Waste Management Rep.

Tipping fee for material.

Year 1 of the agreement \$30.60 per ton - plus \$13.00 per ton Wisconsin taxes and fees

Year 2 of the agreement \$31.60 per ton - plus \$13.00 per ton Wisconsin taxes and fees

Year 3 of the agreement \$32.60 per ton - plus \$13.00 per ton Wisconsin taxes and fees

Any other additional applicable fees or taxes. Currently \$13.00 per ton will be charged for the State of Wisconsin solid waste taxes and fees. This rate will be adjusted to reflect any changes to the taxes and fees.

Insurance requirements if any. N/A

Period of contract. Initial term of the disposal agreement is 36 months.

Options for renewal of (3) additional years. The parties may, by mutual agreement, extend the term for another 36 months.

Years of landfill life. Pheasant Run RDF has 4.5 years of constructed site life, and 71 years of permitted site life.

Limitations or weight restrictions to or at the landfill. Trucks should not arrive via Highway K (60th Street) East of the landfill entrance, or Highway N (38th Street). Trucks must only use U.S. Highway 45, State Highway 50, State Highway 142 and that portion of County Highway K East of U.S. Highway 45 to the current facility gate.

Any licenses requirements for interstate trucking. Not applicable

Types and frequency of reports submitted to Village of Pleasant Prairie. Excel formatted reports can be generated as often as required.

Billing frequency and other pertinent financial information. Pheasant Run RDF invoices bi-monthly, on the 1st and 16th day of each month. Payment terms are net 30 days.

Requirements of the Village such as any public notifications. We are not aware of any public notification requirements that would apply to this work.

Liabilities, if any, including landfill closure costs. Waste Management of Wisconsin, Inc. (WMWI) would indemnify the Village for liability of negligent acts, violation of law or breach of agreement. WMWI has the resources available to cover landfill closure costs.

**VILLAGE OF PLEASANT PRAIRIE
RESPONDENT'S PROPOSAL
SIGNATURE PAGE AND LEGAL STATUS**

THE UNDERSIGNED CERTIFY THAT THEY ARE AN OFFICIAL LEGALLY AUTHORIZED TO BIND THEIR FIRM AND TO ENTER INTO A CONTRACT SHOULD THE VILLAGE ACCEPT THIS PROPOSAL.

PROPOSAL BY: Waste Management of Wisconsin, Inc.
(Name of Firm)

LEGAL STATUS OF RESPONDENT: (Please check the appropriate box)

CORPORATION STATE OF INCORPORATION Delaware

PARTNERSHIP LIST NAMES _____

DBA EXPLAIN _____

SIGNATURE OF RESPONDENT:

TITLE:



President

Thomas Beaulieu
ADDRESS:

CITY:

W132N10487 Grant Drive

Germantown, WI

ZIP:

TEL:

53022

262-250-8700

Signed this 29th Day of MAY 2015.

CONTRACTOR'S AFFIDAVIT

STATE OF WISCONSIN)

(SS.

_____ VILLAGE)

_____, an office of

to-wit: I, _____, being first duly sworn in oath, says:

That _____, has no contractual relationships whatsoever, either direct or indirect, with any officer, agent, or employee of the

Owner: Waste Management of Wisconsin^{Inc.} or has any officer, agent, or employee of the Owner any financial interest, either direct or indirect, present or prospective, absolute or conditional in the contract to which this bond pertains, as this affiant well knows.

Dated at Germantown, WI this 29th day of MAY, 2015

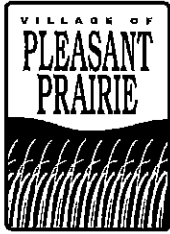
Affiant Thomas Beaulieu
Thomas Beaulieu

Title President

Subscribed and sworn to before me this 29th day of MAY, 2015

[Signature]
Notary Public, State of Wisconsin

My Commission Expires: 1/29/17



Office of the Director of Public Works
John Steinbrink Jr., P.E.

To: Michael Pollocoff
From: John Steinbrink Jr.
Subject: Single Stream Recycling Award of Contract
Date: June 9, 2015

On April 14, 2015, a request for proposal for Processing of Single Stream Recycling was issued to VendorNet where it was sent to 114 vendors. It was also advertised in the Kenosha News Legal Section for two weeks. In addition, the request for proposal was sent to 6 local recycling processing companies.

On Thursday, May 7th at 10:00 a.m. bids were publicly opened and read aloud. Two bids were received.

The proposals were evaluated based on annual tipping cost and travel cost to the recycling facility. Please see the attached bid analysis sheet.

Based on the attached analysis, Advanced Disposal offers the lowest cost for tipping. Advanced Disposal has been under contract for the Village's single stream recycling processing since 2009.

I recommend the contract be awarded to Advanced Disposal for processing of single stream recycling.

Single Stream Recycling Analysis

3-Jun-15

Advanced Disposal

Tipping costs		Year 1
Historical Compensation Value	\$	6,398.00
Transportation costs		
Village Labor	\$	39.48
Village Fleet	\$	40.40
Costs per hour	\$	79.88
Transportation Time		
Hours/ trip		0.75
Trips/year		780
Hours per year		585
Trip Cost		
Cost per year	\$	46,729.80
Estimated Annual Costs		
Tipping and Trip costs	\$	40,331.80

Johns Disposal

Tipping costs		Year 1
Historical Compensation Value	\$	18,677.00
Transportation costs		
Village Labor	\$	39.48
Village Fleet	\$	40.40
Costs per hour	\$	79.88
Transportation Time		
Hours/ trip		1.75
Trips/year		780
Hours per year		1365
Trip Cost		
Cost per year	\$	109,036.20
Estimated Annual Costs		
Tipping and Trip costs	\$	90,359.20



**REQUEST FOR PROPOSAL
RFP #15-SW-02**

VILLAGE OF PLEASANT PRAIRIE

Processing of Single Stream Recycling

April 14, 2015

ISSUED BY:

**Village of Pleasant Prairie, Wisconsin
Department of Public Works
8600 Green Bay Rd
Pleasant Prairie, WI 53158**

REQUEST FOR PROPOSALS

Notice is hereby given that proposals will be received by the Village of Pleasant Prairie ("Village"), Wisconsin for:

Processing of Single Stream Recycling RFP #15-SW-02

Proposals shall be delivered or mailed to: John Steinbrink Jr. P.E., Director of Public Works, Village of Pleasant Prairie, 8600 Green Bay Rd, Pleasant Prairie, WI, 53158.

Proposals will be accepted until:

Date: Thursday May 7, 2015

Time: 10:00 A.M. (CST)

Proposals submitted after the above-noted due date and time will be rejected. Respondents accept all risks of late delivery of mailed submittals regardless of fault.

The Village reserves the right to reject any and all submittals and to waive irregularities and informalities in the submittal and evaluation process. This RFP does not obligate the Village to pay any costs incurred by respondents in the preparation and submission of their statement of qualifications. Furthermore, the RFP does not obligate the Village to accept or contract for any expressed or implied services.

It is the policy of the Village of Pleasant Prairie to assure that no person shall, on the basis of race, color, religion, sex or national origin, as provided by Title VI of the Civil Rights Act of 1964, be excluded from participation in, be denied the benefits of, or be otherwise discriminated against under any of its federally funded programs and activities.

The Village is committed to a program of equal employment opportunity regardless of race, color, religion, sex, age, nationality, disability, or sexual orientation. The successful consultant must comply with the Village of Pleasant Prairie's equal opportunity requirements.



John Steinbrink Jr, P.E.
Director of Public Works

April 10, 2015
Date

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1) PURPOSE & INTENT

This solicitation involves **Processing of Single Stream Recycling** for the Village of Pleasant Prairie.

The Village collects recycling daily with fully automated collection vehicles. The successful firm will be responsible for processing the collected single stream recycling material. The Village will only consider hauling material with collection vehicles directly to a remote transfer or processing site within 30 miles of 8600 Green Bay Road, Pleasant Prairie WI.

The Village of Pleasant Prairie will distribute Request for Proposal requests. A Village of Pleasant Prairie Evaluation and Selection Committee will evaluate the RFP's submitted and establish a short list. Those firms selected on the short list will be ranked. The Village of Pleasant Prairie will then discuss approach, project schedule and resources with the highest ranked firm. Negotiations of fee's, terms and conditions will follow. In the event that the negotiations with the highest ranked firm are unsuccessful, negotiations will then proceed to the second highest ranked firm.

2) SCOPE OF SERVICES/PROJECT REQUIREMENTS

A) DEFINITIONS

- (1) "VILLAGE" means the Village of Pleasant Prairie.
- (2) "COUNTY" means Kenosha County.
- (3) "PROJECT" means the Processing of Single Stream Recycling

B) GENERAL

- (1) The Services under this contract shall consist of all tasks necessary or incidental to accomplish the PROJECT.
- (2) The Respondent shall furnish all services and labor necessary to conduct and complete the services, and shall furnish all materials, equipment, supplies, and incidentals other than those designated in writing as to be furnished by the VILLAGE.
- (3) The services under this contract shall be performed in accordance with generally accepted standards.
- (4) The services shall comply with the applicable State and Federal laws and regulations consistent with the scope of project.

- C) REQUIREMENTS - This project will require the selected firm to provide the following:
Processing of single stream recycling as outlined in this document.

3) GENERAL INSTRUCTIONS AND FORMAT

The evaluation and selection of a consultant will be based on the information submitted in the request for proposal plus references and any required interviews/presentations. Consultants shall respond clearly and completely to all requirements. Failure to respond to any of the requirements in the RFP may be the basis for rejecting a submittal. The submitted qualifications shall be typed and submitted on 8.5" x 11" paper and bound securely. Tables/Graphs/Charts and other exhibits may be submitted on 11" x 17" paper, properly folded to an 8.5" x 11" size and bound securely within the document. There is a 12 total page limit to the responses.

4) Single Stream Recycling Volumes and Breakdown

The Village currently has 20,004 residents and is serviced by in-house collection vehicles with fully automated collection. The average annual residential solid waste and recyclables collected over the past 4 years is illustrated in the table below.

	2011	2012	2013	2014
Recycling (tons)	2,144	2,122	2,111	2,117
Garbage (tons)	5,951	6,009	6,213	6,445

Grade	05/15/14 Sample
News	54.24%
OCC	9.93%
Glass	22.53%
Mixed Paper	0.38%
Tin	2.60%
1-7 Plastic	5.38%
Aluminum	1.31%
Residual	3.63%
Total	100.0%

As of January 1, 2015 there are 7,348 recycling accounts in the Village of Pleasant Prairie.

5) SUBMITTAL PROCESS & REQUIREMENTS

Qualifications are sought from firms with recognized expertise in the **Processing of Single Stream Recycling**. Items A through I shall constitute the technical proposal; Item J shall constitute the historical compensation value.

Proposals shall include the following:

Technical and Qualification Proposal:

- A) Firm name, address, telephone number and contact person. A two-page statement of interest and qualifications for this project.
- B) Brief history of the firm.
- C) A brief (maximum four-page) project understanding description.
- D) Discussion of firm's specific abilities and expertise to provide the required services.
- E) Key personnel proposed as project team members.
- F) Clearly identify sub-consultants, if proposed, with similar information.
- G) Examples of specific knowledge, expertise and project experience related to this type of project.
- H) References of other owners for which the firm has provided similar services. Reference information **must** include:
 - (1) Name of owner.
 - (2) Project name.
 - (3) Brief description of firm's involvement.
 - (4) Contact person.
 - (5) Project/Contact address.
 - (6) Project/Contact telephone number.
 - (7) Firm's key personnel assigned to the referenced project.
- I) Contract Details
 - (1) List of accepted recycling materials.
 - (2) Hours of operation accepting delivery of Single Stream Recycling Material.
 - (3) Location of transfer site or processing facility.
 - (4) Distance from 8600 Green Bay Rd Pleasant Prairie, WI.
 - (5) Any limitations for processing of recycling material- this would include holidays, weekends, weather, volume or weight, etc.
 - (6) Tipping fee for Single Stream recycling material.
 - (7) Any other additional applicable fees or taxes.
 - (8) Frequency of material breakdown (required two times per year).
 - (9) Terms and rates for payment of recyclable material.
 - (10) Schedule for sharing of proceeds from sale of recycling material.
 - (11) Insurance requirements, if any.
 - (12) Period of contract (minimum of 3 years).
 - (13) Options for renewal of three (3) additional years.

- (14) Limitations or weight restrictions to or at the transfer/processing site.
- (15) Any license requirements for Interstate trucking, if applicable.
- (16) Types and frequency of reports submitted to Village of Pleasant Prairie.
- (17) Billing frequency and other pertinent financial information.
- (18) Requirements of the Village such as public notifications.
- (19) Liabilities, if any.

J) Historical Compensation Value (TO BE PROVIDED IN SEPARATE SEALED ENVELOPE)

- (1) Show in detail what revenue/costs Pleasant Prairie would have been collected/charged for 2014. Use the May 15, 2014 breakdown and 2014 collection volumes for your calculation.
- (2) Assume equal average monthly volumes for 2014.

Firms must submit the following items:

- One original (clearly labeled as such) PLUS three copies of all materials required for acceptance of their qualifications (**Proposal Items A through I**), **Completed Respondent's Proposal Signature and Legal Status**, **Completed Contractors Affidavit**.
- One copy of the historical compensation value (**Item J**) in a separate sealed envelope.

On or before 10:00 a.m. on Thursday, May 7th, 2015, via U.S. Mail, UPS, Fed Ex, DHL, Airborne, etc. to:

Village of Pleasant Prairie
Attn: John Steinbrink Jr., P.E.
Director of Public Works
8600 Green Bay Rd.
Pleasant Prairie, WI 53158

The Village of Pleasant Prairie does not accept facsimile or email submitted proposals. A firm, if it so chooses, may hand-deliver its proposal package on or before the date and time listed above. All submittals must be date-stamped by the Village of Pleasant Prairie. Submittals received after 10:00 a.m. on Thursday, May 7th, 2015, will not be accepted.

The complete proposal package shall be plainly marked as shown below. A complete proposal package shall consist of two individual envelopes that are both placed within a single complete proposal package. The **first envelope** shall contain the proposal transmittal letter and the firm's technical and qualification proposal (original plus three copies), without the Historical Compensation Value. A **second sealed envelope** shall also include the proposal transmittal letter and the Historical Compensation Value (one copy).

The single complete proposal package and each of the two individual envelopes shall be marked as shown below.

All submittals must be packaged, sealed, and clearly labeled to show the following information on the outside of the package:

Firm's Name and Address
**"Village of Pleasant Prairie – RFP#15-SW-02
 Processing of Single Stream Recycling
 RFP Due Date of Thursday, May 7th, 2015**

(Label as appropriate: COMPLETE PACKAGE, TECHNICAL AND QUALIFICATION ENVELOPE, or HISTORICAL COMPENSATION VALUE ENVELOPE)

*It is not the intent of this RFP to solicit an **overly long response**, but it is important that the firm's experience/expertise is adequately described. It will, for example, be much more useful to address abilities and expertise directly comparable to this project than to include an exhaustive list of all projects completed by the firm. Village staff will review the submitted proposals. The selected firm will meet with the Village to prepare a contractual agreement between the Village and the firm after the final selection is complete.*

6) SCHEDULE

Listed below are estimated dates and times of actions related to this RFP. In the event that the Village finds it necessary to change any of the specific dates and times, it will do so by issuing amendments to this RFP. Failure by the Village to issue amendments to this schedule will not invalidate this selection process.

RFP SCHEDULE:

EVENT	DATE
RFP Release	April 14, 2015
Questions in Writing (If any) Due	April 30, 2015
RFP Responses Due	Thursday May 7, 2015
Internal Village Review of RFP Responses	Week of May 11, 2015
Scope and Contract Negotiation	Week of May 18, 2015
Contract Recommendation to the Village Board	May 22, 2015
Village Board Approval	June 1, 2015
Contract Begins	August 3, 2015

7) EVALUATION COMMITTEE, SELECTION & AWARD PROCESS

A) Evaluation committee. The Village RFP Evaluation & Selection Committee will consist of:

- John Steinbrink, Jr., Public Works Director
- Joe Middleton, Streets Foreman
- Jesse Houle, Village Construction Manager

The evaluation committee members have been selected because of their special expertise and knowledge of the service(s) and/or product(s) that are the subject of this RFP.

B) The selection committee will evaluate the proposals utilizing the proposal evaluation criteria (except compensation). The Village, because of time constraints and depending upon the thoroughness of the proposals, may at its sole option award a contract based upon the initial proposal submittal. Do not assume there will be an opportunity for submittal of additional information. Submit your proposal as if it were your "best and final offer."

C) If the Village intends to hold interviews, the Proposers to be interviewed will be contacted to schedule an interview. The interview will further evaluate the Proposer in the following categories: (1) Project Manager/Key Personnel; (2) Overall Qualification of Firm/Team; and (3) Project Approach. The point values or ratings for these categories are subject to change based upon the interview.

D) Final evaluation:

Upon completion of any interviews/presentations by the respondents, the Evaluation and Selection Committee will review its evaluations and make any necessary ranking adjustments based on the information obtained in the interview/presentation, possible reference checks, and any other pertinent respondent information.

E) Right to reject qualifications and negotiate contract terms:

The Village reserves the right to reject any and all submittals. The Village reserves the right to negotiate the terms of the contract, including the award amount, with the selected consultant prior to entering into a contract.

F) Award of contract:

The Village reserves the right to make an award without further discussion of the submittals. The firm selected as the apparent successful firm will be expected to enter into a contract with the Village. The foregoing should not be interpreted to prohibit either party from proposing additional contract terms and conditions during the negotiations of the final contract. If the selected firm fails to sign the contract within ten (10) business days of delivery of the final contract, the Village may elect to negotiate a contract with the next-highest ranked firm. The Village shall not be bound, or in any way obligated, until both parties have executed a contract. No party may incur any chargeable costs prior to the execution of the final contract. Following consultant selection, the successful consultant shall prepare a proposal and scope of work for review by the Village. Once the Village and consultant have reached an agreement on the scope of services, a final contract will be prepared by the Village.

8) MISCELLANEOUS

A) Questions

Questions regarding this RFP may be directed to John Steinbrink Jr. P.E., Director of Public Works, via e-mail at jsteinbrink@plprairiewi.com. Unauthorized contact regarding this RFP with other Village employees may result in disqualification from consideration in the proposal. Any oral communications will be considered unofficial and non-binding on the Village, unless it is followed by a written statement from the Village.

B) Rejection of Submittals

The Village reserves the right to reject any and all submittals and to waive irregularities and informalities in the submittal and evaluation process. This RFP does not obligate the Village to pay any costs incurred by respondents in the preparation and submission of their qualifications. Furthermore, this RFP does not obligate the Village to accept or contract for any expressed or implied services.

C) Withdrawal of Qualifications

Consultants may withdraw a submittal, in writing, at any time up to the proposal due date and time. The written withdrawal notice must be received by the Village Director of Public Works. The notice must be signed by an authorized representative of the consultant.

D) Incurring Costs

The Village is not liable for any costs incurred by consultants in responding to this RFP.

E) Proprietary Proposal Material

Any proprietary information revealed in the submittal should be clearly identified as such by the respondent.

F) Terms

There is no expressed or implied obligation of the Village to reimburse firms for any costs incurred in preparing submittals in response to this request. The Village reserves the right to reject any and all submittals and to modify the scope of services. The Village further reserves the right to retain all submittals and to use any idea in a submittal regardless of whether that submittal is selected.

G) Signatures

RFPs shall be signed by one of the legally authorized officers of the submitting firm/corporation. If awarded the contract, the contract shall also be executed by said officer.

H) Contract Negotiation

The Village reserves the right to negotiate all elements of the submittals, proposals, terms and conditions, and/or scope of work as part of the contract negotiation process prior to any formal authorization of the contract by the Village.

I) Equal Opportunity Employment

The successful consultant(s) must comply with the Village equal opportunity requirements. The Village is committed to a program of equal employment opportunity regardless of race, color, religion, sex, age, nationality or disability.

J) Title VI

It is the Village's policy to assure that no person shall, on the basis of race, color, religion, sex or national origin, as provided by Title VI of the Civil Rights Act of 1964, be excluded from participation in, be denied the benefits of, or be otherwise discriminated against under any of its federally funded programs and activities.

K) Insurance Requirements

The selected firm shall maintain insurance that is sufficient to protect the firm's business against all applicable risks. Standard requirements may be negotiated if it is in the best interest of the Village.

L) Non-Endorsement

As a result of the selection of a firm to supply products and/or services to the Village, firm agrees to make no reference to the Village in any literature, promotional material, brochures, sales presentation or the like without the express written consent of the Village.

M) Non-Collusion

Submittal and signature of a statement of qualifications swears that the document is genuine and not a sham or collusive, and not made in the interest of any person not named, and that the consultant has not induced or solicited others to submit a sham offer, or to refrain from proposing.

N) Compliance with Laws & Regulations

In addition to nondiscrimination and affirmative action compliance requirements previously listed, the consultant or consultants ultimately awarded a contract shall comply with Federal, State and local laws, statutes and ordinances, and industry standards relative to the execution of the work. This requirement includes, but is not limited to, protection of public and employee safety and health; environmental protection; waste reduction and recycling; the protection of natural resources; permits; fees; taxes; and similar subjects.

O) Public Records

Under Wisconsin state law, the documents (including but not limited to written, printed, graphic, electronic, photographic or voice mail materials and/or transcriptions, recordings or reproductions thereof) submitted in response to this RFP (the "documents") become a public record upon submission to the Village, subject to

mandatory disclosure upon request by any person, unless the documents are exempted from public disclosure by a specific provision of law. If the Village receives a request for inspection or copying of any such documents it will promptly notify the person submitting the documents to the Village (by U.S. mail and by fax if the person has provided a fax number) and upon the written request of such person, received by the Village within five (5) days of the mailing of such notice, will postpone disclosure of the documents for a reasonable period of time as permitted by law to enable such person to seek a court order prohibiting or conditioning the release of the documents. The Village assumes no contractual obligation to enforce any exemption. The Village does not accept any responsibility for agreements, contracts or purchase orders issued by other public agencies to the consultant. Each public agency accepts responsibility for compliance with any additional or varying laws and regulations governing purchase by or on behalf of the public agency. The Village accepts no responsibility for the performance of the consultant in providing goods and/or services to other public agencies, nor any responsibility for the payment price to the consultant for other public agency purchases.

P) Clarification and/or Revisions of this RFP:

Revisions to this RFP will only be made by an official written amendment issued by the Village. In order to be eligible to receive amendments to this RFP, all respondents are responsible to notify the Village of its official contact person, address and email address. All amendments/clarifications will be forwarded to the respondents of record.

**VILLAGE OF PLEASANT PRAIRIE
RESPONDENT'S PROPOSAL
SIGNATURE PAGE AND LEGAL STATUS**

THE UNDERSIGNED CERTIFY THAT THEY ARE AN OFFICIAL LEGALLY AUTHORIZED TO BIND THEIR FIRM AND TO ENTER INTO A CONTRACT SHOULD THE VILLAGE ACCEPT THIS PROPOSAL.

PROPOSAL BY: _____
(Name of Firm)

LEGAL STATUS OF RESPONDENT: (Please check the appropriate box)

CORPORATION _____ STATE OF INCORPORATION _____

PARTNERSHIP _____ LIST NAMES _____

DBA _____ EXPLAIN _____

SIGNATURE OF RESPONDENT:

TITLE:

ADDRESS:

CITY:

ZIP:

TEL:

Signed this _____ Day of _____ 20____.

CONTRACTOR'S AFFIDAVIT

STATE OF WISCONSIN)

(SS.

_____ VILLAGE)

_____, an office of

to-wit: I, _____, being first duly sworn in oath, says:

That _____, has no contractual relationships whatsoever, either direct or indirect, with any officer, agent, or employee of the

Owner: _____; nor has any officer, agent, or employee of the Owner any financial interest, either direct or indirect, present or prospective, absolute or conditional in the contract to which this bond pertains, as this affiant well knows.

Dated at _____ this _____ day of _____, 20__

Affiant _____

Title _____

Subscribed and sworn to before me this _____ day of _____, 20__

Notary Public, State of Wisconsin

My Commission Expires: _____



7311 Omega Circle
Franksville, WI 53126
262-895-1053

www.johnsdisposal.com

email: norwayoffice@johnsdisposal.com

DISPOSAL SERVICE, INC.

May 6, 2015

The Village of Pleasant Prairie
Attn: John Steinbrink Jr. P.E
Director of Public Works
8600 Green Bay Rd.
Pleasant Prairie, WI 53158

Dear John,

Thank you for the opportunity to submit a proposal for single stream recycle processing. Our new processing facility on Hwy 45 in Racine County has been up and running since January of 2012. I encourage you to contact a few of the references listed in our proposal. Please note a few key features to our proposal:

- Since Johns Disposal is a processor, a processing fee will be charged per ton. Johns does not charge any tipping fees, generator tax, environmental fees or fuel surcharges of any kind.
- Based on our historical compensation value for 2014, Johns Disposal would have paid the Village \$18,677.07 for 2,117 tons of single stream recycling delivered to our facility.
- Schedule A is attached to our technical proposal. Schedule A provides a transparent classification of each commodity, total tons and value per ton. The Village will receive a schedule each month with updated market prices for the month for each commodity.
- The provided single stream breakdown included plastics 1-7 at 5.38%. We chose to divide this category into multiple categories as shown in Schedule A. The multiple categories of 1-7 plastics when combined, remains at a total of 5.38% of the overall stream.
- Being in both the hauling and processing business, we've found that new diesel trucks emission systems operate better when "run" at high RPM for an extended period of time each day. Ultimately, the drive from Pleasant Prairie to our facility on Hwy 45 would be good for your new trucks emission system.

Thank you again for considering our proposal. Feel free to contact me toll free at 888-473-4701 with any questions, to set up a meeting, or if you would like to schedule a tour of our facility.

Sincerely,

Dan Jongetjes
Municipal Sales



Whitewater & Franksville, WI

PROPOSAL

May 7, 2015

Prepared for:
The Village of Pleasant Prairie
Attn: John Steinbrink Jr. P.E
Director of Public Works
8600 Green Bay Rd.
Pleasant Prairie, WI 53158

Prepared by:
Johns Disposal Service, Inc.
7311 Omega Circle
Franksville, WI 53126
262-473-4700

COMPANY BACKGROUND

John Jongetjes founded Johns Pick-Up Service in 1969. The business incorporated in 1992 as Johns Disposal Service, Inc. The Jongetjes family continues to own and operate the company. Cities, towns and villages are serviced in Dane, Kenosha, Jefferson, Milwaukee, Racine, Rock, Walworth and Waukesha counties. This growth is due to the commitment to provide the highest quality service and our inclusive recycling program. Johns Disposal has been processing single stream recycling for the past 15 years.

COMPANY FACILITIES

Company facilities located in the City of Whitewater at 107 Hwy U in Jefferson County:

- A fully staffed office.
- A 20,000 square foot Single-Stream recycle processing facility.

Company facilities located at 7311 Omega Circle in the Town of Norway in Racine County:

- A fully staffed office.
- A 57,000 square foot Single-Stream recycle processing facility.

COMPANY PERSONNEL

Johns currently provides over 175 full time positions. The credit for our success goes to our dedicated, hard working employees. We are proud of the job that they do for our customers.

The officers/managers of our corporation are as follows:

President	Brian Jongetjes	Employed since 1980
Vice President	Ron Jongetjes	Employed since 1982
Secretary	Dan Jongetjes	Employed since 2004
Treasurer – CFO	Duane Schuab - CPA	Employed since 2012
Sales Manager	Wayne Arnold	Employed since 1989
Safety Manager	Brent Flikkema	Employed since 1995
Fleet Manager	Mike Redenius	Employed since 1992
Office Manager	Melissa Fuller	Employed since 2010

EXPERIENCE

Johns Disposal has been in the recycle processing business since the mid 90's. We implemented our first single-stream processing system in our Whitewater facility in 2000. In 2012 we added a second single-stream processing facility in Racine County. Our Racine County facility is approximately 28 miles from the Pleasant Prairie DPW. Being invested in recycle processing for 20+ years demonstrates our commitment, expertise and passion for the processing business.

Johns Disposal works with municipalities similar to Pleasant Prairie. The City of Racine (Racine County) and the City of Watertown (Jefferson County) both direct haul to our processing facilities with City owned trucks. We understand the importance of providing efficiencies, which will allow trucks to maneuver through our facilities in a timely manner. We work to communicate with municipalities regarding audits of materials and monthly tonnage reporting.

Johns Disposal also processes single stream recycling for national haulers such as Waste Management and Advanced Disposal.

SAFETY

Johns Disposal is committed to safety. We realize that the success of our business begins with safety. A safe workplace includes a diverse range of factors such as regular employee training, vehicle maintenance and vehicle safe technology.

PROJECT UNDERSTANDING

Johns Disposal understands the overall obligation of the project. Job obligations include processing of single-stream recycling, marketing and sale of all processed materials. Johns Disposal will also be responsible for direct communication with Village staff regarding recycle tonnage reporting, material audits and monthly rebates.

Johns Disposal has sufficient capacity for the Village of Pleasant Prairie's single stream recycling. Our Franksville facility processes single stream recycling at a rate of 25 tph (tons per hour). Our current daily inbound tonnage averages from 180 to 200 tons per day.

REFERENCES

1. The City of Racine

Contact: Thomas Eeg – Assistant commissioner of Public Works (262)-636-9123

History: Jan 2013 – Present

Description: City delivered single-stream recycling to our Franksville facility

Address: 730 Washington Avenue, Racine WI 53403

2. The City of Watertown

Contact: Rick Schultz – Street superintendent (920)-262-4080

History: 2009 – Present

Description: City delivered single-stream recycling to our Whitewater facility

Address: 106 Jones Street, Watertown WI 53094

3. The City of Wauwatosa

Contact: Bill Porter – Director of Public Works (414)-479-8933

History: Aug 2012 through Dec 2012.

Description: City delivered single-stream recycling to our Franksville facility

Address: 11100 W Walnut Road, Wauwatosa WI 53226

4. Groot Disposal

Contact: Jon Groot – (800)-244-1977

History: July 2013 – October 2013

Description: Semi transferred single-stream from Illinois to our Franksville facility

Address: 2500 Landmeier Road, Elk Grove Village IL 60007

5. Waste Management

Contact: Peggy Slind – Account Manager (262)-250-8707

History: 2014 – Present

Description: Delivered single-stream recycling to our Franksville and Whitewater facilities

Address: W132 N10487 Grant Drive, Germantown WI 53022

6. Advanced Disposal

Contact: Floyd Leo – Landfill Sales (920)-387-0628

History: March 2015 – Present

Description: Delivered single-stream recycling to our Franksville and Whitewater facilities

Address: N7296 Highway V, Horicon WI 53032

CONTRACT DETAILS

(1) Listed below are accepted recycling materials:

- a. Newspaper (ONP)
- b. Mixed paper, junk mail, magazines, brown paper bags, and non-corrugated cardboard
- c. Corrugated cardboard (OCC)
- d. Glass (clear, brown, and green) bottles and jars
- e. Aluminum
- f. Steel (or "tin") cans including aerosol cans
- g. Plastic containers, numbers 1 through 7
- h. Mixed bulky rigid plastic containers

(2) Hours of operation accepting delivery of Single-Stream recycling material:

Our facility is open Monday through Friday 6:45am to 5pm. We maintain a mailbox system for early and late loads. Drivers must manually complete a weight ticket. Our MRF is open for early loads at 5am and for late loads until 9pm Monday through Saturday.

(3) Location of transfer site or processing facility:

Our processing facility is located at 7311 Omega Circle, Franksville WI (Town of Norway) Racine County, WI 53126. Just off Hwy 45 between 6 and 7 mile roads. Transportation is not included. This proposal is for single stream recycling delivered to our processing facility.

(4) Distance from 8600 Green Bay Rd Pleasant Prairie, WI:

28 miles

(5) Any limitations for processing of recycling material – including holidays, weekends, weather, volume or weight, etc:

We are closed on legal holidays. Our mailbox system is available on all legal holidays. We have no limitations regarding weather, volume or weight.

(6) Tipping fee for Single Stream recycling material:

Johns Disposal does not charge a tipping fee. Since we are a processor, we charge a processing fee of \$49 per ton. Our processing fee will increase annually by CPI.

(7) Any other additional applicable fees or taxes:

Johns Disposal will not charge any additional fees or taxes.

(8) Frequency of material breakdown:

We will perform a material audit (2) two times per year.

(9) Terms and rates for payment of recyclable material:

Market value for each recoverable commingle material type will be based on the month's first posted price using current average commodity pricing from the following market index: www.recyclingmarkets.net. Commingled (plastic, aluminum and metal) values based on Secondary Materials for Chicago (Midwest/Central) pricing.

Fiber (paper and cardboard) values will be based on the posted OBM PPI Pulp&Paper Week. High side of the Chicago (Midwest/Central) pricing will be applied. Johns Disposal's processing facility produces a mixed paper grade.

Glass will be fixed at \$-5.00 per ton for the first year of the agreement. The \$-5.00 per ton is subject to an annual CPI increase. Reliability of glass processors in Wisconsin is very limited. In the event of glass processors not accepting glass, Johns Disposal reserves the right to increase the glass rate to the rate of residue for the duration that glass is landfilled.

Residue will be fixed at \$-34.50 per ton for the first year of the agreement. The \$-34.50 per ton is subject to an annual CPI increase.

Johns Disposal will pay the Village within 30 days of the end of each month for all tons received in the prior month.

(10) Schedule for sharing of proceeds from sale of recycling material:

Payments for each recoverable material type will be based on 80% of the value of the material less the cost of processing. Schedule A is attached to our proposal for reference.

(11) Insurance requirements, if any:

General liability – no special requirements.

(12) Period of contract (minimum of 3 years):

The term of this agreement will be 3 years.

(13) Options for renewal of three (3) additional years:

A (3) three year renewal option will be available.

(14) Limitations or weight restrictions to or at the transfer/processing site:

There are no limitations or weight restrictions that apply.

(15) Any license requirements for interstate trucking, if applicable:

No license requirements apply.

(16) Types and frequency of reports submitted to Village of Pleasant Prairie:

A tonnage report will be submitted to the Village each month. Additionally, a payment schedule will be provided each month providing sharing of proceeds for each recoverable material. Additional reports are available upon the request of the Village.

(17) Billing frequency and other pertinent financial information:

Billing and financial information will not be required. Johns Disposal will be responsible for all reporting and payment to the Village for all tons of single-stream recycling delivered to our processing facility.

(18) Requirements of the Village such as public notifications:

Johns Disposal has no public notification requirements.

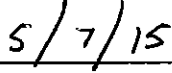
(19) Liabilities, if any:

In low market times, Johns Disposal will not charge the Village if the cost of processing exceeds the rebate value of the material. Therefore, the Village will never pay Johns Disposal for services provided.

This agreement will begin on August 3, 2015 and end on August 2, 2018.



Dan Jongetjes – Municipal Sales Manager



Date

Johns Recycling Report - Schedule A

Single Stream	Breakdown %	Tons	Price for April	Value	
Mixed Paper	54.620%	96.36	\$35.00	\$3,372.62	OBM PPI Pulp&Paper Week Chicago - High side
Cardboard	9.930%	17.52	\$65.00	\$1,138.70	OBM PPI Pulp&Paper Week Chicago - High side
#1 Plastic	1.530%	2.6992	\$250.00	\$674.81	Avg. Price recyclingmarkets.net Chicago
#2 Natural HDPE	1.100%	1.94	\$610.00	\$1,183.78	Avg. Price recyclingmarkets.net Chicago
#2 Colored HDPE	1.000%	1.76	\$590.00	\$1,040.88	Avg. Price recyclingmarkets.net Chicago
#3-7 Plastic	1.000%	1.76	\$60.00	\$105.85	Avg. Price recyclingmarkets.net Chicago
Rigid Plastic	0.750%	1.32	\$0.00	\$0.00	Avg. Price recyclingmarkets.net Chicago
Tin Cans & Scrap Metal	2.600%	4.59	\$90.00	\$412.82	Avg. Price recyclingmarkets.net Chicago
Al Cans	1.310%	2.31	\$1,430.00	\$3,304.88	Avg. Price recyclingmarkets.net Chicago
Residual	3.630%	6.40	-\$34.50	-\$220.94	Fixed rate
Glass	22.530%	39.75	-\$5.00	-\$198.74	Fixed rate
Total Tons	100.000%	176.42		\$10,814.66	
80% value of single stream				\$8,651.73	
Recycle Processing Cost	176.42		\$49.00	\$8,644.58	

\$7.15	Total Due
\$0.04	Payment per ton

Total Tons
176.42

Report completed by:
 Johns Disposal Service, Inc.
 PO Box 329
 Whitewater, WI 53190
 262 473 4700



Village of Pleasant Prairie - April 2015

**Note that current market prices are very low. Johns Disposal will not charge the Village if the cost of processing exceeds the rebate value of the material. Therefore, the Village will never pay Johns Disposal for services provided.

VILLAGE OF PLEASANT PRAIRIE
RESPONDENT'S PROPOSAL
SIGNATURE PAGE AND LEGAL STATUS

THE UNDERSIGNED CERTIFY THAT THEY ARE AN OFFICIAL LEGALLY AUTHORIZED TO BIND THEIR FIRM AND TO ENTER INTO A CONTRACT SHOULD THE VILLAGE ACCEPT THIS PROPOSAL.

PROPOSAL BY: Johns Disposal Service, Inc.
(Name of Firm)

LEGAL STATUS OF RESPONDENT: (Please check the appropriate box)

CORPORATION STATE OF INCORPORATION _____

PARTNERSHIP _____ LIST NAMES _____

DBA _____ EXPLAIN _____

SIGNATURE OF RESPONDENT:

Don Jangster

TITLE:

Municipal Sales Manager

ADDRESS:

7311 Omega Circle

CITY:

Franksville

ZIP: 53126

TEL: 262-473-4700

Signed this 6th Day of May 2015.

CONTRACTOR'S AFFIDAVIT

STATE OF WISCONSIN)

(SS.

_____ VILLAGE)

_____, an office of

to-wit: I, Dan Jongetjes, being first duly sworn in oath, says:

That Johns Disposal Service, Inc., has no contractual relationships whatsoever, either direct or indirect, with any officer, agent, or employee of the Village of Pleasant Prairie

Owner: Dan Jongetjes; nor has any officer, agent, or employee of the Owner any financial interest, either direct or indirect, present or prospective, absolute or conditional in the contract to which this bond pertains, as this affiant well knows.

Dated at Hales Corners WI this 6 day of May, 2015

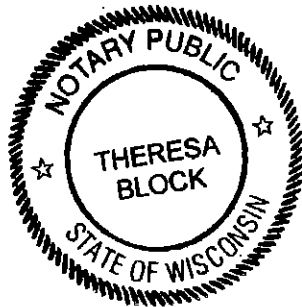
Affiant Dan Jongetjes / Theresa Block

Title Municipal Sales Manager / Notary Public

Subscribed and sworn to before me this 6 day of May, 2015

Theresa Block
Notary Public, State of Wisconsin

My Commission Expires: 2-8-2019





Advanced Disposal

PROPOSAL FOR:

Village of Pleasant Prairie

Attn: John Steinbrink, Jr.

8600 Green Bay Rd.

Pleasant Prairie, WI 53158

Submitted By:

Advanced Disposal Services Solid Waste Midwest, LLC

Contact: Jason Johnson

Phone: 262-893-9156

A) Statement of interest and qualifications

Advanced Disposal Services Solid Waste Midwest, LLC (hereafter Advanced Disposal) is interested in continuing to provide processing of single stream recycling for the Village of Pleasant Prairie at its Material Recovery Facility (MRF) located in Kenosha at 5421 46th Street. Advanced Disposal Services Solid Waste Midwest LLC became the owner of the Kenosha MRF in 2012 as part of a stock purchase agreement with Veolia Environmental Services. The Kenosha MRF has been the outlet for the Village's residential recycling since 2009. The Kenosha MRF management team has not changed since the initial contract was signed with the Village. Bill Neve, Site Manager, has been at the Kenosha MRF since the inception of the contract with the Village. The administrative staff, responsible for the prompt, accurate, billing of customers, remains committed to delivery of the customer services that the Village deserves and expects. With this team in place, the Advanced Disposal Kenosha MRF is prepared to serve the recycling needs of the Village for many years.

B) History of the firm

Advanced Disposal is a company that brings fresh ideas and solutions to the business of a clean environment. It is our people who make the difference and leave the world a cleaner, more beautiful place. We are proud to provide cost-effective and environmentally-sound solutions for waste and recycling collection, transportation, processing and disposal. But we do much more than collect garbage. We truly are an environmental services company.

We provide:

- Waste and recycling reduction solutions for homes and businesses.
- Safe disposal facilities with state-of-the-art engineering and construction techniques to protect the Earth.
- Efficient and effective recycling processing facilities to preserve our natural resources.
- Cleaner operating trucks and equipment utilizing lower sulfur fuels, more efficient oils, and more responsible maintenance practices.
- Support to those communities we serve in particular through environmental and beautification projects and education.

Advanced Disposal has the operational expertise, management strengths, financial capabilities and commitment to quality that few publicly traded or independent companies can rival. Our independent status frees us from the bureaucratic structure of a national company so that decisions that are important to you can be made quickly and implemented effectively.

Our success can be measured through the strong partnerships between our employees and the customers we serve. Whether our customers are municipalities, commercial businesses, construction and demolition companies, industries or individual homeowners, we look forward to creating and building our relationships by providing quality service at a fair price.

RFP #15-SW-02 VILLAGE OF PLEASANT PRAIRIE PROCESSING OF SINGLE STREAM RECYCLING

Locations:

Advanced Disposal has operations in 17 Eastern states including: Alabama, Florida, Georgia, Illinois, Indiana, Kentucky, Maryland, Michigan, Minnesota, Mississippi, Missouri, North Carolina, Pennsylvania, South Carolina, Tennessee, Vermont, Wisconsin and the Bahamas.

Facilities:

- 91 Collection / Hauling Facilities
- 42 MSW and C&D Landfills
- 72 Transfer Stations
- 25 Recycling Facilities

Customers:

- More than 2.3 million residential customers including 746 exclusive city and county contracts
- More than 302,000 commercial & industrial and construction & demolition customers

Fleet:

- More than 3,034 vehicles running routes on a daily basis; 160 run on CNG (compressed natural gas)

Disposal Volumes:

- Collects more than 7.7 million tons of waste annually
- Safely disposes of more than 15 million tons of waste annually in our landfills

Recycling Volumes:

- Collects approximately 594,000 tons of recyclables annually
- Processes approximately 184,000 tons of recyclables annually

Employees:

- Approximately 5,373 people

Industry Standing:

- Advanced Disposal is the largest privately-owned environmental services company in the U.S.

Environmental Integrity

Advanced Disposal is deeply committed to ensuring a clean and safe environment for our employees, our customers, and our communities. We consider environmental stewardship of utmost importance and believe that our true business is making the world a cleaner, more beautiful place to live, work and play.

Advanced Disposal has always had a strong commitment to recycling through our extensive residential and commercial collection operations. All offices participate in some sort of recycling program, whether it's recycling paper, aluminum cans, plastics or a combination of all. Many of Advanced Disposal's recent acquisitions and milestones stem from our goal to be a fully integrated environmental services company and prove our commitment to a cleaner, greener

world. We own and/or operate twenty-five recycling processing facilities throughout the United States and consistently look for opportunities to create a more sustainable workplace for our customers and ourselves in regards to environmental care and economies.

In a business where the end results are clean and safe communities, Advanced Disposal takes its environmental stewardship responsibilities very seriously. Our employees, our neighbors and our families live in the communities we service. We believe it is our primary job to ensure that these communities are clean, safe and healthy for many years to come.

C) Project description

The successful bidder is responsible for the weighing, processing and billing of approximately 2,100 tons of recyclable material per year. This also includes the rebate to the Village of the value of the recyclables based on the compensation value on a monthly basis. All recyclables will be transported by the Village.

D) Abilities and expertise to provide the required services

The Kenosha MRF accepts materials from various municipalities through either direct delivery, such as Pleasant Prairie, or from collections provided as part of Advanced Disposal's municipal collection contracts.

E) Key personnel

Bill Neve-Site Manager: Overall management of Kenosha MRF including operations and administrative functions

Jason Johnson-District Municipal Market Manager, Southern WI/MN: Contract management and liaison between Advanced Disposal and the Village as it relates to additional services or needs of the Village

Chris Rooney-District Manager, Southern WI/MN: Overall management of all operations including landfills, recycling facilities, hauling divisions for Southern Wisconsin and Minnesota

F) Sub-consultants

Not applicable

G) Expertise or experience relative to this work

As the Village's current facility for the delivery of recycling materials, Advanced Disposal has the expertise and ability to continue to deliver prompt and cost-efficient services to ensure the Village collections are not affected in a negative way.

H) References

- a. City of Kenosha
- b. City of Kenosha Recycling Materials
- c. Advanced Disposal accepts the recycling materials collected from City collection vehicles
- d. Mark Willing

RFP #15-SW-02 VILLAGE OF PLEASANT PRAIRIE PROCESSING OF SINGLE STREAM RECYCLING

- e. 625 52nd Street, Kenosha, WI
- f. 262-653-4186
- g. Mark Willing

l) Contract Details

- a. Accepted materials include:
 - i. Newspaper (ONP)
 - ii. Mixed paper (junk mail, magazines, paper bags)
 - iii. Corrugated cardboard (OCC)
 - iv. Glass (clear, brown, green) bottles and jars
 - v. Aluminum
 - vi. Steel, tin, bi-metal containers including aerosol cans
 - vii. Plastic containers (1-7)
 - viii. PET containers
 - ix. HDPE containers
 - x. Aseptic packaging including juice boxes and cartons
- b. Material may be delivered between the hours of 7:30am through 3:30pm Monday through Friday, excluding holidays. In the event of a holiday, the facility is available for delivery of materials on Saturday between on Saturdays.
- c. The MRF is located at 5421 46th Street, Kenosha, Wisconsin
- d. The distance to the Advanced Disposal MRF is approximately 4.5 from 8600 Green Bay Road, with an approximate travel time of nine (9) minutes.
- e. The facility is closed to observe New Years Day, Memorial Day, 4th of July, Labor Day, Thanksgiving Day, and Christmas. When these Holidays occur on Saturday or Sunday they may be observed on the following Monday.
- f. The tipping fee shall be as follows:
 - i. Year 1: \$11.02
 - ii. Year 2: \$11.29
 - iii. Year 3: \$11.57
- g. All tipping fees shall include current local, state and federal fees and taxes. In the event any of these shall increase, such changes will be passed onto the Village, with written documentation.
- h. Advanced Disposal shall provide on a bi-annual basis a breakdown of the materials based on a percentage composition study of the materials delivered by the Village
- i. Tipping fees shall be invoiced on a monthly basis, with payment due within sixty (60) days

VILLAGE OF PLEASANT PRAIRIE
RESPONDENT'S PROPOSAL
SIGNATURE PAGE AND LEGAL STATUS

THE UNDERSIGNED CERTIFY THAT THEY ARE AN OFFICIAL LEGALLY AUTHORIZED TO BIND THEIR FIRM AND TO ENTER INTO A CONTRACT SHOULD THE VILLAGE ACCEPT THIS PROPOSAL.

PROPOSAL BY: Advanced Disposal Services Solid Waste Midwest, LLC
(Name of Firm)

LEGAL STATUS OF RESPONDENT: (Please check the appropriate box)

CORPORATION STATE OF INCORPORATION WI

PARTNERSHIP LIST NAMES _____

DBA EXPLAIN _____

SIGNATURE OF RESPONDENT:
Mary Middleton O'Brien

TITLE:
Chief Marketing Officer

ADDRESS:
90 Fort Wade Road, Suite 200

CITY:
Ponte Vedra, FL

ZIP:
32081

TEL:
904.737.7900

Signed this 7th Day of May 20 15



CERTIFICATE OF LIABILITY INSURANCE

DATE (MM/DD/YYYY)
12/3/2014

THIS CERTIFICATE IS ISSUED AS A MATTER OF INFORMATION ONLY AND CONFERS NO RIGHTS UPON THE CERTIFICATE HOLDER. THIS CERTIFICATE DOES NOT AFFIRMATIVELY OR NEGATIVELY AMEND, EXTEND OR ALTER THE COVERAGE AFFORDED BY THE POLICIES BELOW. THIS CERTIFICATE OF INSURANCE DOES NOT CONSTITUTE A CONTRACT BETWEEN THE ISSUING INSURER(S), AUTHORIZED REPRESENTATIVE OR PRODUCER, AND THE CERTIFICATE HOLDER.

IMPORTANT: If the certificate holder is an ADDITIONAL INSURED, the policy(ies) must be endorsed. If SUBROGATION IS WAIVED, subject to the terms and conditions of the policy, certain policies may require an endorsement. A statement on this certificate does not confer rights to the certificate holder in lieu of such endorsement(s).

PRODUCER Willis of Virginia, Inc. c/o 26 Century Blvd P.O. Box 305191 Nashville, TN 37230-5191	CONTACT NAME: certificates@willis.com	
	PHONE (A/C, No, Ext): (877) 945-7378	FAX (A/C, No): (888) 467-2378
E-MAIL ADDRESS:		
INSURER(S) AFFORDING COVERAGE		NAIC #
INSURER A: Arch Insurance Company		11150
INSURER B: Alterra America Insurance Company		21296
INSURER C: Navigators Insurance Company		42307
INSURER D:		
INSURER E:		
INSURER F:		

INSURED
 ADS Waste Holdings, Inc.
 (See Attached List of Named Insureds)
 90 Fort Wade Rd.
 Ponte Vedra, FL 32081

COVERAGES

CERTIFICATE NUMBER:

REVISION NUMBER:

THIS IS TO CERTIFY THAT THE POLICIES OF INSURANCE LISTED BELOW HAVE BEEN ISSUED TO THE INSURED NAMED ABOVE FOR THE POLICY PERIOD INDICATED. NOTWITHSTANDING ANY REQUIREMENT, TERM OR CONDITION OF ANY CONTRACT OR OTHER DOCUMENT WITH RESPECT TO WHICH THIS CERTIFICATE MAY BE ISSUED OR MAY PERTAIN, THE INSURANCE AFFORDED BY THE POLICIES DESCRIBED HEREIN IS SUBJECT TO ALL THE TERMS, EXCLUSIONS AND CONDITIONS OF SUCH POLICIES. LIMITS SHOWN MAY HAVE BEEN REDUCED BY PAID CLAIMS.

INSR LTR	TYPE OF INSURANCE	ADDL INSD	SUBR WVD	POLICY NUMBER	POLICY EFF (MM/DD/YYYY)	POLICY EXP (MM/DD/YYYY)	LIMITS
A	<input checked="" type="checkbox"/> COMMERCIAL GENERAL LIABILITY <input type="checkbox"/> CLAIMS-MADE <input checked="" type="checkbox"/> OCCUR GEN'L AGGREGATE LIMIT APPLIES PER: <input checked="" type="checkbox"/> POLICY <input type="checkbox"/> PRO-JECT <input type="checkbox"/> LOC OTHER:			31GPP4985402	11/20/2014	11/20/2015	EACH OCCURRENCE \$ 2,000,000 DAMAGE TO RENTED PREMISES (Ea occurrence) \$ 1,000,000 MED EXP (Any one person) \$ PERSONAL & ADV INJURY \$ 2,000,000 GENERAL AGGREGATE \$ 10,000,000 PRODUCTS - COMP/OP AGG \$ 2,000,000
A	AUTOMOBILE LIABILITY <input checked="" type="checkbox"/> ANY AUTO <input type="checkbox"/> ALL OWNED AUTOS <input type="checkbox"/> SCHEDULED AUTOS <input type="checkbox"/> HIRED AUTOS <input type="checkbox"/> NON-OWNED AUTOS			31CAB4985502	11/20/2014	11/20/2015	COMBINED SINGLE LIMIT (Ea accident) \$ 3,000,000 BODILY INJURY (Per person) \$ BODILY INJURY (Per accident) \$ PROPERTY DAMAGE (Per accident) \$
B	<input type="checkbox"/> UMBRELLA LIAB <input checked="" type="checkbox"/> OCCUR <input checked="" type="checkbox"/> EXCESS LIAB <input type="checkbox"/> CLAIMS-MADE DED RETENTION \$			MAXA3EC50001367	11/20/2014	11/20/2015	EACH OCCURRENCE \$ 2,000,000 AGGREGATE \$ 2,000,000
A	WORKERS COMPENSATION AND EMPLOYERS' LIABILITY ANY PROPRIETOR/PARTNER/EXECUTIVE OFFICER/MEMBER EXCLUDED? (Mandatory in NH) If yes, describe under DESCRIPTION OF OPERATIONS below	Y/N	N/A	31WCI4985302	11/20/2014	11/20/2015	<input checked="" type="checkbox"/> PER STATUTE <input type="checkbox"/> OTH-ER E.L. EACH ACCIDENT \$ 1,000,000 E.L. DISEASE - EA EMPLOYEE \$ 1,000,000 E.L. DISEASE - POLICY LIMIT \$ 1,000,000
C	2nd Layer Excess			NY14EXC825662IV	11/20/2014	11/20/2015	Each Occ./Agg. 5,000,000
B	3rd Layer Excess			MAXA3EC30001448	11/20/2014	11/20/2015	See Attached

DESCRIPTION OF OPERATIONS / LOCATIONS / VEHICLES (ACORD 101, Additional Remarks Schedule, may be attached if more space is required)

CERTIFICATE HOLDER

CANCELLATION

For Informational Purposes Only

SHOULD ANY OF THE ABOVE DESCRIBED POLICIES BE CANCELLED BEFORE THE EXPIRATION DATE THEREOF, NOTICE WILL BE DELIVERED IN ACCORDANCE WITH THE POLICY PROVISIONS.

AUTHORIZED REPRESENTATIVE

ADDITIONAL COVERAGE SCHEDULE

COVERAGE	LIMITS
POLICY TYPE: Pollution Liability CARRIER: Illinois Union Insurance Company POLICY TERM: 11/20/2012 – 11/20/2015 POLICY NUMBER: PPLG27059065001	Aggregate: \$25,000,000 Occurrence: \$25,000,000
POLICY TYPE: Excess Liability CARRIER: Alterra America Insurance Company POLICY TERM: 11/20/2014 – 11/20/2015 POLICY NUMBER: MAXA3EC30001448	3rd Layer Excess Each Occurrence: \$5,000,000 Aggregate: \$5,000,000
POLICY TYPE: Automobile Liability CARRIER: Arch Insurance Company POLICY TERM: 11/20/2014 – 11/20/2015 POLICY NUMBER: 31CAB0502300	Automobile Liability - Any Auto Combined Single Limit - \$3,000,000 SIR - \$1,000,000

Named Insured:

Advanced Disposal Waste Holdings Corp.
ADS Waste Holdings, Inc.
Advanced Disposal Services South, Inc.
HWStar Holdings Corp.
Advanced Disposal Services East, Inc.
MWStar Waste Holdings Corp.
Advanced Disposal Services Midwest, LLC
Advanced Disposal Recycling Services Atlanta, LLC
Advanced Disposal Recycling Services, LLC
Advanced Disposal Recycling Services Gulf Coast, LLC
Advanced Disposal Services Alabama CATS, LLC
Advanced Disposal Services Alabama EATS, LLC
Advanced Disposal Services Alabama Holdings, LLC
Advanced Disposal Services Alabama, LLC
Advanced Disposal Services Arbor Hills Landfill, Inc.
Advanced Disposal Services Atlanta, LLC
Advanced Disposal Services Augusta, LLC
Advanced Disposal Services Biloxi MRF, LLC
Advanced Disposal Services Biloxi Transfer Station, LLC
Advanced Disposal Services Birmingham, Inc.
Advanced Disposal Services Blackfoot Landfill, Inc.
Advanced Disposal Services Blue Ridge Landfill, Inc.
Advanced Disposal Services Carolinas, LLC
Advanced Disposal Services Carolinas Holdings, LLC
Advanced Disposal Services Cedar Hill Landfill, Inc.
Advanced Disposal Services Solid Waste Southeast, Inc.
Advanced Disposal Services Central Florida, LLC
Advanced Disposal Services Chestnut Valley Landfill, Inc.
Advanced Disposal Services Cobb County Recycling Facility, LLC
Advanced Disposal Services Cobb County Transfer Station, LLC
Advanced Disposal Services Cranberry Creek Landfill, LLC
Advanced Disposal Services Cypress Acres Landfill, Inc.
Advanced Disposal Services Eagle Bluff Landfill, Inc.
Advanced Disposal Services Emerald Park Landfill, LLC
Advanced Disposal Services Evergreen Landfill, Inc.
Advanced Disposal Services Georgia Holdings, LLC
Advanced Disposal Services Glacier Ridge Landfill, LLC
Advanced Disposal Services Greentree Landfill, LLC
Advanced Disposal Services Gwinnett Transfer Station, LLC
Advanced Disposal Services Gulf Coast, LLC
Advanced Disposal Services Hancock County, LLC
Advanced Disposal Services Hickory Meadows Landfill, LLC
Advanced Disposal Services Hoosier Landfill, Inc.
Advanced Disposal Services Jackson, LLC
Advanced Disposal Services Jacksonville, LLC
Advanced Disposal Services Jones Road, LLC
Advanced Disposal Services Lehigh Valley, Inc.
Advanced Disposal Services Lancaster Landfill, LLC
Advanced Disposal Services Lithonia Transfer Station, LLC
Advanced Disposal Services Macon, LLC
Advanced Disposal Services Magnolia Ridge Landfill, LLC
Advanced Disposal Services Mallard Ridge Landfill, Inc.
Advanced Disposal Services Maple Hill Landfill, Inc.
Advanced Disposal Services Middle Georgia, LLC
Advanced Disposal Services Milledgeville Transfer Station, LLC
Advanced Disposal Services Mississippi, LLC
Advanced Disposal Services Mobile Transfer Station, LLC
Advanced Disposal Services Morehead Landfill, Inc.
Advanced Disposal Services National Accounts, LLC
Advanced Disposal Services National Accounts Holdings, Inc.
Advanced Disposal Services North Alabama Landfill, LLC
Advanced Disposal Services North Florida, LLC
Advanced Disposal Services North Georgia, LLC
Advanced Disposal Services Oak Ridge Landfill, Inc.
Advanced Disposal Services Orchard Hills Landfill, Inc.
Advanced Disposal Services Pasco County, LLC
Advanced Disposal Services Pecan Row Landfill, LLC
Advanced Disposal Services Pontiac Landfill, Inc.
Advanced Disposal Services Prattville C&D Landfill, LLC
Advanced Disposal Services Putnam County Landfill, LLC
Advanced Disposal Services Renewable Energy, LLC
ADS Renewable Energy – Eagle Point, LLC
ADS Renewable Energy – Stones Throw, LLC
ADS Renewable Energy – Wolf Creek, LLC
Advanced Disposal Services Randolph County, LLC
Advanced Disposal Services Rogers Lake, LLC
Advanced Disposal Services Rolling Hills Landfill, Inc.
Advanced Disposal Services Selma Transfer Station, LLC
Advanced Disposal Services Shippensburg, LLC
Advanced Disposal Services Seven Mile Creek Landfill, LLC
Advanced Disposal Services Skippack, Inc.
Advanced Disposal Services Smyrna Transfer Station, LLC
Advanced Disposal Services Solid Waste Leasing Corp.
Advanced Disposal Services Solid Waste Midwest, LLC
ADS Solid Waste of NJ, Inc.
Advanced Disposal Services Solid Waste of PA, Inc.
Advanced Disposal Services Somerset, Inc.
Advanced Disposal Services South Carolina, LLC
Advanced Disposal Services Star Ridge Landfill, Inc.
Advanced Disposal Services Stateline, LLC
Advanced Disposal Services Sumner Landfill, Inc.
Advanced Disposal Services Taylor County Landfill, LLC
Advanced Disposal Services Tennessee Holdings, Inc.
Advanced Disposal Services Tennessee, LLC
Advanced Disposal Services Valley Meadows Landfill, LLC
Advanced Disposal Services Valley View Landfill, Inc.
Advanced Disposal Services Vasko Rubbish Removal, Inc.
Advanced Disposal Services Vasko Solid Waste, Inc.
Advanced Disposal Services Wayne County Landfill, Inc.
Advanced Disposal Services Zion Landfill, Inc.
Arrow Disposal Service, LLC
Baton Rouge Renewable Energy, LLC
Burlington Transfer Station, Inc.
Cartersville Transfer Station, LLC
Caruthers Mill C&D Landfill, LLC
Champion Transfer Station, Inc.
Coastal Recyclers Landfill, LLC
Diller Transfer Station, Inc.
Community Refuse Service, Inc.
Community Refuse Service, LLC
Doraville Transfer Station, LLC
Eagle Point Landfill, LLC
Eco-Safe Systems, LLC

Firetower Landfill, LLC
Hall County Transfer Station, LLC
Harmony Landfill, LP
Highstar Galante, Inc.
Highstar Royal Oaks I, Inc.
Highstar Royal Oaks II, Inc.
Highstar Waste Acquisition Corp.
Hinkle Transfer Station, Inc.
IWStar Waste Holdings Corp.
Jones Road Landfill and Recycling, Ltd.
Land and Gas Reclamation, Inc.
Landsouth, Inc.
Middleton, LLC
Moretown Landfill, Inc.
Mostoller Landfill, Inc.
Mostoller Landfill, LLC
Nassau County Landfill, LLC
NEWS North East Holdings, Inc.
NEWS MA Holdings, Inc.
NEWS Mid-Atlantic Holdings, Inc.
NEWS PA Holdings, Inc.
NEWStar Waste Holdings Corp.
North East Waste Services, Inc.
Old Kings Road, LLC
Old Kings Road Solid Waste, LLC
Parker Sanitation II, Inc.
Pasco Lakes Inc.
PDC Disposal Co., Inc.
St. Johnsbury Transfer Station, Inc.
Site Services, LLC
Somerset Hauling, Inc.
South Hadley Landfill, LLC
South Suburban, LLC
SSI Southland Holdings, Inc.
Stone's Throw Landfill, LLC
Summit, Inc.
Superior Waste Services of New York City, Inc.
Tallassee Waste Disposal Center, Inc.
Trestle Park Carting, Inc.
Trestle Transport, Inc.
Turkey Trot Landfill, LLC
Vermont Hauling, Inc.
Waitsfield Transfer Station, Inc.
WBLF Acquisition Company, LLC
Welcome All Transfer Station, LLC
Western Maryland Waste Systems, LLC
Wolf Creek Landfill, LLC
WSI Medical Waste Systems, Inc.
WSI of New York, Inc.
WSI Sandy Run Landfill, Inc.
WSI Sandy Run Landfill, LLC



November 19, 2014

To Whom It May Concern:

For the policy period 11/20/2014 – 11/20/2015, ADS Waste Holdings, Inc. and all of its entities self-insure medical payments coverage. Should you have any questions, please contact the Risk Analyst listed below:

Kirstyn Shepler
(904) 438-2119
Kirstyn.shepler@advanceddisposal.com

Sincerely,

Marti Dickman

Marti Dickman
Vice President, Risk Management



November 19, 2014

To Whom It May Concern:

For the policy period 11/20/2014 – 11/20/2015, ADS Waste Holdings, Inc. and all of its entities are self-insured for the physical damage of vehicles. Should you have any questions, please contact the Risk Analyst listed below:

Kirstyn Shepler
(904) 438-2119
Kirstyn.shepler@advanceddisposal.com

Sincerely,

A handwritten signature in black ink that reads "Marti Dickman".

Marti Dickman
Vice President, Risk Management

Pleasant Prairie
 Sample Rebate Calculation for May, 2014
 Advanced Disposal

Commodity	Current Prices	Composition	Comp Value
OCC	\$76.00	9.93%	\$ 7.55
ONP	\$52.00	54.24%	\$ 28.20
Mix	\$40.00	0.38%	\$ 0.15
Aluminum	\$1,360.00	1.31%	\$ 17.82
Tin	\$96.00	2.60%	\$ 2.50
HDPE Natural	\$752.00	1.10%	\$ 8.27
HDPE Colored	\$432.00	0.35%	\$ 1.51
PET	\$368.00	3.79%	\$ 13.95
Mixed Plastics	\$264.90	0.14%	\$ 0.37
Glass - Mixed	\$ (5.00)	22.53%	\$ (1.13)
Residue	\$ (3.50)	3.63%	\$ (0.13)
Gross Value		100.00%	\$ 79.06
Less Processing Fee per Ton			\$ (45.00)
Less Transportation Fee per Ton			\$ (17.00)
Less Tipping Fee per Ton			\$ (11.02)
Gross Value			\$ 6.04
Tons Delivered- estimate			2,117
Total Rebate/(Charge)*			\$ 6,398



To: Michael Pollocoff, Village Administrator; and Members of the Village Board

From: Doug McElmury, Chief; Craig Roepke, Deputy Chief

CC: Jane Romanowski, Village Clerk

Date: June 9, 2015

Re: Ambulance replacement project

The Fire & Rescue Department was budgeted \$260,495 in 2014 to replace an ambulance vehicle which was carried over into 2015. We currently utilize three International chassis Medtec ambulances. The department operates two primary advanced life support (ALS) paramedic ambulances (2010/2007) and a third in reserve (2002) that may operate at the ALS level as call-back staffing permit. Vehicles historically have been replaced about every ten years.

For the past three replacement cycles we elected to utilize a medium duty chassis such as the International truck chassis due to past repetitive maintenance such as replacing alternators due to electrical loads. With that chassis choice, came a sacrifice of a rougher ride. Within the last two years we have seen an increase in maintenance issues as they related to the environmental equipment as well as dealer/manufacturer service/warranty challenges.

With the advent of LED technology and improved liquid suspension, we look to depart from the concept of the International style chassis and revert back to the Ford truck chassis, specifically a Ford F-550. We have researched, visited, and test driven, several of this style of chassis, spoke with other departments in the region and the ride and capabilities of the newer Ford chassis' is much improved over the Fords of years past. This ultimately equates to a better patient transport experience and we believe value-added maintenance. In addition to the new chassis style we plan on equipping the vehicle with four wheel drive capabilities.

Another departure from the current past three models is that of interior design and function. Historically we have purchased vehicles that were practically identical from each previous model. In this rendition, we formulated a group of Pleasant Prairie fire medics that were EMS motivated to look at how our paramedic system has changed since its inception in 1991, and its likely changes in the next 2-7 years. We also consulted our current Medical director, Fire/EMS partners in the region, as well as reviewed new designs and trends at trade shows such as the Fire Department Instructors Conference (FDIC) and the Wisconsin EMS Association (WEMSA). Some new elements include securing controlled medications (Knox Medvault), IV fluid warmer, and refrigerator (cool IV Fluid & cool medications to enhance their longevity).

A detailed Request for Proposal (RFP) was issued to three major ambulance manufacturers, Horton, LifeLine, and Road Rescue and their Wisconsin Authorized Distributors. We received two responses to the RFP from Everest Emergency Vehicles (Road Rescue), and Foster Coach (Horton).

A review was performed on the two submittals. Foster Coach/Horton provided the more comprehensive proposal, including drawings which aided in visually understanding the finished product and the ability to deliver vehicle that most closely represents the proposal.. There were several exceptions within the Foster



proposal; most were clarifications, direct equivalencies for discontinued products, or statements were specifications requested were in conflict with the build process.

The proposal from Everest Emergency vehicles did not include any proposal drawings. More concerning is that the patient module proposal was dimensionally smaller than specified. The fundamental design of the patient module was predominately a custom request.

Cost Proposals

Both vendors proposed cost for the combined vehicle chassis and ambulance module were very close.

Vendor/Dealer	Cost
Everest Emergency vehicles / Road Rescue	\$195,000
Foster Coach / Horton	\$196,502 ^{1,2}

¹ after a \$2500 WEMSA discount & \$3500 pre-payment discount

² an additional \$820 for added items post proposal

Closing Comments

Foster Coach out of Sterling, IL has a proven relationship of delivering a solid product. To this day they continue to supply ambulance module parts for our three Medtec ambulances, even as Medtec as an ambulance manufacturer no longer exists. Horton as well is a solid and reputable ambulance manufacturer with numerous customers in the region and nationwide.

We budgeted an additional \$44,000 for loose equipment expenditures such as a radio package of \$16,875 and a Stryker Power cot/stretchers at \$16,861. The remainder is earmarked for items such as Medvault, traffic pre-emption, secure key system, & smaller EMS equipment

Recommendation

Purchase the ambulance vehicle (chassis) and module as proposed from Foster Coach / Horton for the cost of \$196,502 and associated loose equipment for a total of \$240,238.

Village of Pleasant Prairie Fire & Rescue Department

Request for Proposal

Type 1 Ambulance



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Introduction and General Description

Sealed proposals subject to the terms and considerations of this request for proposal will be received by the Village Clerk, Village Of Pleasant Prairie, Village Hall, 9915 39th Avenue, Pleasant Prairie, Wisconsin, 53158-6504.

Issue date: May 5, 2015

Request for proposal: Village of Pleasant Prairie Fire & Rescue Department
For a Type 1, Ambulance

Deadline for receipt of proposal: **1:00 pm CST, Friday, May 29, 2015**

Opening of proposal: 1:15 pm CST, Friday, May 29, 2015
Village Hall, 9915-39th Avenue

Contact person: Deputy Chief Craig Roepke

Fire & Rescue Department
8044 88th Avenue
Pleasant Prairie, Wisconsin 53158-2015
(262) 694-8027
croepke@plprairiewi.com

Proposal

In accordance with the above and in compliance with all terms and conditions, unless otherwise noted, the undersigned offers and agrees, if the proposal is accepted, to furnish a new a Type 1 ambulance vehicle for which prices are quoted, manufactured and delivered to the (Village of Pleasant Prairie) Fire & Rescue Department within the time specified. If the Village accepts a proposal, the Village intends to enter into a contractual agreement with the manufacturer providing the selected equipment. Contract discussions and negotiations will follow selection of the most appropriate proposal. The Village reserves the right to negotiate further with one or more bidders. The contents of this Request for Proposal and the successful respondent's proposal will become an integral part of the contract, but may be modified by provisions of the contract. Bidders must be amenable to including in the contract; any information provided either in response to this Request for Proposal or subsequently during the selection process. Bidders are requested to submit their current contract forms with their response for review by the Village Administrator and the Fire Chief. In the event that a potential respondent wishes to respond to this Request for Proposal, but was not sent one directly, they should immediately direct their response to the contact person listed above.

NOTE: RESPONDENTS ARE ENCOURAGED TO STATE WHAT TYPES OF DISCOUNTS CAN BE OFFERED FOR PRE-PAYMENT OR OTHER PAYMENT SCHEDULES.

General Information

A. Request for Proposal

The purpose of this Request for Proposal from interested and qualified vendors is to award a contract to purchase and thereby provide the citizens of Pleasant Prairie with a Type 1, Ambulance vehicle.

B. Introduction and General Description

The Village of Pleasant Prairie Fire & Rescue Department intends to ultimately purchase an ambulance (Type 1) vehicle that meets the following minimum requirements:

Federal & State Regulations

The manufacturer of this vehicle shall demonstrate that the vehicle proposed meets or exceeds all federal and state of Wisconsin regulations governing the operation of this type of vehicle.

Chassis

- A. Ford F-550 Super Duty Regular Cab XLT
- B. Four wheel drive (4x4)
- C. Ford Ambulance Prep Package
- D. Dual battery system
- E. Dual Alternator system
- F. Engine: 6.7L Power Stroke Diesel V8
- G. Engine block heater – wired to module shoreline with ability to turn on/off
- H. Fuel Capacity: 40 Gallons – single tank
- I. Wheelbase: 189” – Cab to Axle: 108”
- J. Skid Plate – Transfer case
- K. Tow hooks (2) frame front mounted - (Identify capacity)
- L. Chassis to have curb side exhaust. Extend tail pipe out to edge of rub rail as needed to accept existing department Plymovent exhaust system. Verify fittings prior to final installation.
- M. Liquid suspension system for ambulance module

Chassis Modifications, Hardware & Accessories

- A. Remote Keyless entry (3)
- B. Power Equipment - door locks and windows with one-touch up and down driver and passenger window
- C. Mirrors - Manual Telescoping Trailer Tow with Power Heated Glass and Heated Spotter Mirror
- D. Seats - Front, Cloth 40/Mini-Console/40 Seats
- E. Cruise control
- F. Floor Mats – WeatherTech® All-weather mats for driver and passenger areas. Match to vehicle type.
- G. Engine High-Idle speed control shall be furnished. The control shall be set to automatically increase the engine speed (RPM) to the engine manufacturer’s recommended setting to sustain the ambulance’s total continuous electrical load at the regulated voltage and provide maximum heating/air conditioning output. The device shall operate only when switched to the “ON” position and the transmission is in “PARK” or “NEUTRAL.” The parking brake shall be applied at all times when the Engine High-Idle speed control is in use. The device shall disengage high idle operation according to OEM and/or engine manufacturer disablement strategy, or if not specified, when the operator depresses the service brake pedal or the transmission is placed in gear.
- H. Map light – one (1) minimum
- I. Heating and Air conditioning to support cab interior
- J. Interior grab handles, one each, Driver and passenger

- K. Provide options for OEM GPS
- L. Provide red LED dome light over each seated position, switchable from light.
- M. Install customer provided CAD mount. Provide fused continuous 12VDC power and terminate at base mount leaving 10 inch pigtail
- N. Provide on passenger door, medical glove box compartment capable of carrying two (2) boxes based on standard glove box dimensions.
- O. Program auto door lock feature for 100 mph (Essentially, disable auto locks)
- P. VEHICLE SUSPENSION: activation & dump override
 - Activated by the left rear patient compartment entry door.
 - Switch will be installed where specified, to override the automatic dump feature

General Requirements (Conversion)

1. The overall ambulance module will be no less than 173” long and 96” in width.
2. The module interior headroom will be no less than 74”
3. Curbside module access door will be placed forward providing no space between door and corner of module. May also be referred to as “door forward”.
4. Should the vendor have a standard chassis/module configuration that closely resembles the exterior and interior compartments, cabinetry, and configuration as described within this request, it should be noted and the information provided.

Module Body

- A. Mud Flaps – install individual rear mud flaps on each set of rear wheels. Ensure mud flaps are as wide as rear tires.
- B. Rear patient loading doors to have standard window tinting, double pane, each pane tinted.
- C. Two rubber dock bumpers shall be bolted to the rear step end caps for protection when backing.
- D. The center section of the rear step bumper shall be constructed of aluminum grip strut and be hinged to assist in patient handling.
- E. Reinforce end caps of rear bumper for greater impact resistance.
- F. The lower body rub rails shall be rubber.
- G. STAINLESS STEEL SPLASH SHIELDS Install brushed stainless splash shields on the lower front face of the body just behind the cab access doors. These splash shields are to be the same height as the diamond plate front corner guards.
- H. MODULE BODY FENDERS: Rubber extruded fenders shall be installed around the rear wheel well opening.
- I. Install power activated door locks on all patient area access doors. Locks to be activated by a switch at each patient area door, and controlled with the access door locks. Locks may be overridden by a manual slide lever or by the door key. Power locks are to tie into OEM cab door lock operation.
- J. Install a concealed switch on the exterior of the vehicle to operate the power door lock circuit. Doors shall be wired to unlock only on this circuit. Location to be in/at rear license plate area.
- K. PATIENT AREA SOUND PROOFING/ACOUSTIC ENHANCEMENT PACKAGE - Install standard sound control package.

- L. Provide & install a rear vision safety, Safety Vision SV-CLCD70B or its superseded model, camera system weatherproof color camera with built-in infrared illuminators system. The monitor shall either be incorporated into the vehicle's informational/LCD screen or a monitor shall be installed in the cab, forward center overhead console. Locations to be verified before placement. The camera shall be activated when the vehicle's transmission is placed in *Reverse*.
- M. NFPA COMPLIANT VENTING - Install an NFPA style venting system to include a 400CFM exhaust vent along with a static vent. Install 9.5" square polished covers
- N. HEAT/AC SYSTEM FORD F-SERIES CHASSIS - Install a ProAir 12V heat/AC system below the attendant seat. Unit is to include a replaceable filter at the air intake point. The control of the HVAC system will be based on a digital thermostat system with varied fan speeds.
- O. REFLECTORS: All patient compartment entry doors shall have red reflectors in the lower corner.
- P. Drawings provided in Appendix A are for conceptual and visual reference and discussion use only.

Interior Module Lighting

All lighting to be of type LED. Note any exceptions

PATIENT CEILING DOME LIGHTS: WHELEN LED - Install Whelen 80COEHCR LED dome lights in the patient area ceiling per the locations listed below. The lights will have a high/low feature. Lighting to be activated by any module main control areas. Reference attached drawings in Appendix A.

LOCATE: (5) OVER STREETSIDE BETWEEN EDGE OF COT AND SQUAD BENCH.
(4) OVER COT.
(1) OVER ATTENDENT SEAT

Arrange so that lighting units are evenly spaced within ceiling.

STEP WELL LIGHT- Install step well LED lighting for the right side patient door. Light to come on when door is opened.

Provide for LED Red light horizontal strip on side patient/access door located in the lower mid-section. LED strip to be activated when door is opened and when vehicle ignition is running.

Interior Wall & Cabinets – Streetside

All cabinet plexi-doors are to be lift-up cabinet frames where available

Cabinet Aft Streetside – locate a cabinet with Plexiglas doors at top of module. Cabinet to have one adjustable shelf. Dimensions to be: 27.75" (L) x 12.5" (H) x 18" (D)

Secondary Action area Aft of CPR Seat: Locate small shelf secondary action working area aft of the CPR seat. Discuss area of lengths available. Electrical connections above this area to include 110v receptacle powered by either shoreline or inverter. One (1) 12v cigarette style outlet located adjacent to 110v receptacle. 12v power from either shoreline or vehicles power system.

Cabinet forward streetside - locate a cabinet above main action area with Plexiglas doors at top of module. Cabinet will have one solid vertical divider at the center. Each side of the cabinet will have one adjustable horizontal shelf. Overall dimensions in general to be: 42" (L) x 18" (H) x 18" (D)

Action Area / inhalation Panel – to be in general, 55” (l) x 18”(D) and to include the following equipment. Action area must have switching ability of interior lighting and HVAC controls.

All countertop material to be Avonite material approved alternative non-porous material.

Recessed pocket: See attached drawing regarding notes on recessed pocket for phone and radio storage. Install a recessed pocket for smart phone storage. Locate in the inhalation panel between the control panel and the radio head. Size: 7”w x 4.5”h x 8”d. (all i.d.) Include a 1” retaining lip on the outboard edge. Include (2) 12v battery hot 12v outlets in the back wall for charging jacks.

LED dome light under inhalation panel: Install RED/White switchable flush mounted LED dome light on the underside of the inhalation panel under recessed pocket. Location to be verified.

Radio cut-out panel: to be located between the recessed pocket area and the lighting/HVAC control panel. Radio cut-out to fit Motorola APX6500 remote head

Action area

- (1) 110v receptacle (duplex)
- (1) 12v cigarette style outlet

A pull out writing table will be installed below the inhalation counter top. The writing surface is to be positioned for use at the attendant seat location.

Include a small pull ring latch (not Southco) and a large ballcatch latch to hold it in the close position. Install with locking slides to hold it in the open position. Writing tray size: 13.5”w x 16”d. (i.d.) Install a small trim lip on the rear facing edge to help hold the computer in place.

Vertical column opposite patient access door – behind attendant seat

- a. Locate at the base of the vertical column (1) Norcold DE0740 refrigerator or approved alternate. Unit to power from either shoreline or on-board inverter.
- b. Locate above the refrigerator (1) Smithworks floor mount IV Warmer or approved alternate. Provide for a 12”-14” gap/space from the top of the IV Warmer to the bottom of the next shelf/cabinet. Unit to power from either Shoreline or vehicle.
- c. Located above refrigerator/IV Warmer, an open lockable cabinet for storage. Cabinet to have one adjustable shelf. Cabinet to have both (1) 110v receptacle (duplex) and (1) 12v cigarette receptacle. These to be mounted on the rear wall at the top aft of the cabinet.
- d. Reminder of upper vertical space reserved for system electronics as necessary by manufacturer.

Interior Wall & Cabinets– Curbside

See Appendix A for attached drawing for general conceptual design.

Review conceptual curbside drawing; dimensions should provide for the ability to maintain a squad bench that can accept a 72” backboard. The aft exterior compartment must be able to accept and store a Stryker Stair-chair model 6252, a “scoop” stretcher and two standard 72” tall backboards.

Knox Med-Vault 2: Install customer provided unit in the in the header above the curbside cabinet, general location specified in Appendix A. Med-Vault 2 to have constant hot 12v fused power to support the device. Discuss options for mounting WIFI antenna as part of Med-Vault 2 system. Exact install dimensions to be discussed.

Glove storage above curbside cabinet: Storage for (3) glove boxes shall be provided in the header above the curbside cabinet. The face of the storage area will be covered in matching interior vinyl and include openings for access to each storage area. The header will be hinged for restocking or removal of glove boxes.

Curbside wall under Med-Vault 2 and Glove location to have the following wall receptacles:

- (1) 110v duplex outlet
- (1) 12v cigarette outlet (constant hot)
- (1) Chemetron Oxygen quick-connect
- (1) Firecom headset connection plate

Refer to Appendix A for visual locations. Final locations to be verified.

Material curbside countertop to be Avonite material.

Drawer below countertop have hardware in place to ensure locked closure (so not to open during vehicle movement). Provide a reconfigurable methodology to divide and segment interior space.

Interior Accessories & Trim

CPR Seat - Streetside: CPR seat to be 32" wide.

Squad Bench – Curbside: The ambulance shall be equipped with a squad bench no less than 73" long by 22" wide. The squad bench lid shall have a 2" foam pad covered with color coordinated heavy-duty vinyl. The squad bench lid shall be one-piece and have two heavy-duty, bi-directional gas piston hold-open devices. The force value selected and ball stud locations shall provide lift assistance after twenty degrees of bench lid lift angle. The ball stud mounts shall be at least ten millimeter. The lid shall be attached to the bench with a continuous hinge. Each lid shall have a self-latching, stainless steel, paddle style-latching device.

Passenger restraints: Appropriate restraints for seated passengers and belts for backboard patients on squad bench shall be provided. A minimum 3-point restraint for seated position will be provided at attendant seat, and at each end of squad bench. If advanced restraint system is available, please add and identify. No seat occupancy sensors to be installed. Seat Belt color to be RED, if available.

Spare Oxygen Cylinders storage: Provide two (2) Zicomatic QR-D-2 cylinder brackets under the squad bench seat at the head. Install base of brackets towards head of bench. Exact location to be determined.

IV Hangers - There shall be two CPI Model IV 2008 recessed rubber or equivalent, non-swinging type IV hangers that fold flat when not in use mounted in the ceiling over the lower portion of the patient area. They shall be bolted to the roof framing structure

Clock - There shall be Intellitec clock/time manager mounted in the rear of the module inside at the top. This clock shall be digital and include an elapsed timer with alarms.

Interior Vehicle Directional Indicators: Provide for small LED turn & stop indicators on the interior rear of the patient module to be visible by the person occupying the attendant seat.

Attendant's Seat - There shall be a high back, EVS brand child seat Model 1880S or equivalent. The EVS seat shall be a captain's chair-type attendant seat located at the front of the patient compartment. This seat shall be covered with color coordinated heavy-duty vinyl with heat sealed seams for ease of cleaning. The seat shall include a 3-point adult safety harness with push button release and a 5-point child safety harness. The seat shall have a fore and aft adjustment of six inches on heavy duty sliding tracks. This and all seats in the patient compartment shall meet all FMVSS requirements.

Vertical grab bar – *Mount vertical grab bar aft of the CPR seat. Bar to be 24" long (tall) and vertically mounted upper 3rd-level.*

Cot Mounts & Accessories

Prep module floor for future Stryker Power-load system Model 6390 per manufacturer's specifications.

Install standard cot "antler" frame to support a Stryker Power-Pro patient cot/stretchers. Ensure adequate attendant spacing between head of cot and attendant seat.

Cardiac Monitor Bracket

There shall be one (1) swivel style heart monitor bracket(s) installed for a **Zoll E-Series monitor**. General location to be located Aft of the counter area on the main action area. The specific location shall be determined.

NOTE: Fabricate Avonite spacer sized to fit beneath a Zoll E-Series heart monitor. Spacer to allow bracket to swivel over counter top lip.

Exterior Compartments – Streetside

Streetside Forward – Oxygen Cylinder compartment vented to outside. Single outside hinged door. General dimensions to be 21(w) x 82(h) x 20"(d)

Streetside forward top – Located above Oxygen cylinder compartment, compartment to provide for installation of both the electrical inverter and the suction vacuum motor. Door to be accessible from the exterior.

Streetside Intermediate – Double outside hinged doors both having exterior door handles and latching devices. General dimensions to be 55"(w) x 41"(h) x 20"(d). The rear wall of the compartment shall have installed a diamond plate backboard to afford the ability to mount equipment.

Streetside Aft - Double outside hinged doors both having exterior door handles and latching devices. General dimensions to be 38"(w) x 61"(h) x 20"(d). The rear wall of the compartment shall have installed a removable diamond plate backboard to allow the ability to mount equipment.

Exterior Compartments - Curbside

Patient access side door - to be located forward towards front of curbside module.

Curbside Intermediate (aft-patient door): Exterior door to allow access to inside shelf under vertical interior drawer. Size dimensions to match interior shelving.

Curbside Aft - Single outside hinged door. General dimensions to be 29”(w) x 83”(h) x 20”(d).
Compartment floor to be *sweep-out* style. This compartment to be designed for Styker Stair Chair Pro Model #6252, backboards (72” tall) (2), and a Ferno Model 65EXL scoop stretcher storage. Provide detail for additional shelf(s) above stair chair storage.

Exterior Compartment General Notes

All compartments shall have LED strip lighting secured to illuminate the interior.

The Streetside Aft compartment shall have two (2) 12 gauge sized 12v fused circuits with a 24” wire pigtail each originating from the upper left rear interior. Pigtail circuits to be clearly marked. Fuse locations to be customer accessible.

Cab/Chassis Emergency Lighting

Install (4) four Whelen M7 RED LEDs blank flange.

LOCATE: on front grill of cab location.

CONFIGURE: Flash to "X" pattern

Install (2) two Whelen M7 Split Red/White LED black flange – white forward.

LOCATE: One each at top of and forward at cab front wheel well.

Module Emergency Lighting

Front of Module

Seven (7) Whelen M9 series L.E.D. light heads all with black flanges with independent top and bottom flash control shall be installed in the designated locations.

LOCATE: (2) RED / WHITE, outboard on uppermost portion of front module.

LENS COLOR: Clear.

CONFIGURE: program reds and whites for alternating "X" pattern flash with fast random demo flash pattern.

LOCATE: (4) split RED / RED lights. Space evenly between the outboard lightheads.

LENS COLOR: RED

CONFIGURE: The (4) outer red/red lights to be controlled as defined below. Flash in an "X" pattern - streetside inner upper with curbside outer lower, etc.

LOCATE: (1) split WHITE/WHITE LED light head at the center of the top of module in line and spaced evenly with adjacent module light heads.

LENS COLOR: White

CONFIGURE: program center light for alternating double flash pattern.

Rear of Module

Install (4) Whelen M9 series RED LED light heads with black flanges shall be installed in the designated locations.

LOCATE: (2) at rear door window level so that lights are positioned center window when doors are open.

LOCATE: (2) at rear, upper outboard of corners of the rear module.

CONFIGURE: program reds alternating "X" pattern flash pattern.

Install (1) Whelen M9 series YELLOW LED light head with black flange shall be installed in the designated locations.

LOCATE: (1) upper top center of module in line with existing M9 series .

NOTE: Leave available space below Yellow light head module for installation of rear view camera.

Install (2) Whelen M6 series RED LED light head with chrome flanges shall be installed in the designated locations.

LOCATE: Install in rear riser in line with tail lights in body.

Module Sides

Install (4) Whelen M9 series RED LED light heads with black flanges shall be installed in the designated locations.

LOCATE: (2) at the upper outboard of each module side both street and curb side.

CONFIGURE: program reds alternating "X" pattern flash pattern amongst four side M9 lights.

Install (2) Whelen M7 series RED/WHITE LED light heads with black flanges

LOCATE: (1) each side of module centered over the rear wheel well (white forward).

Module Scene Lighting

Install (2) Whelen (Pioneer) PSL1R5B 15° surface mount slim-line LED Flood – black flange shall be installed in the designated locations.

LOCATE: two each side of module at top (curbside/streetside) just inside adjacent to M9 emergency side lighting.

Install (2) Whelen (Pioneer) PSL1R5B 15° surface mount slim line LED Flood – black flange shall be installed in the designated locations.

LOCATE: at rear (patient loading) of module at top, at the outside corners of each patient door. Keep in-line to adjacent M9 light heads at outboard corners

Module Rear Vehicle lighting

One (1) set of Whelen M6 series L.E.D. lights shall be installed on the rear of the vehicle as designated. They shall include L.E.D. stop/tail, turn and backup lights all with Black flange.

LOCATE: Stacked and centered on rear above riser.

- TURNS (M6T)
- BRAKE/TAILS (M6BTT) Program brake lights for signal alert feature.
- REVERSE/BACKUP (M6BUW)

NOTE: Program rear scene and rear side module scene lighting to activate when vehicle in reverse gear.

Emergency Lighting General Comments

Program all white lights to operate in Emergency Mode 3 - only.

White emergency lighting and Pre-emptions systems should be disabled (turned-off) when vehicle is in Park.

Terminate pattern control wires in the electronics cabinet and provide pattern control switches.

Configure rear RED LED warning light at door level to activate w/ Brake lighting when not in emergency activation.

Emergency Audible Warning Systems

Specified siren to be a Federal E-Q2B siren –siren speakers shall be recess mounted into the ends of the front chassis bumper, typical for Ford chassis.

SIREN SPEAKERS: Cast Products #SAD3806 & SAP3806 siren speakers shall be recess mounted into the ends of the chassis bumper. Locate siren amp on floor of electronics cabinet Configure siren unit to integrate with vehicle horn ring to change modes (yelp/wail)

AIR HORNS: UNDER BUMPER FORD F-SERIES -Install (2) Buell #1062 trumpets or an approved alternate under the front bumper recessed into the air dam valance. System to include compressor and remote air tank. Configure air horn operation so that devices can only be activated when the vehicle is in gear. Configure

Provide console switch for air horn operation.

Cab Console & Communications

GENERAL NOTE: All antenna, communication, and power connections must be labeled as to function and their markings, traceable to applicable fused locations.

Provide for-(middle) console in between front cab seats. Console to provide locations for radio, siren units, and emergency & scene lighting controls

Install customer provided Knox Sentralok® key retention system. System to be integrated to the main VHF radio system per manufacturer's instructions.

NOTE: Cabling to be clearly marked at each end

Provide in the console location two standard cup holders.

Reference console drawing for visual conceptual placement.

Install customer provided VHF radio transceivers (2) behind driver seat. Alternate location to be electronics cabinet. If alternate selected, appropriate changes to antenna & power connections to be identified.

ANTENNA COAX INSTALLATIONS:

- Install antenna coax wire of type RG58U and accesses so that ambulance conversion need not be disassembled
- Antenna mounts shall be of NMO type roof mounting.
- All radio end cable terminations shall be of type MINI-UHF Male connections.
- Cables and pull wires will be marked at each end.
- Antenna roof locations are to be evenly spaced across the longitudinal axis of the module roof at the center to provide maximum separation taking into account other devices (ie: Cellular Antenna).
- All Interior access to roof antenna locations will be accomplished by either individual access panel or via center ceiling channel.

Locations as designated below:

COAX #1: Locate cable to module rooftop, terminating behind the driver cab seat.

COAX #2: Locate cable to module rooftop, terminating behind the main action area in the patient compartment.

COAX #3: Locate cable to module rooftop, terminating behind the driver cab seat.

RADIO CABLE PULL WIRE - A reliable pull wire for future radio installation shall be installed from behind the driver's seat to behind the module inhalation panel.

CELLULAR Connectivity: Install customer provided CradlePoint dome antenna and equipment for cellular communications.

LOCATE: TBD, preferred in line with VHF antennas. Locate base unit in Electronics cabinet.

PREMPTION DEVICE: Install customer provided GTT Pre-emption system. Wire according to manufacturer specifications into emergency lighting and vehicle signal directional lighting.

LOCATE: on patient front module center, just below white LED light head.

Activation: System to activate by either manual switch located on cab console or level 3 EM lighting switch. Follow EM white lighting activation rules

Install dealer provided Firecom 5200D (two radio capability) intercom system. System to include appropriate radio interface cables (2) and provide (4) Firecom FH-51 wired headsets.

INSTALL: (2) FIRECOM headset jacks on the rear of cab centered between the chassis seats. Exact location to be determined prior to installation.

INSTALL: (1) Firecom intercom headset jack in patient module on curbside offset above squad bench.

INSTALL: (1) Firecom intercom headset jack on wall above and behind primary attendant seat.

INSTALL: (1) Firecom intercom headset jack on wall above and behind CPR seat.

CONFIGURE: Firecom unit to interface with both mobile radio units (front/rear) (A/B)

All headset locations to provide headset hang hook, location to be determined.

Exact headset locations to be verified.

RADIO POWER/GROUND: Install (3) 10 gauge cables to positive and ground studs for radio power. Wire one constant hot, one ignition hot and one ground. All wiring to have appropriate sized fuse, customer accessible.

LOCATE: (2 SET) behind driver seat as low as possible.

LOCATE: (1 SET) behind main action area/inhalation panel

STEREO SPEAKERS – Install one pair of radio stereo speakers in the patient module. These to be integrated to the vehicle's AM/FM radio system. Provide volume control at main patient action/inhalation area. Locate speakers fore and Aft along the center ceiling channel.

Fabricate metal slid-in tray/shelf for metal clip board. Allows clip board to be slid in from the top.

LOCATE: on inside wall of cab centered above rear of console.

Oxygen and Suction

Both Suction and Oxygen location shall not be obstructed by the location served by the mounted cardiac monitor. Wall outlet locations to be verified

Minimum four (4) Chemetron Quick-Connect oxygen wall outlets

LOCATE: (2) action area; (1) ceiling over head of cot and (1) over squad bench.

Provide two (2) Chemetron Oxygen Quick-connectors

- (2) Single Oxygen flow meters

Main Oxygen compartment to accept standard “M” Oxygen cylinder.

Electrical Notes

Shoreline, Utility Power:

- Install a 20 amp Kussmaul non-arcing auto eject shoreline receptacle. Install a Red cover on this inlet. Locate this receptacle on the driver side of the ambulance module at mid-level. Verify location prior to installation.
- This receptacle shall energize the vehicle’s internal AC circuit from an external power source (utility power). The purchaser’s stationary utility power circuit supplying the ambulance’s 125-volt AC power shall incorporate ground fault protection.
- An Indicator lamp/light adjacent to the shoreline plug shall be provided to indicate when shoreline power is being provided to the ambulance module. This indicator shall be sufficient in size to be seen at a reasonable distance across a station apparatus bay.

Inverter - A Vanner Inverter #20-1050CUL-DC shall be provided and installed. Installation location to be above outside Oxygen compartment.

Module HVAC Operation: Configure operation of module HVAC system to operate from either Shoreline connectivity or typical vehicle powered operation.

General Notes:

- All 12v outlet connections shall operational either from vehicle operation or when the utility power shoreline is attached.
- All 110v outlet connections shall be operational either from the in-vehicle inverter or when utility power shoreline is attached.
- Program an audible alarm to activate if the vehicle voltage drops below 11.8 volts for 120 seconds.

Provide electrical schematics/drawing of ambulance module and non-OEM chassis wiring.

Installation of Customer Provided Equipment

The following is a list of customer supplied equipment that will require installation. Exact locations to be discussed & verified.

Item/Description	Location	Notes
Mobile radio	Front console	Power
Knox Sentralok	Front console	Power
Computer (MDC) mount	Front Cab right of console	Power
Portable radio charger (2)	Front console	Power
Mobile radio	Inhalation panel/Action Area	Power
Thermometer	Inhalation panel/Action Area	
5 Gas meter Monitor/charger	Patient module - TBD	Power
Knox med vault	Curbside ceiling area	power
Traffic Pre-emption/GTT	Front of module/exterior	Power/manufact. spec
Cradle point Cell/WIFI	Roof antenna/Electronic cab	Power/antenna run
Handlight charger/base	Exterior aft streetside comp.	Power
SCBA (3) bracket set	Exterior aft streetside comp.	
Hand tools Ax/halligan	Exterior aft streetside comp.	
Thermo imaging Camera mount	TBD	Power
Knox MABAS 3200	Front Console	

Paint & Striping

The ambulance module shall be undercoated with automotive grade under-coating. All chassis manufacturer recommendations shall be followed.

Chassis & patient module to be painted Red. Discuss specific color variations and best match to truck chassis & module color.

Diamond grade chevrons-around rear patient module doors:

- Install Diamond grade chevron striping on the rear of the body around the rear doors. Install inboard of the corner posts and below the upper drip rail.
- Colors: fluorescent yellow green #983-23 with a red transparent film #1172 over the top of the yellow green to form the alternating red stripe. The fluorescent yellow green to be single sheet on the rear body panels.
- Install a 0.25" Blue Scotch-lite pinstripe around the perimeter of the chevrons on each rear face body side. Includes top, bottom and both vertical sides.

Apply 3M #8115 sealer on all body seams and all locations where the standard sealant material is applied. Apply the material to minimize (smooth outs) the transition between the body panels and the extrusion lips.

Single white strip to run down mid-section of module – SEE Appendix A for example, discuss cab door options. Discuss other vendor striping options applicable to the chassis / module type.

Utilize customer provided logo graphic. Logo placement to be determined.

Roof to be white with blue Star of Life located on roof, exact size and location to be discussed.

Appendix A - Conceptual Drawings

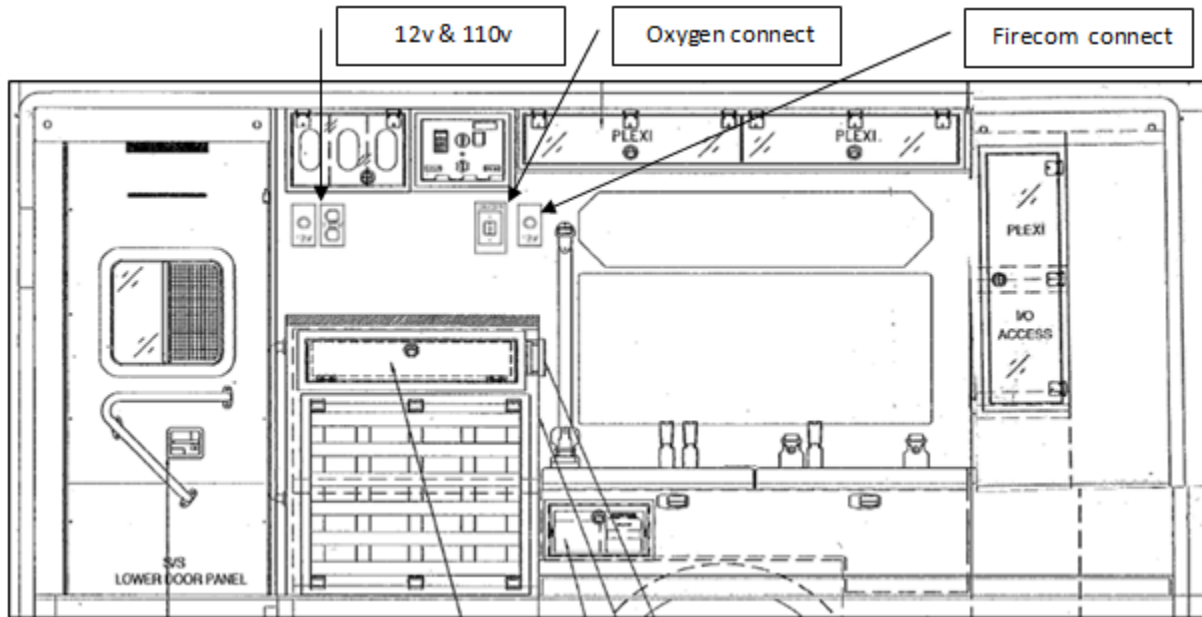


Figure 1 - Curbside interior module wall

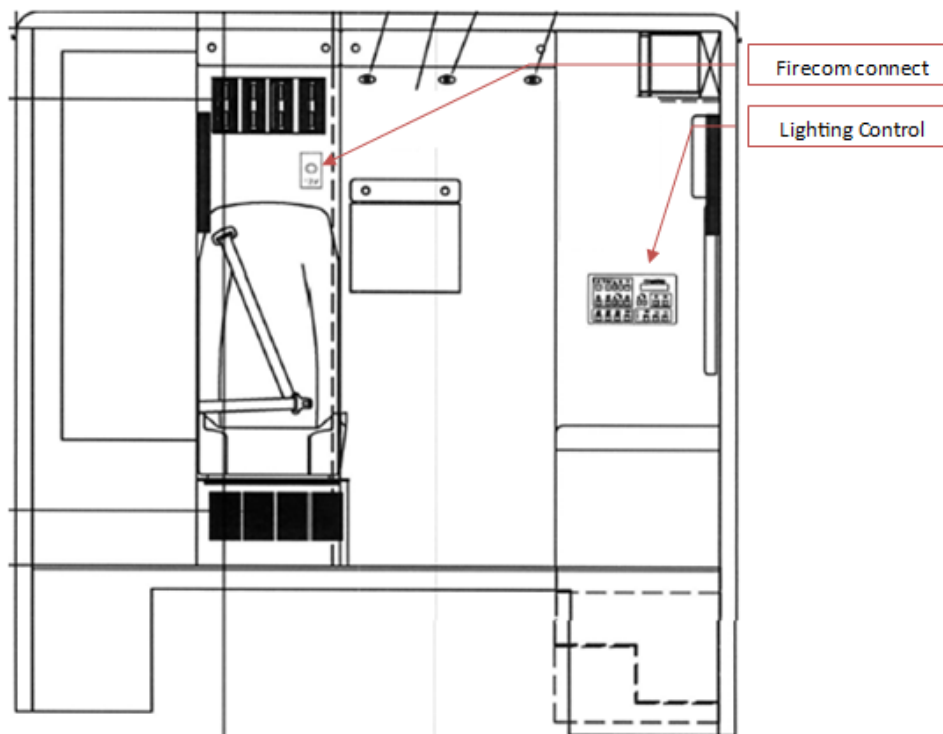


Figure 2 – Module forward View

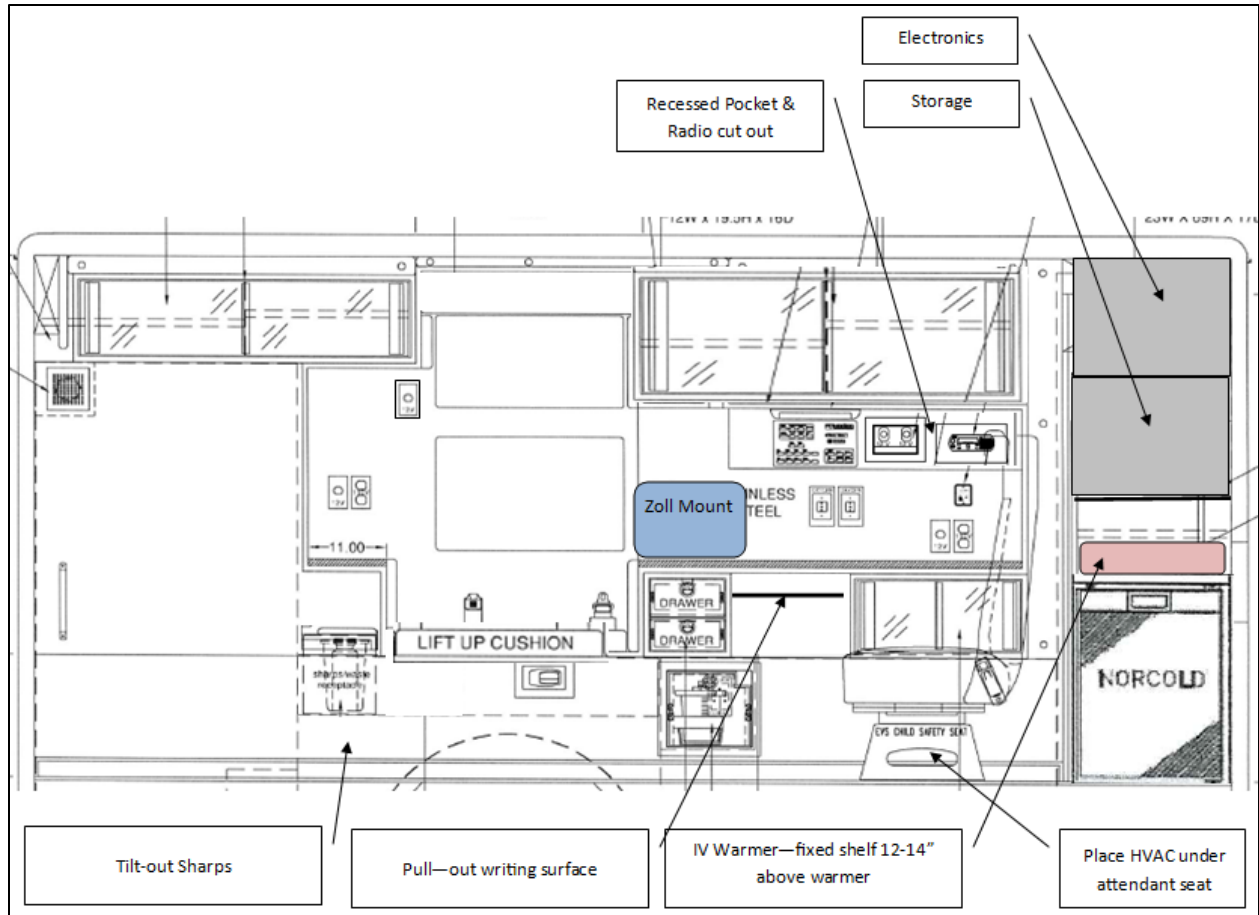


Figure 3 - Streetside Interior module wall

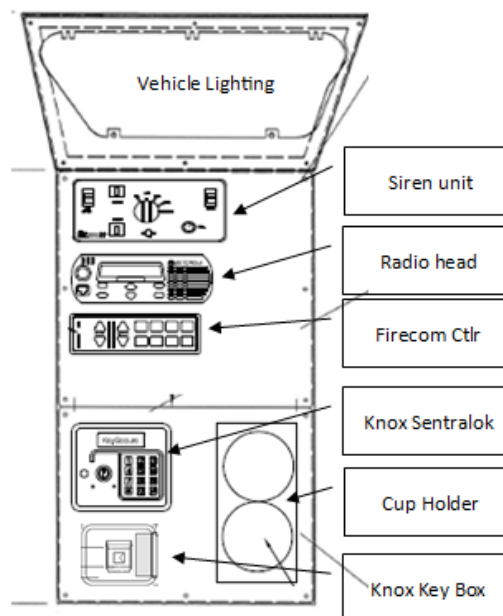


Figure 4 – Cab console configuration

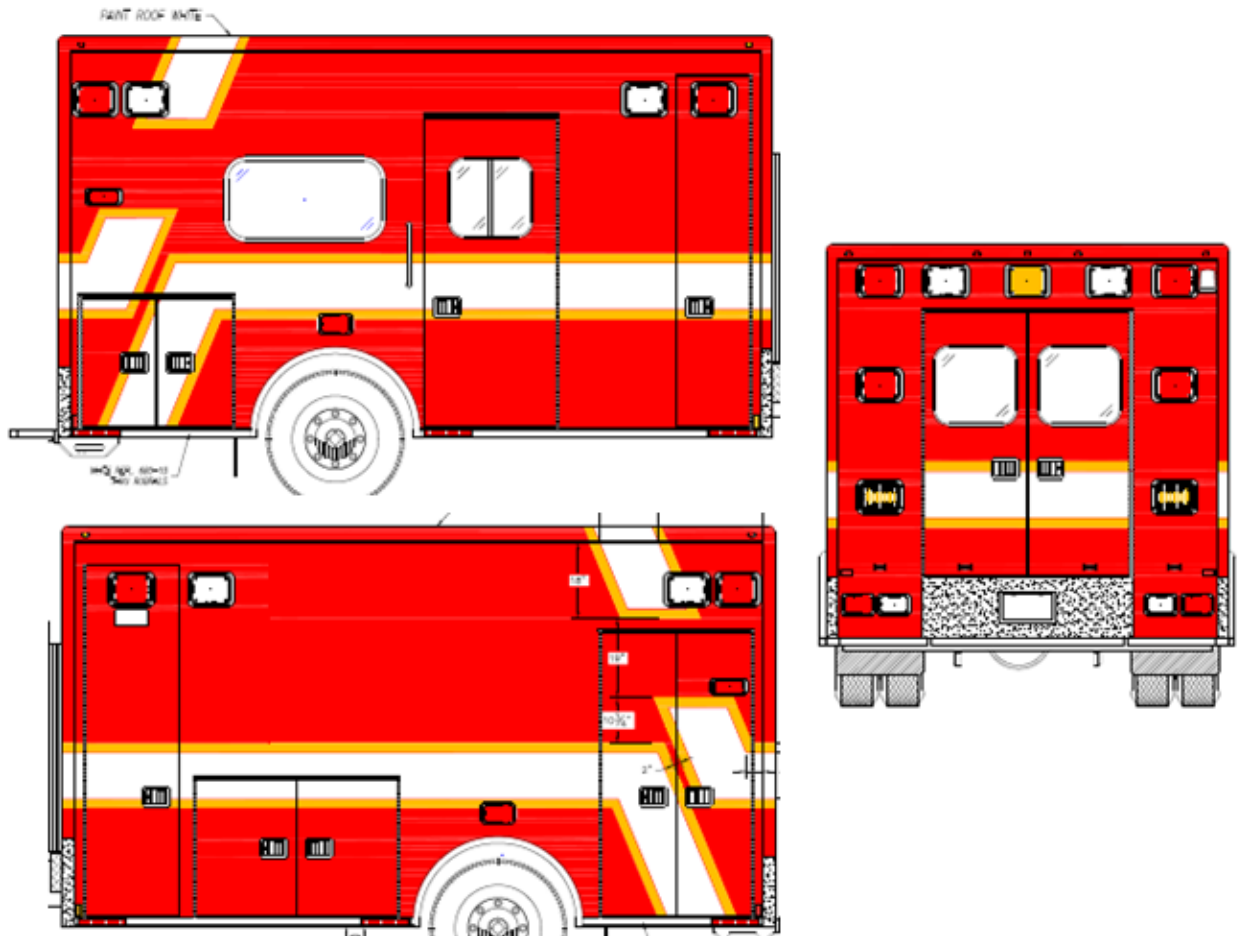


Figure 5 – Module Striping Examples

General Bid Information

1. Any questions and or apparent conflicts shall be brought to the attention of the Deputy Fire Chief. The ambulance chassis shall meet all applicable federal, state and local laws and safety standards.
2. The ambulance shall be the manufacturer's current conventional design and heavy-duty construction, and allied equipment specified, complete with all necessary operating accessories customarily furnished with vehicles of this type whether stipulated herein or not, together with such modification and attachments as may be necessary to enable the vehicles to function reliably and efficiently in sustained operation. The design of the ambulance shall permit ready accessibility for servicing, replacement, and adjustment of component parts and accessories with minimum disturbance of other components and systems.
3. It is the intent of these specifications to cover the furnishing and delivering to the purchaser a complete vehicle equipped as hereinafter specified. Minor details for construction and materials where not otherwise specified, are left to the discretion of the contractor who shall be solely responsible for the design and construction of all features.

Additionally, it is the intent of these specifications to secure an ambulance constructed to withstand the continuous use of emergency medical service.
4. The ambulance shall be constructed with due consideration to the nature and distribution of the load to be sustained and to the general characteristics of the service. All components shall be strong enough to withstand the road speed, practical tests and general service under full load.
5. The ambulance shall be designed with due consideration of load between front and rear axles, that all equipment will be carried without injury to the vehicle.
6. Each proposal shall be accompanied by a detailed description of the equipment for an ambulance.
7. The Village of Pleasant Prairie reserves the right to accept the proposal, which in their judgment, will best serve the interest of the Fire & Rescue department. The Village of Pleasant Prairie reserves the right to reject any and all bids.
8. The ambulance shall be designed so that the various parts are readily accessible for lubrication, inspection, adjustment and repair. When special tools, which are not normally used or available, such tools shall be supplied.
9. It is not the intention of the Village of Pleasant Prairie to eliminate vendors or manufactures of similar or equal equipment of the types specified. The Village of Pleasant Prairie shall be the sole judge of equipment that is the most advantageous and the decision of the Department shall be final. The ambulance must meet the major specific requirements and the intent of the requirements as specified herein. All items of these specifications shall conform to the character of the equipment for an ambulance and the purpose for which it is intended. Respondent shall provide only that equipment as required in the following request.
10. Bidders shall set forth in the proposal sheet the number of days after acceptance of proposal required for delivery.

11. The prices proposed by the respondent shall be the prices to be paid for all items furnished under this contract, irrespective of the time of shipment or delivery or beginning of use. No increase in the prices will be allowed under any circumstances and any proposal where escalator clauses are added or appended by the respondent will be rejected.

12. Any respondent to whom the contract is awarded is prohibited from assigning, transferring, sub-letting or otherwise disposing of the same or any part of the work called for by the same to any other person, company or corporation.

The successful respondent will provide an instructor, at no cost to the department at the time of delivery of the vehicle, to instruct the members of the department in the operation of the vehicle.

13. Two copies of a complete operation and maintenance manual shall be provided.

14. Two inspection trips with two representatives of the Fire & Rescue Department are to be included; One to be discussed as to best step in the process, and the second before delivery.

15. Drawings and descriptive language will be required for approval prior to start of construction.

16. Each respondent shall furnish warranty language that meets the requirements of the purchaser as to specific provisions in the specifications. This warranty shall state in simple, plain language exactly what the respondent promises as to repairs, replacements, parts, materials, workmanship costs, shipping and location of repair facilities.

17. The respondent(s) shall provide proposal(s) for an extended warranty for the chassis and the ambulance module.

This language shall be taken into account when the bids are reviewed.

18. Proposal evaluation:

The Fire Chief, Deputy Fire Chief, and Village Administrator shall evaluate the proposals received. This evaluation will be based as a minimum, on the following criteria:

- Commitment to the general conditions contained herein, particularly to that which applies to warranty.
- Completeness of the proposal.
- Manufacturing and delivery schedule.
- Contractor's demonstrated capabilities and qualifications.
- Equipment suppliers demonstrated capabilities and qualifications.

19. Technical approach to include:

- Design and engineering reliability factors.
- Maintenance considerations and recommendations.

20. Planning documentation addressing:

- Design and engineering data
- Drawings and schematic layout
- Logistical support
- Training
- Operation and maintenance

21. Exceptions:

Conditional proposals may be considered non-responsive and may be rejected.

Note: Certain exceptions may be accepted if they are minor, or if they will provide equipment or components, which are equal to or superior to these specifications.

Exceptions must be noted in the proposal on the form provided by the Village of Pleasant Prairie.

Exceptions shall be referenced to the paragraph and page of these specifications where the item appears, and drawings or photographs and technical information about the exception shall be included. The Village of Pleasant Prairie will consider any exceptions during the evaluation process, and the Village of Pleasant Prairie decision shall be final.

Proposals taking total exception to the specifications will not be considered.

The Village of Pleasant Prairie reserves the right to accept or reject any or all bids on the basis as purchaser deems to be in the best interest of the Village of Pleasant Prairie.

22. Contract Award

The Village of Pleasant Prairie reserves the right, before awarding the contract to require a respondent to submit such evidence of their qualifications, as it may deem necessary. Documentation that may be required is financial, technical, and other qualifications and abilities of the respondent. The Village of Pleasant Prairie shall be the final authority in the award of bids.

The Village of Pleasant Prairie reserves the right to negotiate with all bidders.

The Village of Pleasant Prairie reserves the right to purchase equipment from multiple sources.

The Village of Pleasant Prairie shall be shown an ambulance as similar as possible to the unit proposed, prior to the signing of the contract.

23. Bonding

A bond for 10% of the proposal price will accompany all bids submitted.

The Village of Pleasant Prairie may require the successful respondent to submit a performance bond for 100% of the proposal price. The bond is due fourteen (14) days after respondent has been awarded the contract to build. (The bond is subject to approval by the Village of Pleasant Prairie).

25. History and financial background

Respondent shall complete the history and financial background survey as provided by the Village of Pleasant Prairie

PLEASE READ CAREFULLY: YOU NEED TO FOLLOW THE DIRECTIONS BELOW:

Your proposal shall be returned in the following prescribed manner:

The first three (3) pages of your proposal shall be the forms provided by the Fire & Rescue Department (in the request for proposal packet) completed in full.

Page 1: respondent's proposal

Page 2: respondent's proposal "history and financial background"

Page 3: respondent's proposal "signature page and legal status"



May 28, 2015

Village of Pleasant Prairie Fire & Rescue Department
8044 88th Avenue
Pleasant Prairie, Wisconsin 53158-2015

Dear Friends;

Thank you for the opportunity to provide a new ambulance for your community. I am enclosing a quote for a 2016 Horton Model 623 all-aluminum modular body with 74" headroom on a 2016 Ford F550 4X4 chassis.

Any pre-payment option will be credited to the final invoice or a check written back to the city if paid in full. A 3% per annum interest rate will be calculated from time of receipt of pre-payment check until the time of completion at the Horton factory. Delivery would be approximately 240-days following order submission, pending receipt of chassis.

Horton Emergency Vehicles have a 30-year structural warranty, a 7-year/100,000 mile warranty on the electrical system, and a paint warranty of 7-years or 84,000 miles (prorated after four years). Conversion components are warranted for 2-years/24,000 miles. Horton Emergency Vehicles is QVM approved and KKK 1822-F certified. Horton has a reputation as a pioneer in occupant safety and technology with over thirty years of crash testing. Horton is recognized industry wide as one of the finest ambulances built today.

Foster Coach Sales, Inc. has been in business since 1957 and offers a complete brand new, service facility with certified Horton trained mechanics, loaner vehicles, remount facility, complete new state of the art body shop, and an extensive parts inventory. We pride ourselves in selling quality products at a reasonable price, backed by the best service in the industry. Our continuous growth is evidence of our success at doing just that, thus making us the oldest and largest volume dealer in the Midwest, serving Rescue Squads, Fire Departments, Hospitals, Private Services, City and County governments, and Volunteer groups, in Illinois, Iowa, Missouri, and Wisconsin.

Again thank you for your consideration and patience in this process. Please feel free to call me anytime if you have any questions or concerns. Our toll free number is 1-800-369-4215. I am sure you will find our product and our service worthy of your business again.

Sincerely,

A handwritten signature in blue ink, appearing to read 'Shawn Foster', is written over a light blue horizontal line.

Shawn Foster
Regional Sales Manager

FOSTER COACH SALES, INC.

903 Prosperity Drive Street P.O. Box 700
 Sterling, Illinois 61081

Phone: (815) 625-3276
 (800) 369-4215

Fax: (815) 625-7222

Web site: www.fostercoach.com

SF00509

QUOTATION

TO: Village of Pleasant Prairie Fire & Rescue
 8044 88th Avenue
 Pleasant Prairie, WI 53158

DATE: 05/28/15

ATTN: Deputy Chief Craig Roepke

REFERENCE: 2016 FORD F550 HORT

We are pleased to submit the following quotation in accordance with your request and subject to the Terms and Conditions listed below and on the reverse side hereof.

QTY.	DESCRIPTION	EACH	NET PRICE
1	FORD F550 4X4 ON A HORTON CONVERSION MODEL 623 (INCLUDING EQUIPMENT PER RFP)	\$ 201,682.00	\$ 201,682.00
1	WEMSA DISCOUNT (DISCOUNT PROVIDED AFTER DELIVERY OF VEHICLE)	\$ -2,500.00	\$ -2,500.00
1	OPTIONAL PRE-PAY DISCOUNT (DISCOUNT PROVIDED AFTER DELIVERY OF VEHICLE)	\$ -3,500.00	\$ -3,500.00
	OPTIONAL - PERFORMANCE BOND \$604.00		
	OPTIONAL INSPECTION TRIP \$1,500.00 EACH TRIP INSPECTION TRIP FOR (2) DEPARTMENT PERSONNEL ***INCLUDES AIRFARE, LODGING, AND EXPENSES		
		\$ Total	\$ 195,682.00
	ACCEPTED BY: _____		
	TITLE: _____ Date: _____		

ESTIMATED DELIVERY:
 240 DAYS PENDING CHASSIS DELIVERY

PROPOSED BY: 
 SHAWN FOSTER, REGIONAL SALES MANAGER



NON-COLLUSION BIDDING CERTIFICATE

The following statement is hereby made by the undersigned to accompany the sealed bid by the undersigned;

The undersigned hereby certifies:

1. That the bid which is accompanied by this statement has been arrived at without collusion with any seller of the same materials, and
2. That the contents of the bid have not been communicated by the bidder or one of its agents to any persons outside of the company.

Foster Coach Sales, Inc.
Name of Company

903 Prosperity Drive Sterling, IL 61081
Address

Mary J. Picha
By:

Sales Support
Title:

5/22/15
Date

Village of Pleasant Prairie
Respondent's Proposal

Village of Pleasant Prairie
Kenosha County, Wisconsin

We hereby propose to furnish one (1) Medium Duty Type I ambulance module & vehicle chassis for the following price:

Medium Duty Type I ambulance module & vehicle chassis: Total cost \$195,682.00
**INCLUDES PRE-PAY AND WEMSA DISCOUNTS*

Exceptions:
*SEE ATTACHED FOR ITEMIZED LIST

**Note: if more room is needed, please attach sheet.*

1. If awarded the contract, delivery will be in 240 days, or less.
2. Enclosed is a proposal bid bond for 10% of proposal price (BID BOND).

Respectfully submitted,

SHAWN FOSTER REGIONAL SALES MANAGER

By:  Regional Sales Manager 5/28/2015
Signature Title Date

MV-2793
State of Wisconsin Dealer
License Number

AVAILABLE UPON REQUEST
State of Wisconsin Salesperson
License Number

Village of Pleasant Prairie
Respondent's Proposal
Signature Page and Legal Status

The undersigned certified that they are an official legally authorized to bind their firm and to enter into a contract should the Village of Pleasant Prairie, Fire & Rescue Department accept this proposal.

Proposal by: FOSTER COACH SALES INC
(Name of Firm)

Legal status of respondent: *(please check the appropriate box)*

A. Corporation: FOSTER COACH SALES INC State of Incorporation: IL

B. Partnership: NONE List Names: _____

C. DBA: NONE explain: _____

D. Wisconsin Ambulance Dealers License # MV-2793

Signature of respondent:  _____

Title: REGIONAL SALES MANAGER

Address: 1400 LEETS BIR ROAD

City: STURTEVANT Zip: 53177

Telephone no. (800) 369-4215

Signed this 28 day of MAY, 2015



AIA

Document A310™ – 2010

Bid Bond

CONTRACTOR:

(Name, legal status and address)

Foster Coach Sales, Inc.
P.O. Box 700
Sterling, IL 61081

SURETY:

(Name, legal status and principal place of business)

Developers Surety and Indemnity Company
1603 22nd St., Ste 200
West Des Moines, IA 50266

OWNER:

(Name, legal status and address)

Village of Pleasant Prairie
9915 - 39th Ave.
Pleasant Prairie, WI 53158-6504

BOND AMOUNT: ten percent (10%) of attached bid

This document has important legal consequences. Consultation with an attorney is encouraged with respect to its completion or modification.

Any singular reference to Contractor, Surety, Owner or other party shall be considered plural where applicable.

PROJECT:

(Name, location or address, and Project number, if any)

Supply one 2016 Horton Model F623
with 74" headroom on a 2016 F-550 4 X 4 cab/chassis

Project Number, if any:

The Contractor and Surety are bound to the Owner in the amount set forth above, for the payment of which the Contractor and Surety bind themselves, their heirs, executors, administrators, successors and assigns, jointly and severally, as provided herein. The conditions of this Bond are such that if the Owner accepts the bid of the Contractor within the time specified in the bid documents, or within such time period as may be agreed to by the Owner and Contractor, and the Contractor either (1) enters into a contract with the Owner in accordance with the terms of such bid, and gives such bond or bonds as may be specified in the bidding or Contract Documents, with a surety admitted in the jurisdiction of the Project and otherwise acceptable to the Owner, for the faithful performance of such Contract and for the prompt payment of labor and material furnished in the prosecution thereof; or (2) pays to the Owner the difference, not to exceed the amount of this Bond, between the amount specified in said bid and such larger amount for which the Owner may in good faith contract with another party to perform the work covered by said bid, then this obligation shall be null and void, otherwise to remain in full force and effect. The Surety hereby waives any notice of an agreement between the Owner and Contractor to extend the time in which the Owner may accept the bid. Waiver of notice by the Surety shall not apply to any extension exceeding sixty (60) days in the aggregate beyond the time for acceptance of bids specified in the bid documents, and the Owner and Contractor shall obtain the Surety's consent for an extension beyond sixty (60) days.

If this Bond is issued in connection with a subcontractor's bid to a Contractor, the term Contractor in this Bond shall be deemed to be Subcontractor and the term Owner shall be deemed to be Contractor.

When this Bond has been furnished to comply with a statutory or other legal requirement in the location of the Project, any provision in this Bond conflicting with said statutory or legal requirement shall be deemed deleted herefrom and provisions conforming to such statutory or other legal requirement shall be deemed incorporated herein. When so furnished, the intent is that this Bond shall be construed as a statutory bond and not as a common law bond.

Signed and sealed this 8th day of May, 2015 Foster Coach Sales, Inc.

(Witness)

By:
(Principal) PRESIDENT *(Seal)*

(Witness)

By:
(Surety) Daniel P. Curran, Attorney-In-Fact *(Seal)*
(Title)

POWER OF ATTORNEY FOR
DEVELOPERS SURETY AND INDEMNITY COMPANY
PO Box 19725, IRVINE, CA 92623 (949) 263-3300

KNOW ALL BY THESE PRESENTS that except as expressly limited, DEVELOPERS SURETY AND INDEMNITY COMPANY, does hereby make, constitute and appoint:
Daniel P. Curran, Mark J. Schwab, Robert D.S. Karli, Deanna L. Schwab, jointly or severally

as its true and lawful Attorney(s)-in-Fact, to make, execute, deliver and acknowledge, for and on behalf of said corporation, as surety, bonds, undertakings and contracts of suretyship giving and granting unto said Attorney(s)-in-Fact full power and authority to do and to perform every act necessary, requisite or proper to be done in connection therewith as each of said corporation could do, but reserving to each of said corporation full power of substitution and revocation, and all of the acts of said Attorney(s)-in-Fact, pursuant to these presents, are hereby ratified and confirmed.

This Power of Attorney is granted and is signed by facsimile under and by authority of the following resolution adopted by the Board of Directors of DEVELOPERS SURETY AND INDEMNITY COMPANY, effective as of January 1st, 2008.

RESOLVED, that a combination of any two of the Chairman of the Board, the President, any Executive Vice-President, Senior Vice-President or Vice-President of the corporation be, and that each of them hereby is, authorized to execute this Power of Attorney, qualifying the attorney(s) named in the Power of Attorney to execute, on behalf of the corporation, bonds, undertakings and contracts of suretyship; and that the Secretary or any Assistant Secretary of the corporation be, and each of them hereby is, authorized to attest the execution of any such Power of Attorney;

RESOLVED, FURTHER, that the signatures of such officers may be affixed to any such Power of Attorney or to any certificate relating thereto by facsimile, and any such Power of Attorney or certificate bearing such facsimile signatures shall be valid and binding upon the corporation when so affixed and in the future with respect to any bond, undertaking or contract of suretyship to which it is attached.

IN WITNESS WHEREOF, DEVELOPERS SURETY AND INDEMNITY COMPANY has caused these presents to be signed by its officers and attested by its Secretary or Assistant Secretary this May 23, 2013.

By: *Daniel Young*
Daniel Young, Senior Vice-President

By: *Gregg N. Okura*
Gregg N. Okura, Vice-President



State of California
County of Orange

On May 23, 2013 before me, Gina L. Garner, Notary Public
Date Here Insert Name and Title of the Officer

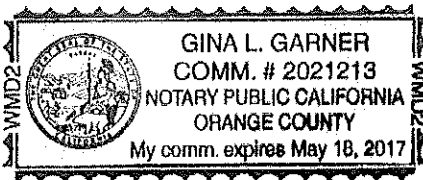
personally appeared Daniel Young and Gregg N. Okura
Name(s) of Signer(s)

who proved to me on the basis of satisfactory evidence to be the person(s) whose name(s) is/are subscribed to the within instrument and acknowledged to me that he/she/they executed the same in his/her/their authorized capacity(ies), and that by his/her/their signature(s) on the instrument the person(s), or the entity upon behalf of which the person(s) acted, executed the instrument.

I certify under PENALTY OF PERJURY under the laws of the State of California that the foregoing paragraph is true and correct.

WITNESS my hand and official seal.

Signature *Gina L. Garner*
Gina L. Garner, Notary Public



Place Notary Seal Above

CERTIFICATE

The undersigned, as Secretary or Assistant Secretary of DEVELOPERS SURETY AND INDEMNITY COMPANY does hereby certify that the foregoing Power of Attorney remains in full force and has not been revoked and, furthermore, that the provisions of the resolution of the Board of Directors of said corporation set forth in the Power of Attorney are in force as of the date of this Certificate.

This Certificate is executed in the City of Irvine, California, this 8th day of May, 2015 .

By: *Mark J. Lansdon*
Mark J. Lansdon, Assistant Secretary



FOSTER COACH SALES INC--903 PROSPERITY DRIVE--STERLING, IL 61081

PLEASANT PRAIRIE FIRE & RESCUE DEP
8044 88TH AVENUE
PLEASANT PRAIRIE, WI 53158-2015
CRAIG ROEPKE, DEPUTY CHIEF

Year: 2016
Chassis: Ford Type I, 189" WB, 108"
Type: 1
Model: 623
Rev 1 Print Features **11902**

CATEGORIES:

- A** Chassis
- B** Body Connection Pass Thru Type
- BH** Chassis Accessories
- BL** Chassis Electrical
- C** Conversion Model
- CB** Module Body Modifications
- D** Module Body Hardware
- E** Paint and Striping
- F** Cabinet Doors, Handles and Hardware
- G** Interior Colors
- H** Interior Cabinets, Streetside
- I** Interior Cabinets, Curbside
- J** Front Wall Cabinets
- K** Interior Accessories and Trim
- L** Cot Mount, and Patient Handling
- M** Warning Systems, Visual
- MN** Warning Systems, Audible
- O** Lights, Non Emergency
- P** Electrical Power Group
- Q** Heat, Ventilation, Air conditioning
- R** Console, Radio, and communications
- T** Oxygen and Suction
- U** Lettering and Graphics
- Z** Other



Selected Feature List 11902

Year: 2016
 Chassis: Ford Type I, 189" WB, 108"
 Model: 623

Option	Std	Qty	Header/Description/Data	Proposal Total
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BH02A100	1	1	STAINLESS STEEL WHEEL COVERS Stainless steel wheel covers shall be installed.	
BH03B000	1	1	MUD FLAPS: rear Install individual rear mud flaps behind each set of rear wheels.	
BH06A000	0	1	WIDE DOCK BUMPERS Install wide style rear dock bumpers. Not applicable with bumper option BH11E000.	
BH070000	1	1	REINFORCE REAR BUMPER END CAPS Reinforce end caps of rear bumper for greater impact resistance. <i>A section of 2" x 2" steel angle shall be attached to the back side of the std Horton step. (Note, this option is not available on recessed or impact steps).</i>	
BH08H000	0	1	RUNNING BOARDS: STAR PUNCHED Install heavy duty aluminum diamond plate running boards and splash shields. Running boards are to be star punched for enhanced drain and foot grip.	
BH110000	1	1	REAR STEP/ BUMPER ASSEMBLY The center section of the rear step bumper shall be constructed of aluminum grip strut and be hinged to assist in patient handling. <i>!!! ALL STEEL COMPONENTS OF THE REAR BUMPER ASSEMBLY SHALL BE COVERED WITH BLACK SCORPION COATING.</i>	
BH16L000	1	1	LIQUID SPRING SUSPENSION WITH DUMP FEATURE A Liquid Spring rear suspension shall be installed. The suspension system shall dump when the left rear entry door is opened. The system shall include a dump override switch. <i>Dump Switch Location: CURBSIDE REAR ENTRY DOOR PANEL ABOVE DOOR LOCK BEZEL.</i>	
BH32OR00	1	1	SUSPENSION SWITCH: dump override A switch will be installed where specified, to override the automatic dump feature activated by the left rear patient compartment entry door. <i>Switch Locate: CURBSIDE REAR ENTRY DOOR PANEL ABOVE DOOR LOCK BEZEL.</i>	



Selected Feature List 11902

Year: 2016
 Chassis: Ford Type I, 189" WB, 108"
 Model: 623

Option	Std	Oty	Header/Description/Data	Proposal Total
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BL073000	1	1	BATTERIES, STANDARD ON F-SERIES FORD CHASSIS (2) OEM batteries shall be installed in the manufacturers under hood battery tray locations.	
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BL08A000	1	1	BATTERY HEAT SHIELDS Battery heat shields will be provided for any battery located under the hood which is not protected by the OEM manufacturer.	
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BL26B000	1	1	BACKUP ALARM RESET Backup alarm to automatically reset to on if alarm was manually canceled using the control panel alarm cut-off switch (FL25) during previous use.	
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BL26I000	0	1	CAMERA SYSTEM FOR USE WITH I4g A camera system shall be installed as noted below. The system is to use the i4G front screen as its monitor. <i>Quantity: (1)</i> <i>Locate: ABOVE REAR DOORS.</i> <i>Note: CAMERA TO BE A SAFETY VISION #SV-625B (COLOR).</i>	
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BL32A000	1	1	OEM AM/FM/CD PLAYER SHALL BE PROVIDED BY THE OEM MANUFACTURER OEM AM/FM/CD player shall be provided by the OEM manufacturer on all Horton supplied chassis. <i>Note: This option applies only to chassis supplied by Horton. Chassis supplied by either the customer or the dealer must be ordered with this feature from the chassis OEM.</i>	
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C	CONVERSIONS			
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Selected Feature List 11902

Year: 2016
 Chassis: Ford Type I, 189" WB, 108"
 Model: 623

Option	Std	Oty	Header/Description/Data	Proposal Total
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CA640000 0 1 CONVERSION MODEL: F623 FORD F SERIES

MINIMUM BODY DIMENSIONS:

(Exterior)

-Height: 89"
 -Width: 96"
 -Length: 173"

(Interior)

-Height: 72"
 -Aisle 20"
 -Width: 20" (from edge of cot in wall position to squad bench riser)

-Length: 169"

OVERALL DIMENSIONS (Including Chassis, Module and Step):

-Height: 117" (to top of vent)
 -Width: 100"
 -Length: 307"

!!! VEHICLE TO HAVE A PASS-THROUGH CONFIGURATION WITH SLIDING WINDOW PROVIDED VIA OEM SLIDING WINDOW IN CAB.

!!! 74" HEADROOM.

!!! CRASH BARRIER CONFIGURATION.

CA640001 1 1 F623 STREETSIDE FORWARD:

Clear Door Opening: 18.7" wide x 79.1" high
 Actual Compartment 24.2" wide x 82.1" high x 20.3"
 This area shall be accessed through a single outside hinged door.
 The compartment shall house the vehicle's primary O2 cylinder. The compartment shall be vented to the outside.

cb040000 0 1 COMPARTMENT HEIGHT Modify: first, street side

The height of the standard compartment for this model shall be modified. This is the first street side compartment height modification and shall include affected modifications to the interior cabinet.

Height Modification: LOWER 22" TO ALLOW FOR ELECTRICAL COMPARTMENT ABOVE.
 Locate: STREETSIDE FORWARD.

CA640002 1 1 F623 STREETSIDE INTERMEDIATE:

Clear Door Opening: 51.7" wide x 38.3" high
 Actual Dimensions: 55.5" wide x 41.5" high x 20.3"
 This area shall be accessed through double, outside hinged doors.
 Both doors shall have exterior door handles and latching devices.

cb04a000 0 1 COMPARTMENT HEIGHT MODIFY: second, street side

The height of the standard compartment for this model shall be modified. This is the second street side compartment height modification. Interior cabinet modifications included in the "COMPARTMENT HEIGHT MODIFY: first, street side" option B04

Height Modification: LOWER 12" TO ALLOW FOR INTERIOR CABINETS.
 Locate: STREETSIDE INTERMEDIATE COMPARTMENT.



Selected Feature List 11902

Year: 2016
Chassis: Ford Type I, 189" WB, 108"
Model: 623

<u>Option</u>	<u>Std</u>	<u>Qty</u>	<u>Header/Description/Data</u>	<u>Proposal Total</u>
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	0	1	TOOL MOUNTING PLATE	
dr10d000			A .250" thick aluminum plate with a sanded D/A finish shall be installed in the designated area. The plate will be secured to the compartment wall using existing or additional extruded aluminum "C" channels. <i>Size: 24"W X 24"H.</i> <i>Location: CENTERED ON BACK WALL.</i>	
	0	2	EXTRUDED ALUMINUM "C" CHANNEL, ADDITIONAL	
dr10e000			Additional section of aluminum "C" channel shall be installed on the designated compartment walls. Standard tool boards require two C-channels. <i>Location: IN CONJUNCTION WITH TOOL PLATE.</i>	
CA640004	1	1	F623 STREETSIDE REAR: Clear Door Opening: 32.0" wide x 58.3" high Actual Dimensions: 36.6" wide x 61.5" high x 20.3" This area shall be accessed through double, outside hinged doors. Both doors shall have exterior door handles and latching devices:	
	0	1	TOOL MOUNTING PLATE	
dr10d000			A .250" thick aluminum plate with a sanded D/A finish shall be installed in the designated area. The plate will be secured to the compartment wall using existing or additional extruded aluminum "C" channels. <i>Size: 35"W X 24"H.</i> <i>Location: ON BACK WALL IN UPPER PORTION OF COMPARTMENT.</i>	
	0	2	EXTRUDED ALUMINUM "C" CHANNEL, ADDITIONAL	
dr10e000			Additional section of aluminum "C" channel shall be installed on the designated compartment walls. Standard tool boards require two C-channels. <i>Location: IN CONJUNCTION WITH TOOL PLATE.</i>	
CA640005	1	1	F623 CURBSIDE REAR: Clear Door Opening: 25.0" wide x 79.1" high Actual Dimensions: 29.7" wide x 82.1" high x 20.3" This area shall be accessed through a single outside hinged door	
	0	1	SWEEP OUT COMPARTMENT FLOOR MODIFICATION	
cb03so00			Compartment floor is to be sweep-out style. <i>!!! FLOOR TO BE FLAT ALUMINUM WITH A D/A FINISH.</i>	



Selected Feature List 11902

Year: 2016
 Chassis: Ford Type I, 189" WB, 108"
 Model: 623

Option	Std	Oty	Header/Description/Data	Proposal Total
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cb060w00	0	1	<p>COMPARTMENT WIDTH Modify: first, curbside</p> <p>The width of the standard compartment for this model shall be modified. This is the first curbside compartment width modification and shall include affected modifications to the interior cabinet.</p> <p><i>Width Modification: NARROW TO APPROXIMATELY 23" TO PROVIDE A 73" SQUAD BENCH LENGTH WITH DESIRED CRASH BARRIER COMPARTMENT WIDTH.</i></p> <p><i>Locate: CURBSIDE REAR COMPARTMENT.</i></p>	
dr08i000	0	1	<p>SHELF FOR LED LIGHTED VERTICAL COMPARTMENT</p> <p>A diamond plate adjustable shelf shall be installed in the following location.</p> <p><i>Locate: ABOVE FIXED SHELF FOR I/O ACCESS USE.</i></p>	
dr08lf00	0	1	<p>FIXED SHELF FOR LED LIGHTED VERTICAL COMPARTMENT</p> <p>Install a fixed diamond plate shelf using (4) cast shelf brackets, (2) for right wall, (2) for left wall. Install cast brackets directly to the compartment walls. Install shelf in the following location.</p> <p><i>Locate: 40" ABOVE COMPARTMENT FLOOR ON RH SIDE OF DIVIDER.</i></p>	
dr11a000	0	1	<p>DIVIDER FIXED, VERTICAL COMPARTMENT</p> <p>Install a 16" deep fixed vertical divider shall be installed in the location listed below. (Divider material is to match the compartment material)</p> <p><i>Locate: 11" FROM RH WALL.</i></p>	
dr11p000	0	1	<p>STAIR CHAIR POCKET, RECESSED</p> <p>A pocket that is 40" high x 10" wide x 1.2" deep shall be recessed into the inner compartment door panel. The pocket shall be installed as close to the bottom and hinged side of door as possible unless otherwise specified.</p> <p>Install a raised aluminum hat section on the floor of the stairchair storage area to raise the bottom of the stairchair to the bottom of the door pocket.</p> <p><i>Locate: HINGE SIDE OF CURBSIDE REAR COMPARTMENT DOOR.</i></p>	
CAB93AF0	0	1	<p>KKK-A-1822F CERTIFICATION LABEL</p> <p>The vehicle shall have weight/payload, electrical load and KKK-A-1822F certification stickers installed in the O2 compartment.</p>	

CB	BODY MODIFICATIONS/OPTIONS
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Selected Feature List 11902

Year: 2016
 Chassis: Ford Type I, 189" WB, 108"
 Model: 623

Option	Std	Qty	Header/Description/Data	Proposal Total
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CB030000 0 2 MODULE HEAD ROOM, INCREASE FROM STANDARD (PER INCH)

The standard module body headroom shall be increased from standard per the quantity listed below. The quantity represents inches and must be in increments of 2".

Headroom Required: 74".

Note: REAR DOORS TO REMAIN AT STANDARD 72" HEADROOM HEIGHT TO ALLOW CAMERA INSTALLATION BELOW M9 AMBER LIGHT.

CB06CC00 0 1 CRASH BARRIER SAFETY CONFIGURATION

The side access door is to be relocated at the forward most area on the curbside of the body. The space between the door and the head of the bench is to be occupied by an inside/outside access compartment with three standard shelves .125" thickness. The bench must remain a minimum of 72" long.

Size: ACTUAL DIMENSIONS: 31.5"W X 60"H.
 CLEAR OPENING: 28.8"W X 46.44"H X 20.3"D.

Note: MINIMUM 72" SQUAD BENCH LENGTH REQUIRED.

- !!! INSTALL A CUSTOM DIAMOND PLATE CRASH BARRIER COMPARTMENT BETWEEN THE SIDE ENTRY DOOR AND WHEEL HOUSE.
- !!! INTERIOR ACCESS TO EXTEND AS CLOSE TO FLOOR AS POSSIBLE.
- !!! INSTALL A SINGLE-HINGED DOOR WITH DUAL NADER PINS ON EXTERIOR.
- !!! INSTALL (3) ADJUSTABLE DIAMOND PLATE SHELVES IN COMPARTMENT WITH 1" LIPS. LOCATE (1) SHELF AT INTERIOR FLOOR LEVEL AND OTHER (2) SHELVES SPACED EVENLY ABOVE IN THE INSIDE/OUTSIDE ACCESS AREA.
- !!! INSTALL AN AISLE-FACING CARGO NET WITH ANKRA FASTENERS ON THE INTERIOR ACCESS.
- !!! INSTALL (1) AISLE-FACING DRAWER (IK040000) WITH BOTTOM MOUNTED SLIDES IN THE UPPER PORTION OF THE CRASH BARRIER PER DRAWING. SIZE: APPROXIMATELY 25.5"W X 5.5"H X 16"D (ALL I.D.)
- !!! TOP OF COMPARTMENT TO HAVE FLAT STAINLESS STEEL (NO LIPS) TO ALLOW FOR DEALER INSTALLATION OF A CORIAN COUNTERTOP.

CB06E000 0 1 SIDE DOOR: relocate

The side patient access door shall be relocated from its standard location.

Locate: FULL FORWARD FOR CRASH BARRIER CONFIGURATION.

CB06SX00 0 1 DOUBLE STEP CURBSIDE ENTRY 6" DROP SKIRT

The curbside skirt, forward of the rear wheel well shall be dropped 6 inches. Two integral ALD steps within the side patient door step well shall be available upon opening the side door for easier and lower access to the patient compartment.

CB09SA00 1 1 PATIENT AREA SOUND PROOFING/ACOUSTIC ENHANCEMENT PACKAGE

Install standard sound control package.



Selected Feature List 11902

Year: 2016
 Chassis: Ford Type I, 189" WB, 108"
 Model: 623

Option	Std	Qty	Header/Description/Data	Proposal Total
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CBR12E00 0 1 ELECTRICAL COMPARTMENT

Install a 21" high single door compartment above the oxygen compartment to house the add-on electrical equipment. Install a Cast Products vent in the compartment door. The streetside forward compartment height must be reduced 22" to accommodate this feature.

Note: for Cool Tech options with solar panel the electrical compartment will be 23" high and include a 2" notch in the top. The streetside forward compartment height must be reduced 24" to accommodate this feature.

Note: Must reduce the height of the #1 compartment

!!! NO COOL TECH ORDERED.

CBSP0001 0 1 SPECIAL INSTRUCTION, BODY MODIFICATIONS

!!! NOTE CG-TECH FOR INTERIOR DOOR PANEL FABRICATION.
 !!! NOTE 2-PIECE DOOR PANELS PER GF03C000.

D MODULE BODY HARDWARE

DG05C100 0 1 PRIVACY WINDOWS, REAR MODULE BODY ENTRY DOORS

The rear module entry doors shall have solid windows. The windows shall have dark privacy glass. The windows shall meet FMVSS glazing standards. No films are to be used.

DG06C200 0 1 PRIVACY WINDOW, SLIDING FOR SIDE ENTRY DOOR

The side entry door shall have a sliding window. The window shall have dark privacy glass. The window shall meet FMVSS glazing standards. No films are to be used.

DG080000 1 1 EMERGENCY DOOR RELEASE MECHANISM

Install standard emergency release knobs on top and bottom of rear module entrance doors.

DH000000 1 1 Vi-Tech body mounts

Install standard Vi-Tech body mounts.



Selected Feature List 11902

Year: 2016
 Chassis: Ford Type I, 189" WB, 108"
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Option	Std	Qty	Header/Description/Data	Proposal Total
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DH030000	0	1	STAINLESS STEEL SPLASH SHIELDS Install brushed stainless splash shields on the lower front face of the body just behind the cab access doors. These splash shields are to be the same height as the diamond plate front corner guards. <i>Note: Check option OL41B000 for running board light pre-punches.</i>	
DH04B000	0	1	MODULE BODY FENDERS: rubber Rubber extruded fenders shall be installed around the rear wheel well opening.	
DH050000	0	1	RUB RAILS, RUBBER The lower body rub rails shall be rubber ILOS.	
DH330000	1	1	REAR DOOR HOLD OPENS, GRABBER Install chrome Cast Products "Grabber" style rear door hold opens. (NOTE: HOLD OPENS MUST BE RELOCATED IF TELESCOPIC LIGHTS ARE ORDERED)	
DL48A000	1	1	ELECTRIC DOOR LOCKS: access doors Install power activated door locks on all patient area access doors. Locks to be activated by a switch at each patient area door, and controlled with the access door locks. Locks may be overridden by a manual slide lever or by the door key. A separate lock switch will be located in the front radio console if option DL48D000 (wired to OEM locks) is not selected.	
DL48B000	0	1	ELECTRIC DOOR SWITCH: concealed Install a concealed switch on the exterior of the vehicle to operate the power door lock circuit. Doors shall be wired to unlock only on this circuit. <i>Locate: REAR LICENSE PLATE BRACKET.</i>	
DL48D000	0	1	ELECTRIC DOOR LOCKS WIRED TO OEM SWITCHES The module door locks and compartment locks (if ordered) are to be wired to the chassis door lock switches.	
DLH40000	0	1	RECESSED LICENSE PLATE BRACKET A Cast Products #LP0002 recessed license plate bracket shall be installed per the attached drawing. The bracket will include lighting in the top to illuminate the license plate. <i>Locate: CENTER OF REAR RISER.</i>	



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DM100000	1	1	REFLECTORS: All patient compartment entry doors shall have red reflectors in the lower corner.	
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DR220000	1	1	RUBBER MATTING IN EXTERIOR COMPARTMENTS Ribbed rubber matting will be installed on the floor and shelves of all exterior compartments.	
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DR23A000	1	1	RUBBER COVERED WALLS IN BACKBOARD COMPARTMENT The interior of the backboard compartment is to be covered with rubber matting to protect equipment stored in this area. Color: BLACK.	
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E PAINT AND STRIPING				
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ED050000	0	1	MODULE PAINT: other color Paint MODULE special color Paint Color: RED. Paint Number: CROSSMATCH OEM RACE RED TO SIKKENS EQUIVALENT. Paint Color: WHITE. Paint Number: FLNA4002. !!! CHASSIS TO BE RECEIVED IN OEM RACE RED. DO NOT REPAINT CHASSIS. !!! DO NOT PAINT MODULE UNTIL CHASSIS ARRIVES. PAINT BODY TO MATCH OEM CHASSIS COLOR. !!! ALL FLAT PORTIONS OF MODULE ROOF TO BE PAINTED WHITE. NO WHITE PAINT TO BE VISIBLE FROM THE GROUND. ***CUSTOMER NOTE: IF A SPECIFIC COLOR OF RED IS REQUIRED, REQUIRING THE CHASSIS TO BE REPAINTED, THEN ADD \$1343.00.	
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ED20PS00	1	1	PAINT PANEL: sample A painted test panel shall be prepared with the following paint color and number, to confirm correct paint selection. The panel will be mailed as designated. Color: RED. Paint Number: CROSSMATCH OEM RACE RED TO SIKKENS EQUIVALENT.	
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G **INTERIOR COLORS, UPHOLSTERY AND SEATING**

GF01CG00 0 1 INTERIOR COLOR SCHEME: CG TECH

The patient area walls, inner door panels and inhalation panel are to be finished with CG Tech material. Install brushed stainless risers and lower door panels.

Color: POLAR WHITE.

GF01R000 0 1 INTERIOR COLOR SCHEME: Custom

Interior colors are as follows:

- FLOOR: TO BE DETERMINED.
- RISERS: STAINLESS STEEL.
- WALLS: CG-TECH POLAR WHITE.
- CABINET: WHITE PAINT - SMOOTH (NON-TEXTURED).
 WHITE ALUMINUM COMPOSITE - MAIN WALL AND SQUAD BENCH CEILING CABINET.
- UPHOLSTERY: EVS VACUUM FORMED - COLOR TO BE DETERMINED.

- !!! ALL CUSHIONS, INCLUDING THE ATTENDANT SEAT, SHALL BE EVS VACUUM FORMED MATERIAL.
- !!! INSTALL ALL SEAT CUSHIONS WITH FULL PERIMETER VELCRO - DO NOT USE PINS OR CLIPS.
- !!! ALL BACKREST CUSHIONS AND CLOSEOUT PANELS SHALL BE COVERED WITH A MATCHING EVS MATERIAL.
- !!! INSTALL BACKREST CUSHIONS AND CLOSEOUT PANELS USING STANDARD FASTENERS - DO NOT USE VELCRO.
- !!! PROVIDE A .500" OVERHANG ON THE SQUAD BENCH AND CPR SEAT CUSHIONS (IF EQUIPPED).

GF02M000 1 1 INSIDE CABINET FINISH: paint

The interior of all aluminum cabinets shall have a durable paint finish. The painted surface shall be washable and non-absorbent. See the appropriate Interior Color Scheme option for paint color information.

!!! SMOOTH (NON-TEXTURED).

GF02P000 1 1 RISERS:

The interior of this vehicle is to contain no wood or wood products. The risers are to be made of reinforced structural composite board covered with Formica per the color description. Formica will not be installed on the risers if stainless steel riser options are selected.

Note: Riser thickness to be modified if option #PL27CP00 is ordered.



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GF030000	0	1	STAINLESS STEEL RISER, cabinet wall Install stainless steel on cabinet wall riser. If a cabinet riser accent stripe is ordered the stainless steel will stop at the bottom edge of the stripe.	
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GF03A000	0	1	STAINLESS STEEL RISER, bench wall Install stainless steel on squad bench riser.	
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GF03C000	0	1	STAINLESS STEEL LOWER DOOR PANELS Finish lower section of inside patient compartment doors with stainless steel. If an interior accent stripe is ordered the stainless is to stop at the bottom of the stripe. <i>!!! INSTALL 2-PIECE INTERIOR DOOR PANELS FOR ALL THREE ENTRY DOORS. INSTALL LOWER STAINLESS STEEL DOOR PANELS SIZED TO COVER THE 2-PIECE DOOR PANEL SEAM.</i>	
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GK12D000	0	1	EVS CHILD SAFETY SEAT WITH 3-POINT SEAT BELT Delete the standard attendant seat cushion. Install a high back bucket seat with built in child seat restraints and 3-point occupant restraint. The seat to be adjustable front to rear. <i>!!! SEAT TO BE VACUUM FORMED. !!! SEAT BELTS TO BE RED. !!! INSTALL HEAT/AC GRATE WITH MACHINE SCREWS AND NYLOCK NUTS.</i>	
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GK140000	1	1	SEAT BELTS: Install standard seat belt package for use with HOPS. <i>Note: ALL SEAT BELTS TO BE RED ILOS.</i>	
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H4	INTERIOR CABINETRY, STREET SIDE, COMPOSITE All cabinetry to be fabricated from aluminum composite material. <i>Color: WHITE.</i>			
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HK000000 1 1 LINEN CLOSET

Install a vertical storage cabinet behind the attendant seat. The upper storage area shall house the primary electrical distribution area. The lower section shall be used for miscellaneous storage. Each area shall be accessed through hinged doors. The electrical distribution area shall include a Southco key lock/latching device. The entire cabinet shall be fabricated from aluminum and shall then be painted, unless otherwise specified.

Include the following provisions beneath the electronics cabinet (positioned from top to bottom):

- LOCKABLE STORAGE WITH (1) ADJUSTABLE SHELF. INCLUDE A 110-V AND A 12-VOLT OUTLET SURFACE MOUNTED ON UPPER BACK WALL - LH SIDE.
- AN APPROXIMATE 12"H STORAGE AREA TO HOUSE A SMITHWORKS IV WARMER. INCLUDE A SOLID RH HINGED DOOR.
- NORCOLD REFRIGERATOR IN BASE OF CABINET.

!!! ALL (3) DOORS OF THE LINEN CLOSET SHALL HAVE C2 LEVER LATCHES.
 !!! THE ELECTRONICS CABINET AND THE CABINET IMMEDIATELY BELOW IT SHALL HAVE LOCKABLE LATCHES.

HK001000 1 1 HOPS SYSTEM:

The unit is to be equipped with the HOPS system.

Note: Deletion of the system along with the selection of option #HK01A000 requires that the credit provided under HK01B000 be deleted.

HK001B00 1 1 RADIUS STYLE CABINET TRIM

Install smooth 1.5" radiused trim on all applicable interior cabinets.

HK01B000 1 1 STREETSIDE WALL WITH CPR SEAT

Street side wall to have CPR configuration. All cabinets over 14"H will include (1) standard adjustable shelf.
 SEE DRAWINGS

Configure: PER DRAWINGS.

- !!! STREETSIDE CABINETS TO BE 18"D UNLESS OTHERWISE SPECIFIED.
- !!! UPPER INHALATION CABINET TO HAVE (1) FIXED CENTER DIVIDER AND (1) ADJUSTABLE SHELF ON EACH SIDE OF THE DIVIDER.
- !!! UPPER REAR CEILING CABINET TO BE APPROXIMATELY 51.75"W X 22"H.
- !!! CPR SEAT WIDTH TO BE 34" TO CREATE A 14" WIDE REAR COUNTERTOP AREA.
- !!! INCLUDE A LOWER INHALATION AREA CABINET WITH SLIDING PLEXIGLAS DOORS FORWARD OF PULL-OUT WRITING SURFACE.



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HK01R000	0	2	LIFT UP CABINET FRAMES: street-side Fasten the Plexiglas door frame extrusion to a piano hinge secured to the cabinet wall. The complete frame and doors shall hinge open upward providing total access to the cabinet behind. The frame shall be held open with gas charged cylinders and secured in the down position with a sliding dead bolt. <i>Locate: BOTH UPPER CEILING CABINETS.</i>	
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HK01T000	0	1	RECESSED-PULL OUT ASPIRATOR STORAGE IN MAIN CABINET RISER The vehicle aspirator shall be install in a pull out drawer in the main cabinet riser. A recessed pocket will provide the storage for the pull out drawer and the pocket will be trimmed with aluminum diamond plate inside the exterior compartment. <i>Locate: IN RISER BELOW AIRWAY DRAWERS - JUST FORWARD OF CPR SEAT.</i> <i>Door Style: PLEXIGLAS WITH NOTCH IN UPPER RH CORNER FOR SUCTION TUBING.</i> <i>Size: 8.75"W x 9"H x 11"D (ALL I.D. DIMENSIONS).</i> <i>Note: INSTALL USING ACCURIDE SIDE MOUNTED SLIDES.</i>	
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HK01U000	0	1	PULL OUT WRITING TABLE A pull out writing table will be installed below the inhalation counter top. The writing surface is to be positioned for use at the attendant seat location. <i>Locate: THROUGH THE MAIN WALL RISER FOR USE FROM THE ATTENDANT SEAT. POSITION AS HIGH AS POSSIBLE.</i> <i>!!! INCLUDE A SMALL PULL RING LATCH (NOT SOUTHCO) AND A LARGE BALLCATCH LATCH TO HOLD IT IN THE CLOSED POSITION. INSTALL WITH LOCKING SLIDES TO HOLD IT IN THE OPEN POSITION.</i> <i>!!! WRITING TABLE SIZE: 13.5"W x 16"D. (I.D.)</i> <i>!!! INSTALL A SMALL TRIM LIP ON THE REAR FACING EDGE TO HELP HOLD THE COMPUTER IN PLACE.</i>	
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HK02LU00	0	1	STORAGE BENEATH THE CPR SEAT A trimmed storage area shall be accessed by raising the hinged side attendant seat base cushion. <i>!!! CUSHION OVERHANG TO BE .500".</i>	
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HK02LU10	0	1	RECESSED PADDLE LATCH FOR ACCESS BELOW THE CPR SEAT CUSHION A recessed paddle latch will be installed in the cabinet riser to provide access to the CPR seat storage area.	
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HK02LU20	0	1	GAS HOLD OPEN FOR LIFT UP CPR SEAT CUSHION A gas holdopen shall be installed to keep the CPR seat cushion in the upright position when opened.	
HK03D000	0	2	PULL OUT DRAWER INSTALLATION IN MAIN CABINET WALL Pull out drawer(s) shall be installed in the main cabinet wall at the locations listed below. <i>Locate: (2) DRAWERS STACKED ABOVE THE SUCTION DRAWER JUST FORWARD OF THE CPR SEAT PER DRAWING.</i> <i>Size: 9.25"W x 3.625"H x 16"D (ALL I.D. DIMENSIONS)</i> <i>Note: INSTALL USING ACCURIDE SIDE-MOUNTED SLIDES. ALL SURFACES OF DRAWER TRAYS TO BE NON-PAINTED WITH A D/A FINISH.</i>	
HK11B000	0	1	LOCKING DOOR: single Install a single lockable door. Include a National key lock. <i>Locate: UPPER CENTER PORTION OF LINEN CLOSET.</i> <i>Note: INCLUDE A LOCKING C2 LEVER LATCH ILOS.</i>	
HK160000	0	1	EXTRA SHELF: interior Install an additional adjustable shelf. <i>Locate: UPPER INHALATION CABINET.</i>	
HK16A000	0	1	DIVIDER: interior Install a cabinet divider as noted below. <i>Locate: CENTER OF UPPER INHALATION CABINET.</i>	
HK17G000	0	1	SHARPES/WASTE STORAGE IN A TIP OUT DOOR IN MAIN CABINET WALL Install both sharps and waste containers on a tip-out door within the main cabinet wall. The door shall be specifically located as noted below. <i>Locate: BELOW REAR COUNTERTOP PER DRAWING.</i> <i>Size: 14"W X 17.5"H X 18"D (O.D. DIMENSIONS).</i> <i>TO HOUSE A WINFIELD #185 SHARPS CONTAINER AND A RUBBERMAID #2953 6-QUART WASTE CONTAINER.</i>	

INTERIOR CABINETS AND SQUAD BENCH, CURB SIDE



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IA200000	1	1	SQUAD BENCH STORAGE: Storage shall be provided under the bench cushions. The area shall run where possible under the bench. The storage pan shall be fabricated from aluminum and shall be accessed by raising the split cushions. <i>!!! MINIMUM 72" SQUAD BENCH LENGTH REQUIRED.</i>	
ig05f000	0	1	SQUAD BENCH: DELETE CUPS AND WELLS A 22" wide x 72" long bench cushion shall be provided on the curb side of the patient area. The cushion shall be split. No provisions for stretcher cups and wells are provided. <i>!!! CUSHION OVERHANG TO BE .500".</i>	
IG080000	0	1	BENCH HEAD END (OTHER) OPTIONS: The following bench restraint shall be installed at the head end of the squad bench.	
ig09e000	0	1	CARGO NET AT HEAD OF BENCH Install a cargo net at the head of the bench. Net to be removeable for cleaning purposes. <i>Locate: FOR INTERIOR ACCESS TO CRASH BARRIER COMPARTMENT. INCLUDE ANKRA FASTENERS.</i>	
IG10A000	1	1	BENCH HOLD OPENS: gas Install gas spring hold opens on squad bench lid.	
IG10Q000	1	1	BENCH HOLD-DOWN: paddle latches (Pair) Install recessed paddle latches into the squad bench riser to retain the squad bench lids in the closed position. The latches shall be both passive and positive.	
IG15B000	1	1	BENCH BACK CUSHION: full size Delete individual squad bench back cushions. Install full size cushion.	



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ISP00001 0 1 SPECIAL INSTRUCTION, CURBSIDE CABINERY

Provide and install a ceiling cabinet forward of the squad bench ceiling cabinet to house the following:

- !!! FORWARD PORTION TO PROVIDE STORAGE FOR (3) GLOVE BOXES MOUNTED VERTICALLY. INCLUDE A HINGE-UP PLEXIGLAS LID WITH OVAL ACCESS OPENINGS.
- !!! TO ACCOMMODATE A GLOVE BOX SIZE OF 10.5"H X 5.5"W X 4"D (UNLESS OTHERWISE SPECIFIED).
- !!! REAR PORTION TO HOUSE A CUSTOMER-SUPPLIED MED-VAULT 2 WITH WI-FI CAPABILITIES.
- !!! MED-VAULT TO BE WIRED TO A FUSED BATTERY HOT CIRCUIT.
- !!! SIZE: APPROXIMATELY 15"W X 11.625"H X 11"D (O.D.)

K MODULE INTERIOR ACCESSORIES AND TRIM

KG02B000 2 2 IV HANGER, CAST PRODUCTS WITH RUBBER ARM

Cast products recessed IV hangers with rubber arms for attaching solution bags shall be installed in the designated locations.
 Locate: (1) EACH SIDE OF COT IN CENTER POSITION AT APPROXIMATELY WAIST LEVEL.

KG09B000 1 1 CEILING GRAB RAIL: standard (2) 2ft rails or (1) 6ft rail anti-microbial

Cabinet wall "A" will have (1) 6' rail over cot as standard.
 Cabinet wall "B" will have (2) 2' rails, 1-forward and 1-to rear of CPR seat.
 Locate: (1) 6' RAIL ABOVE COT IN CENTER POSITION ON CURBSIDE EDGE OF CENTER CEILING PLATE.

KG09I000 0 2 VERTICAL GRAB RAIL INSTALLATION, 2 FT. ANTI-MICROBIAL

Install 2' stainless steel grab rail with anti-microbial coating.
 Locate: (1) ON REAR OF MAIN WALL FOR ENTRY ASSIST.
 (1) ANGLED ON THE CRASH BARRIER COMPARTMENT - FACING THE STEPWELL.

KG09K000 1 1 PATIENT DOOR GRAB RAILS: angled with anti-microbial coating

All patient access doors to have heavy duty angled stainless steel grab rails with smooth radius corners and flange mounting and anti-microbial coating.



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KG110000	1	1	FLOOR TRIM Trim floor with cove molding at non rolled areas of floor.	
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KG120000	1	1	CABINET TRIM Trim all vertical and horizontal edges.	
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KG16B000	1	1	CEILING MATERIAL, PLATINUM WHITE ALUMINUM COMPOSITE The standard module ceiling material shall be platinum white aluminum composite.	
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KG17A000	2	2	FIRE EXTINGUISHERS: 5lb ABC Supply five pound fire ABC extinguisher(s). Ship loose, or identify mounting location(s). <i>Locate: SHIP LOOSE.</i>	
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KL33B000	0	1	REAR RADIO SPEAKERS Install two rear speakers in patient compartment. Volume control to be integral to the rear switch panel and controlled by individual up/down momentary switches. <i>Locate: (1) FRONT AND (1) REAR OF PATIENT AREA CEILING NEAR CENTER LINE.</i>	
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L	COT MOUNTS AND ACCESSORIES			
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LG03F000	0	1	COT MOUNT, SINGLE: Stryker Install Stryker cot mount for single position. <i>Cot Model: POWER PRO.</i> <i>Locate: CENTER POSITION - SPACED EVENLY BETWEEN RISERS.</i> <i>Note: FLOOR TO BE DESIGNED TO ACCOMMODATE THE POSSIBLE FUTURE INSTALLATION OF A STRYKER POWER LOAD SYSTEM.</i>	
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M	LIGHTBARS FRONT AND REAR			
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MAM60000	0	1	INSTALL CUSTOMER SUPPLIED TRAFFIC EMITTER A customer-supplied traffic emitter shall be installed as noted below: Model: GTT - EXACT MODEL # TO BE DETERMINED. Location: CENTERED ON FRONT FACE JUST BELOW WHITE LED. Configure: WIRE THROUGH NEUTRAL SAFETY SWITCH TO CANCEL IN "PARK". INCLUDE SEPARATE ON/OFF SWITCH LABELED "OPTICOM". UNIT TO ALSO BE WIRED INTO THE CHASSIS TURN SIGNAL CIRCUITRY.	
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MM	ELECTRICAL EMERGENCY VISUAL WARNING SYSTEMS		
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MM030000	1	1	WHELEN M SERIES LIGHTING OPTIONS: !!! ALL M-SERIES LIGHTS TO HAVE BLACK FLANGES ILO CHROME FLANGES.	
mm06r000	0	2	M6 SERIES LED, RED Whelen M6 series L.E.D. lights shall be installed in the designated locations. Location: IN REAR RISER - IN LINE WITH TAIL LIGHTS. Lens color: RED.	
mm07r000	0	4	M7 SERIES LED, RED Whelen M7 series L.E.D. lights shall be installed in the designated locations. Location: (2) ON UPPER BAR OF CHASSIS GRILLE. (2) ON LOWER BAR OF CHASSIS GRILLE. Lens color: RED. Note: FLASH LIGHTS IN AN "X" PATTERN.	
mm07rw00	0	4	M7 SERIES LED, RED/WHITE SIDE TO SIDE SPLIT Whelen M7 series L.E.D. lights shall be installed in the designated locations. Location: (1) EACH SIDE ON CHASSIS FENDER - WHITE PORTION FORWARD. (1) EACH SIDE ABOVE REAR WHEEL WELL - WHITE PORTION FORWARD.	
mm09a000	0	1	M9 SERIES LED, AMBER Whelen M9 series L.E.D. lights shall be installed in the designated locations. Location: ABOVE REAR DOORS. Lens color: TO BE DETERMINED. Note: LIGHT TO FLASH IN "PARK" ONLY PER WI TRANS 309 REQUIREMENTS.	



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mm09r000	0 12	M9 SERIES LED, RED Whelen M9 series L.E.D. lights shall be installed in the designated locations. <i>Location:</i> (4) ON FRONT FACE - INBOARD OF RED/WHITE SPLIT M9s. (2) STREETSIDE - UPPER OUTBOARD CORNERS. (2) CURBSIDE - UPPER OUTBOARD CORNERS. (2) REAR FACE - UPPER OUTBOARD CORNERS. (2) REAR FACE - WINDOW LEVEL. <i>Lens Color:</i> RED. <i>Front Layout:</i> RW/R/R/W/R/R/RW <i>Note:</i> FRONT FACE RED LIGHTS TO BE PROGRAMMED FOR TOP/BOTTOM FLASH. WIRE TO FLASH IN AN "X" PATTERN - STREETSIDE INNER UPPER WITH CURBSIDE OUTER LOWER. REAR FACE LIGHTS TO FLASH IN AN "X" PATTERN WITH SOLID FLASH.	
mm09rw00	0 2	M9 SERIES LED, RED/WHITE SIDE TO SIDE SPLIT Whelen M9 series L.E.D. lights shall be installed in the designated locations. <i>Location:</i> (2) FRONT FACE - OUTBOARD POSITIONS. <i>Note:</i> LIGHTS TO FLASH IN AN "X" PATTERN. CURBSIDE RED WITH STREETSIDE WHITE.	
mm09w000	0 1	M9 SERIES LED, WHITE WITH CLEAR LENS Whelen M9 series L.E.D. lights shall be installed in the designated locations. <i>Location:</i> CENTER OF FRONT FACE. <i>Note:</i> FLASH LIGHT TOP TO BOTTOM USING ALTERNATING DOUBLE FLASH PATTERN.	
MM10L000	0 1	L.E.D. WARNING: patient entry door, single color (EACH) Install custom made L.E.D. light strips on the interior patient entry door panels. The light strips shall have a integral lens strip for maximum light disbursement. Installation will be flush with the inner door panel. The light strip shall flash when the door is open, and the red flashing light circuit is activated. <i>Color:</i> RED. <i>Locate:</i> LOWER PORTION OF CURBSIDE ENTRY DOOR. <i>Activate:</i> WITH DOOR OPEN AND AND VEHICLE RUNNING.	
MM26SHH0	0 1	HOUSING FOR 700 SERIES LED INTERSECTION LIGHTS Cast Products #LH37774-1 & LH37775-1 polished housings for Whelen 700 series LED's shall be installed on the chassis fenders. !!! INCLUDE HORTON FABRICATED ADAPTER PLATE TO ACCOMMODATE M7 LIGHTS.	



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MM7A0000	0	1	M SERIES CONTROL WIRE Install a control wire to change flash patterns on M series lights as noted below. <i>Lights: PROGRAM ALL M-SERIES LIGHTING, UNLESS OTHERWISE NOTED, FOR ALTERNATING STANDARD RATE DOUBLE FLASH (75 FPM) WITH ABILITY TO SET THE INTERNAL FLASH PATTERNS AT THE LIGHT HEADS.</i>	
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MM800000	0	1	WHITE LIGHT CUTOFF SWITCH A switch shall be installed in the front control panel that will deactivate all forward facing white flashing lights. This include wig-wag flashers if ordered.	
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MMSP0010	0	1	SPECIAL INSTRUCTION, VISUAL EMERGENCY WARNING SYSTEMS <i>Program all White emergency lighting and the pre-emption system to be "OFF" in "Park" (Secondary Mode).</i>	
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MN AUDIBLE EMERGENCY WARNING SYSTEMS				
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MN09F500	0	1	AIR HORNS: under bumper FORD F-SERIES Install (2) Buell #1061 10" trumpets under the front bumper recessed into the air dam valance. System to include a #16122 compressor and remote air tank. The air horns can only be activated when the vehicle is in gear. <i>Configure: INSTALL (1) 10" AND (1) 12" TRUMPET BELOW BUMPER PER QVM REQUIREMENTS. INCLUDE A VELVAC #035105 RESERVOIR TANK. Activate: VIA (2) CHROME SWITCHES IN FRONT CONSOLE.</i>	
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MN35EF20	0	1	SIREN, FEDERAL EQ2B Specified siren to be: Federal EQ2B 200 watt electronic with digital output control. <i>Locate: FORWARD PORTION OF CONSOLE FACEPLATE - LH CORNER. SIREN AMPLIFIER ON FLOOR OF ELECTRONICS CABINET.</i>	
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MN40A000	1	1	SIREN INSTALL: The siren listed above shall be supplied and mounted as defined.	
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MN45DH00	1	1	SIREN SPEAKERS: C.P. SAD3806 & SAP3806 RECESSED IN BUMPER Cast Products #SAD3806 & SAP3806 siren speakers shall be recess mounted into the ends of the chassis bumper. <i>For use with current model F-Series chassis.</i>	
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MN50A000	1	1	SPEAKER INSTALL: bumper Speaker Selections (Bumper Installed)	
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O LIGHTS				
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OL09M000	0	1	KKK SIDE BODY MARKER LIGHTS, M6 SERIES L.E.D. (PAIR) Install red Whelen L.E.D. M6 series turn/marker lights on each rear side of the module body. Lights provide module body night time side lighting visibility and turning signal indication. <i>Locate: ON REAR COMPARTMENT DOORS PER DRAWINGS.</i>	
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OL34L000	1	1	LED EXTERIOR COMPARTMENT LIGHTING All exterior compartments will be lighted with LED strip lighting. A vertical strip will be installed inside both sides of each compartment. The lights shall be directed toward the back of the compartment. <i>Note: This does not include wheelwell compartments.</i>	
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OL350000	0	1	ICC MARKER LIGHTS LED ICC marker lights shall be installed.	
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OM30D000	0	1	TAIL LIGHTS, M6 SERIES Whelen M6 series brake/tail, amber arrow turn and backup lights to be installed in the selected location. <i>Locate: STACKED ON BODY ABOVE RISER. TURN ARROWS. BRAKE/TAIL. REVERSE.</i> <i>Note: ALL LIGHTS TO HAVE BLACK FLANGES ILOS CHROME.</i>	
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Selected Feature List 11902

Year: 2016
 Chassis: Ford Type I, 189" WB, 108"
 Model: 623

Option	Std	Oty	Header/Description/Data	Proposal Total
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OSP00001 0 1 SPECIAL INSTRUCTION, ELECTRICAL

Install a brake and turn signal indicator cluster on a removable panel in the header panel above the rear entry doors. Include (1) Red and (2) Amber Whelen OS LED marker lights with chrome flanges and clear lenses.

Locate: CENTERED BELOW INTELLITEC CLOCK.

OSP00002 0 1 SPECIAL INSTRUCTION, ELECTRICAL

Provide and install Whelen Pioneer Slimline series lights #PSL1R5B lights as described below.

Quantity: (6)

Locate: (2) PER SIDE AS SCENE LIGHTS.

(2) ABOVE REAR DOORS AS LOAD LIGHTS.

Note: WIRE TO FUNCTION PER STANDARD LOAD AND SCENE LIGHT FUNCTIONS.

***CUSTOMER NOTE:

THESE LIGHTS HAVE A BLACK HOUSING BUT ARE NOT AVAILABLE WITH A BLACK FLANGE. IN ADDITION, THEY ARE A SEMI-RECESSED LIGHT.

P	ELECTRICAL POWER GROUP			
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PAL00006 1 1 INTELLIPLEX i4G:

Install the Intelliplex i4G electrical system. System is to include a USB port installed under the dash for ease of programming.

PAL00A00 0 1 MODULE DISCONNECT DEFAULT

The module disconnect circuit shall default to the "on" position when the battery switch is activated.

PAL00T20 0 1 Three switch panel, i4g

Install an i4G 3-switch panel as noted below.

Location: ON CRASH BARRIER COMPARTMENT - REAR FACING AT HEAD OF BENCH.

Switch Functions: (1) SWITCH FOR EACH BANK OF DOME LIGHTS.
 (1) SWITCH FOR OXYGEN ACTIVATION.



Selected Feature List 11902

Year: 2016
Chassis: Ford Type I, 189" WB, 108"
Model: 623

<u>Option</u>	<u>Std</u>	<u>Qty</u>	<u>Header/Description/Data</u>	<u>Proposal Total</u>
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PAL38E00	0	1	SHORELINE, KUSSMAUL SUPER AUTO EJECT, 20 AMP NON ARCING Install a 20 amp Kussmaul non-arcing auto ejection shoreline receptacle. <i>Locate: STREETSIDE OF MODULE BODY AS FAR FORWARD AS POSSIBLE.</i> <i>Color: RED.</i>	
PAL38Q00	0	1	EZ PLATE Install the shoreline using a Kussmaul EZ Plate. <i>Note: INCLUDES POWER INDICATOR LAMP.</i>	
PAL39000	0	1	WIRE ENGINE BLOCK HEATER: Wire engine block heater to shoreline. Make provisions to disable engine block heater when desired.	
PAL40000	1	1	EXTRA 12VDC CIRCUIT BREAKER: An extra circuit breaker shall be installed.	
PL10A000	1	1	SWITCH PANEL MOUNT: Switch panel to be flush mounted in upper face of console.	
PL11A000	0	1	LIGHT PROGRAMMING: side rear scene The side rear scene lights shall be wired to transmission reverse, plus standard mode of operation.	
PL11B000	0	1	LIGHT PROGRAMMING: right side scene The right side scene lights shall come "on" when the side patient door is opened.	
PL11C000	0	1	LIGHT PROGRAMMING: load light The rear load lights shall be wired to transmission reverse, plus the standard mode of operation.	
PL11F000	1	1	PROGRAMMING: AUDIBLE LOW VOLTAGE ALARM Program an audible alarm to activate if the voltage drops below 11.8 volts for 120 seconds.	



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Option	Std	Qty	Header/Description/Data	Proposal Total
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PL11G000	1	1	LIGHT PROGRAMMING: park brake A warning shall display on the front console readout, advising to set the Parking Brake, should the modular disconnect switch be "ON" and the transmission placed in "PARK" or "NEUTRAL". It will also advise to Disengage the Parking Brake should the vehicle be placed into gear. <i>Configure: wire alarm to activate with red flasher circuit</i>	
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PL19A000	1	1	REPORT LIGHT Install a 12" Tecniq LED bar style report light. <i>Locate: ABOVE REAR CONTROL PANEL.</i> <i>!!! ILOS, INSTALL (1) TECNIQ #E13-WS0R-1 RED/WHITE FLUSH MOUNT LED LIGHT WITH STAINLESS STEEL TRIM RING.</i> <i>!!! EACH COLOR OF LIGHT TO BE SWITCHED SEPARATELY FROM REAR CONTROL PANEL.</i>	
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PL20E000	0	1	CLOCK, INTELLITEC LED Install an Intellitec Time Manager clock. <i>Locate: ABOVE REAR DOORS.</i>	
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PL22B000	0	1	STEP WELL LIGHT: Install a Tecniq step well light above the step well in the headliner. Light to activate with door open. <i>Locate: RH SIDE OF STEPWELL - BELOW FRONT WALL.</i> <i>Note: LIGHT TO BE A TECNIQ #E06-WS00-1 4" ROUND LED LIGHT WITH STAINLESS STEEL TRIM RING ILOS.</i>	
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PL230000	0	1	CAB CEILING LIGHTS: Two (2) Signal Stat lights shall be installed in the cab headliner and shall have an individual switch on each light.	
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PL24T000	0	1	LAMP TIMER: programmable Install an electronic momentary touch timer switch where specified. The switch will enable time limited operation of the below listed lights, with the battery switch in the off position. <i>Locate: ON CRASH BARRIER COMPARTMENT FACING STEPWELL.</i> <i>Light(s) Controlled: DOME LIGHT.</i> <i>Configure: SET ELAPSED TIME FOR 15 MINUTES FOR INITIAL SET-UP.</i>	
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Selected Feature List 11902

Year: 2016
 Chassis: Ford Type I, 189" WB, 108"
 Model: 623

Option	Std	Oty	Header/Description/Data	Proposal Total
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|----------|---|----|--|--|
| PL26D000 | 0 | 1 | NORCOLD REFRIGERATOR
Install a Norcold NR740BB-H 12V refrigerator in the location noted below.
<i>Locate: BASE OF LINEN CLOSET PER DRAWING.</i> | |
| PL27CA00 | 0 | 10 | PATIENT CEILING DOME LIGHTS: WHELEN LED
Install Whelen 80C0EHCR LED dome lights in the patient area ceiling per the locations listed below. The lights will have a high/low feature with the switch in the rear control panel.
<i>Locate: (5) OVER STREETSIDE EDGE OF COT IN CENTER POSITION.
 (5) OVER CURBSIDE EDGE OF COT IN CENTER POSITION PER DRAWING.</i>

<i>!!! LIGHTS TO OPERATE AT "HIGH" INTENSITY WHEN DOME TIMER IS MANUALLY ACTIVATED.
 !!! WITH BATTERY SWITCH IN "OFF" POSITION, PROGRAM THE DOME LIGHTS TO ACTIVATE AT "LOW" INTENSITY WHEN A MODULE ACCESS DOOR IS OPENED. LIGHTS TO REMAIN ON FOR (5) MINUTES.
 !!! INCLUDE SEPARATE DIMMER SWITCHES FOR EACH BANK OF LIGHTS.</i> | |
| PL29P000 | 0 | 2 | PREWIRE FOR FUTURE STREAMLIGHT INSTALLATION
Prewire shall be provided as noted below for future installation of Streamlight charger bases.

<i>Quantity: (2)
 Locate: UPPER LH CORNER OF STREETSIDE REAR COMPARTMENT.
 Configure: SHORELINE HOT AND IGNITION ACTIVATED.
 Note: LEAVE 24" COILED AND CAPPED.</i>

<i>!!! FUSE LOCATIONS TO BE EASILY ACCESSIBLE TO CUSTOMER.</i> | |
| PM110000 | 0 | 1 | LIGHT PROGRAMMING: brake light
Lower red flashing lights to operate as brake lights when not in the warning mode.

<i>Lights: WINDOW LEVEL M9s.
 Configure: FLASHERS TO OVERRIDE BRAKE LIGHT FUNCTION.
 Note: FEATURE TO ACTIVATE REGARDLESS OF MODULE DISCONNECT SWITCH STATUS</i> | |

Q	HEATING, AIR CONDITIONING AND INTERIOR ENVIRONMENT
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| QH280000 | 0 | 1 | BRUSHLESS MOTOR
The vehicle heat/ac unit selected below is to include a brushless motor ILOS. | |
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Selected Feature List 11902

Year: 2016
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 Model: 623

Option	Std	Qty	Header/Description/Data	Proposal Total
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QH29F100	1	1	HEAT/AC SYSTEM FORD F SERIES CHASSIS Install a ProAir 12V heat/AC system below the attendant seat. Unit is to include a replaceable filter at the air intake point.	
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QH29F500	1	1	RECEIVER/DRYER KIT, ALL FORD F-SERIES CHASSIS Receiver/dryer kit #63 000 744 w/pressure switch shall be installed in the air conditioning system.	
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QL43N000	1	1	NFPA COMPLIANT VENTING-CAST VENTS Install an NFPA style venting system to include a 400cfm exhaust vent along with a static vent. Install 9.5" square polished covers on both vents per drawings.	
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R CAB CONSOLE AND COMMUNICATIONS				
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RJ01A000	1	1	ANTENNA COAX 1: An RG 58U coax shall be installed so that the ambulance conversion need not be disassembled. <i>Exterior Termination: FRONT CENTER OF MODULE ROOF.</i> <i>Interior Termination: ELECTRONICS CABINET.</i> !!! DEALER TO INSTALL A CUSTOMER SUPPLIED CRADLEPOINT DOME ANTENNA/COAX IN THIS LOCATION. COAX TO SERVE AS A "FISH-TAPE" FOR DEALER.	
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RJ01B000	0	1	ANTENNA COAX 2: Install additional antenna coax and accesses. <i>Exterior Termination: SPACED EVENLY ON MODULE ROOF.</i> <i>Interior Termination: BEHIND DRIVER'S SEAT.</i>	
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RJ01C000	0	1	ANTENNA COAX 3: Install additional antenna coax and accesses. <i>Exterior Termination: SPACED EVENLY ON MODULE ROOF.</i> <i>Interior Termination: BEHIND INHALATION PANEL.</i>	
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RJ01D000	0	1	ANTENNA COAX 4: Install additional antenna coax and accesses. <i>Exterior Termination: REAR CENTER OF MODULE ROOF.</i> <i>Interior Termination: BEHIND DRIVER'S SEAT.</i>	
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Selected Feature List 11902

Year: 2016
Chassis: Ford Type I, 189" WB, 108"
Model: 623

<u>Option</u>	<u>Std</u>	<u>Qty</u>	<u>Header/Description/Data</u>	<u>Proposal Total</u>
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RJ03MM00	0	3	ANTENNA: K-94 mount. Supply and install K-94 antenna base as designated. This option is less the antenna coax. An antenna coax option must be selected to be installed. Coax Number(s): 2, 3, 4 ONLY.	
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RJ040000	1	1	RADIO CABLE PULL WIRE A standard pull wire for radio installation shall be installed from behind the driver's seat to behind the inhalation panel.	
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RJ05B000	1	1	FRONT CONSOLE: A console shall be fabricated to coordinate with the interior cab color. Room shall be provided on the face of the console for installation of radio and siren controls.	
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RJ05C000	0	1	CONSOLE EXTENSION: A box for mounting radio heads and storage maps, books, or binders will be attached to the standard engine cover console. Standard box includes (3) 2.5 storage sections, and 8.5 faceplate for radio mounting. !!! CONSOLE TO INCLUDE (2) SEPARATE HORIZONTAL FACEPLATES. FRONT PLATE TO BE APPROXIMATELY 15"L. REAR PLATE TO BE APPROXIMATELY 13"L - LEAVING ROOM FOR CLIP BOARD SLOT TO BE MOUNTED ON REAR CAB WALL. Locate: EQ2B SIREN HEAD IN UPPER LH CORNER. LEAVE ADDITIONAL SPACE REARWARD OF SIREN FOR A DEALER INSTALLED MOTOROLA RADIO HEAD AND A FIRECOM CONTROL HEAD. Note: ALL COMPONENTS OF FORWARD HORIZONTAL FACEPLATE TO BE ON LH SIDE OF PLATE, LEAVING RH PORTION OF FACEPLATE CLEAR. !!! THE FOLLOWING PROVISIONS SHALL BE INCLUDED ON THE REAR FACEPLATE. Locate: (2) CUPHOLDERS FRONT TO BACK IN RH PORTION. (1) KNOX SENTALOCK IN UPPER LH CORNER - DEALER INSTALLED. (1) KNOX KEY BOX IN LOWER LH CORNER - DEALER INSTALLED.	
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RJ05CP00	0	1	CUP HOLDERS Install two (2) large cup holders recessed into the console per drawing.	
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RJ080000	1	1	RADIO POWER/GROUND: Install 6 gauge cable to positive and ground studs for radio power. Locate: FRONT CONSOLE. Configure: BATTERY HOT.	
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Selected Feature List 11902

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Option	Std	Qty	Header/Description/Data	Proposal Total
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RJ08A000 0 2 RADIO POWER/GROUND:

Install three 6 gauge cables to positive and ground studs for radio power. Wire one constant hot, one ignition hot and one ground.

Locate: BEHIND DRIVER'S SEAT (AS LOW AS POSSIBLE).
 BEHIND INHALATION PANEL.

Configure: BATTERY HOT, IGNITION HOT, AND GROUND.
 FUSES TO BE EASILY ACCESSIBLE TO CUSTOMER.

***CUSTOMER NOTE: EACH POWER STUD HAS A 50-AMP CAPACITY.

RSP00001 0 1 SPECIAL INSTRUCTION, CAB CONSOLE & COMMUNICATIONS

Blank removable panels shall be installed for future installation of radio heads.

Locate: FORWARD PORTION OF INHALATION PANEL.

Note: EXACT LOCATION TO BE DETERMINED AT PRE-BUILD CONFERENCE DUE TO CONFLICTING LOCATIONS DESCRIBED IN RFP (PAGE 7).

RSP00002 0 1 SPECIAL INSTRUCTION, CAB CONSOLE & COMMUNICATIONS

Install a (1) slot angled clip-board holder on the rear cab wall between the cab seats. Holder to be coated in Black Scorpion material.

Locate: CENTERED ABOVE THE REAR OF THE CONSOLE EXTENSION.

Size: 14"W x 8"H x 1.75"D (O.D. DIMENSIONS)

RSP00003 0 1 SPECIAL INSTRUCTION, CAB CONSOLE & COMMUNICATIONS

!!! HORTON TO RECEIVE A SPOOL OF 6C FIRECOM INTERCOM CABLE. INSTALL CABLE FROM THE FRONT CONSOLE TO THE INTENDED HEADSET LOCATIONS LISTED BELOW.

Locate: (1) ABOVE INBOARD CORNER OF EACH CAB SEAT.
 (1) ON UPPER CURBSIDE WALL ADJACENT TO FORWARD SEATING POSITION.
 (1) ON MAIN WALL BELOW UPPER REAR CEILING CABINET ADJACENT TO CPR SEAT.
 (1) ON INHALATION AREA WALL PER DRAWING.

Note: TERMINATE ALL LOCATIONS IN MODULE WITH A STAINLESS STEEL COVER AND RUBBER GROMMET, LEAVING AN 18" SERVICE LOOP.
 COIL CABLES IN CAB NEAR REAR OF HEADLINER FOR EASY ACCESS.

!!! HEADSET INSTALLATION AND FINAL HOOK-UPS TO BE HANDLED BY DEALER.

T	OXYGEN AND SUCTION
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Selected Feature List 11902

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Option	Std	Oty	Header/Description/Data	Proposal Total
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TN002000	1	1	OXYGEN BOTTLE MOUNT, VERTICAL TRACK FOR QRM-V Vertical track for mounting of a QRM-V O2 bottle mount shall be welded on the back wall of the compartment in the right hand corner. The O2 bottle mount is adjustable for "M" or "H" size tanks.	
TN01MW00	1	1	OXYGEN BOTTLE, CYLINDER BRACKET: Zico Zico QRM-V oxygen bracket shall be installed. <i>Locate: STANDARD LOCATION.</i> <i>Tank Size: "M".</i>	
TN020000	0	1	OXYGEN ACCESS: A clear Plexiglas door shall be provided in the patient area wall for access to the oxygen cylinder valve. The door shall be hinged so that it swings into the oxygen cylinder storage compartment. The opening shall be trimmed with anodized aluminum edging. O.D. dimension of the access is 7" wide x 11" high. <i>Note: DOOR TO SWING INTO THE PATIENT CARE AREA OF THE MODULE.</i> <i>INCLUDE A NON-LOCKING FLUSH PULL RING LATCH.</i>	
TN030000	2	2	OXYGEN OUTLETS, STANDARD Two oxygen outlets to be provided as standard and located in the inhalation panel unless relocated per option TN03RL00. <i>Locate: ON INHALATION AREA WALL DUE TO REDUCED LENGTH INHALATION PANEL.</i>	
TN03A000	0	2	OXYGEN OUTLET: additional Install additional oxygen outlet(s). <i>Locate: (1) ON CURBSIDE WALL ABOVE REAR PORTION OF CRASH BARRIER COMPARTMENT - UPPER RH CORNER.</i> <i>(1) ABOVE HEAD OF COT.</i>	
TN040000	0	1	STANDARD OXYGEN OUTLETS, NCG STYLE (ALSO SUCTION IF APPLICABLE) Oxygen and suction outlets are to be NCG style.	
TN060000	1	1	OXYGEN WRENCH: Install oxygen wrench in oxygen compartment. Mount secure so not left hanging. Mount with length of chain or cable so not removable. <i>Locate: ON RH COMPARTMENT WALL.</i>	



Selected Feature List 11902

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TN100000 1 1 VACUUM OUTLET: inhalation wall

A single vacuum panel shall be installed in the inhalation area. The outlet shall be of the same style as the oxygen system and hooked to the onboard vacuum pump.

Locate: IN CONJUNCTION WITH SUCTION DRAWER LOCATION.

TN11B000 1 1 ASPIRATOR: SSCOR

Install an SSCOR aspirator. The system shall include a #22000 wall mounted regulator, and a #23002 canister holder. Plumb to the pump selected.

Locate: IN CONJUNCTION WITH SUCTION DRAWER LOCATION.

TN12C000 0 1 VACUUM PUMP:

Install a CFP #107CDC20 12vdc electric suction pump.

U LETTERING

UP08D000 0 1 STAR OF LIFE 36: (each)

Install 36" star of life.

Locate: CENTER OF MODULE ROOF.

UP180000 1 1 DOOR REFLECTORS

Install 2" x 12" strips of red Scotchlite at the top of each entry door placed horizontally.

Z04 0 1 Fuel, Tags, Etc.

Allowance for fuel, tags and administrative.

Z05 0 1 NOTES:

Note.

Two (2) copies of the Horton owner's manual shall be furnished with the completed vehicle.



Selected Feature List 11902

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Option	Std	Oty	Header/Description/Data	Proposal Total
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Z12	0	1	Factory Pickup and Transportation Allowance for vehicle pickup, transportation and drivers fee. !!! INCLUDES DEALER PREPARATION AND DELIVERY.	
Z15	0	1	NOTES: Note. The following services and/or items shall be furnished and/or installed by Foster Coach Sales Inc. <ol style="list-style-type: none"> 1) (1) ADDITIONAL REMOTE KEY FOB - TOTAL OF (3). REQUIRES FORD DEALER PROGRAMMING. 2) WEATHER-TECH ALL-WEATHER FLOOR MATS FOR DRIVER AND PASSENGER'S SIDE. ***CUSTOMER NOTE: WEATHER TECH MATS ARE NOT DESIGNED NOR RECOMMENDED FOR USE ON VINYL FLOORS. CHASSIS TO BE ORDERED WITH CARPETED FLOORS. 3) INSTALL A CUSTOMER-SUPPLIED CAD MOUNT CONNECTED TO A FUSED 12-VOLT BATTERY HOT POWER SOURCE - INCLUDES HORTON PRE-WIRE. 4) INSTALL GLOVE BOX STORAGE IN PASSENGER SIDE CAB DOOR ONLY. ***CUSTOMER NOTE: DUE TO SPACE LIMITATIONS, NOT ALL GLOVE BOXES WILL FIT IN THE SPACE AVAILABLE IN THE DOOR PANEL. 5) (3) CORIAN COUNTERTOPS AND MONITOR SPACER. 6) (2) ZICO QR-D-2 CYLINDER BRACKETS INSTALLED BENEATH BENCH - BASE OF BRACKET TO BE TOWARD HEAD OF BENCH. EXACT LOCATION T.B.D. 7) (1) NCE H9000 MONITOR BRACKET FOR ZOLL E-SERIES MONITOR WITH CORIAN SPACER ON REAR PORTION OF INHALATION COUNTERTOP. 8) INSTALL CUSTOMER-SUPPLIED RADIOS AND KNOX SENTRALOCK INTEGRATED INTO MAIN VHF RADIO. RADIO TRANSCEIVERS TO BE LOCATED BEHIND DRIVER'S SEAT. 9) INSTALL CUSTOMER-SUPPLIED CRADLEPOINT DOME ANTENNA AND RELATED EQUIPMENT. BASE UNIT TO BE LOCATED IN ELECTRONICS CABINET. WIRE FOR OPERATION. FURTHER DETAILS MUST BE PROVIDED. 10) (2) FLOWTEC DIAL TYPE FLOWMETERS WITH NCG ADAPTERS. 11) INSTALL MISCELLANEOUS CUSTOMER-SUPPLIED EQUIPMENT NOT ALREADY SPECIFIED: <ul style="list-style-type: none"> - (2) PORTABLE RADIO CHARGERS IN FRONT CONSOLE - BATTERY HOT. - THERMOMETER - MOUNT ONLY, NO POWER. - 5-GAS METER MONITOR/CHARGER - LOCATION/POWER T.B.D. - HANDLIGHT CHARGER/BASE - STREETSIDE REAR. INCLUDES HORTON PRE-WIRE. - (3) SCBA BRACKETS - STREETSIDE REAR. - AX & HALIGAN BAR - STREETSIDE REAR. MOUNTS TO BE SPECIFIED. - THERMAL IMAGING CAMERA MOUNT - LOCATION/POWER T.B.D. - KNOX MABAS 3200 KEY BOX - FRONT CONSOLE. NO POWER REQUIRED. 12) STRIPING AND CHEVRONS PER SPECIFICATION. **DOES NOT INCLUDE LETTERING - LETTERING NOT REQUESTED IN RFP** 13) (2) FACTORY INSPECTION TRIPS FOR (2) DEPARTMENT PERSONNEL. ONE TRIP TO BE UPON COMPLETION. THE OTHER TRIP INTERVAL IS T.B.D. **PRICED SEPARATELY ON QUOTATION DOCUMENT** 14) PROVIDE EQUIPMENT AND COMPLETE INSTALLATION OF A FIRECOM 5200D INTERCOM SYSTEM PER FIRECOM QUOTE. HORTON TO PRE-WIRE HEADSET JACK LOCATIONS. 15) PHOENIX REAR VALVE STEM EXTENSION KIT. 16) DISABLE AUTOMATIC DOOR LOCK FUNCTION. 	

HORTON 2016 SUPER DUTY F-550 4X4 CHASSIS CAB AMBULANCE

BODY CODE: F5H F-550 REGULAR CHASSIS DRW 4X4

PEP: 663A XLT TRIM

ENGINE: 99T 6.7L OHV POWER STROKE DIESEL ENGINE V8
INCLUDES DIESEL EMISSIONS FLUID (DEF)

TRANS: 44W TORQ-SHIFT 6-SPEED SELECTSHIFT AUTOMATIC O/D

WHEEL BASE: 189 INCH (108" cab to axle) TURNING DIAMETER 54.8 ft.

PAINT: PQ RACE RED

TRIM 3S 40/20/40 STEEL CLOTH SPILT BENCH
(CENTER REMOVED FOR HORTON CONSOLE)

TIRES: THB 225/70R_x19.5G BSW (4) TRACTION TIRES REAR (2) A/S TIRES FRONT

GVWR: 18,000

AXLE; X4N 4:10 LIMITED-SLIP AXLE

EQUIPMENT: SOS POST CRASH ALERT SYSTEM (STANDARD)
 REMOTE KEYLESS ENTRY (KEY FOB) (STANDARD)
 PERIMETER ANTI-THEFT ALARM (STANDARD)
 AIR CONDITIONING (STANDARD)
 CRUISE CONTROL & TELESCOPING STEERING COLUM (STANDARD)
 AM/FM STEREO CLOCK CD MP3 PLAYER (STANDARD)
 91M SYNC SYSTEM (STANDARD)
 39S SAT RADIO/6MONTH SUBSCRIPTION (STANDARD)
 POWER LOCKS AND WINDOWS (STANDARD)
 DUAL BEAM JEWEL EFFECT HEADLAMPS (STANDARD)
 ROOF CLEARANCE LIGHTS (STANDARD)
 UNDER HOOD SERVICE LIGHT (STANDARD)
 TOW HOOKS TWO, FRONT (STANDARD)
 AIR BAGS DRIVER AND PASSENGER FRONTAL
 AND SIDE AIR BAG CURTAINS INCLUDES
 PASSENGER SIDE DEACT. SWITCH ON
 REGUALR AND SUPER CABS (STANDARD)
 (4) UPFITTER SWITCHES (STANDARD)
 BRAKES 4 WHEEL ABS (STANDARD)
 40 GALLON FUEL TANK AFT OF AXLE (STANDARD)
 FRONT AND REAR STABILIZER BARS (STANDARD)
 CHROME BUMPER FRONT (STANDARD)
 GRILLE CHROME SURROUND WITH PLATINUM INSERT (STANDARD)
 REAR VIEW MIRROR (STANDARD)
 54K MANUALLY TELESCOPING TRAILER TOW MIRRORS WITH POWER
 HEATED GLASS, HEATED CONVEX SPOTTER MIRROR, INTEGRATED
 CLEARANCE AND TURN SIGNAL LIGHTS
 41H ENGINE BLOCK HEATER
 213 SHIFT ON THE FLY
 433 SLIDING REAR WINDOW
 512 SPARE TIRE AND WHEEL
 61J JACK
 62M ADJUSTABLE GAS AND BRAKE PEDALS
 942 DAYTIME RUNNING LIGHTS
 47L AMBULANCE PREP WITH SPECIAL EMISSIONS
 DUAL ALTERNATORS 357 AMPS TOTAL
 X4N 4.10 LTD-SLIP AXLE
 7,000lb FRONT SPRINGS 13,660lb REAR
 STATIONARY ELEVATED IDLE CONTROL
 INTEGRATED INTO THE ENGINE CONTROL MODULE
 98R MANUAL REGENERATION SWITCH

OTHER WHEEL BASES

F5H 141" REGULAR CAB 60" CAB TO AXLE	42.3 TD	W5H 176" CREW CAB 60" CAB TO AXLE	51.5 TD
F5H 165" REGULAR CAB 84" CAB TO AXLE	48.6 TD	W5H 200" CREW CAB 84" CAB TO AXLE	57.8 TD
F5H 201" REGULAR CAB 120" CAB TO AXLE	57.9 TD		
X5H 162" SUPER CAB 60" CAB TO AXLE	47.8 TD		
X5H 186" SUPER CAB 84" CAB TO AXLE	52.4 TD		
		HORSEPOWER	TORQUE
		300@2800	660@1600

AMBULANCE SPECIFICATIONS

PLEASANT PRAIRIE FIRE & RESCUE DEPARTMENT

8044 88TH AVENUE

PLEASANT PRAIRIE, WI 53158-2015

CHASSIS, 2016 FORD, 4 X 4 189" F-550 CAB/CHASSIS:

The vehicle converter shall supply a 2016 189" wheelbase Ford F-550, 4X4 cab/chassis. The chassis shall include Ford's 47L Ambulance Prep Package.

!!! ORDER CHASSIS WITH CARPETED FLOORING ILOS CARPET DELETE (RUBBER FLOORS) DUE TO CUSTOMER REQUEST FOR WEATHER-TECH FLOOR MATS (DEALER-SUPPLIED).

!!! ORDER CHASSIS IN OEM RACE RED.

!!! ORDER CHASSIS WITH MIRROR OPTION 54K (ILO 54F).

ENGINE AND RELATED EQUIPMENT:

- 6.7 L Power Stroke V-8 Turbo Diesel
- Operator commanded regeneration (OCR)
- Includes diesel exhaust fluid (DEF)
- Engine Block Heater
- 40 gallon fuel tank mounted between the frame rails aft of the rear axle

TRANSMISSION:

- Automatic. TorqShift electronic 6-Speed SelectShift with overdrive
- Transmission Oil Cooler.

REAR AXLE:

- Ratio: 4.10:1 limited slip

OVERALL WEIGHT RATINGS:

- GVW: 18,000 lbs.
- Front Axle/Springs: 7,000 lbs. Combined Rating @ Ground
- Rear Axle: 13,660 lbs.
- Shock Absorbers: 1.38" Front and Rear, Heavy Duty Gas Type
- Stabilizer Bar: Front and rear

TIRES AND WHEELS:

- Quantity of seven (7) tires with spare shipped loose
- Tire size to be 225/70R19.5 Load Range 'G'
- Rear tires: traction, front tires: all season radials
- 19.5" steel wheels
- Tire jack

BRAKES:

- Front and Rear: Four wheel disk with antilock
- Power Assist: Hydro Boost
- Parking Brake: Foot Operated, hand release

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INTERIOR APPOINTMENT STANDARDS:

- HD Ford XR-50 power steering
- Tilt/telescoping steering with factory speed control
- Dual padded sun visors
- Power door locks
- Power windows
- Power/Heated mirrors with clearance lights in lieu of PowerScope mirrors
- (2) 12V power points
- Adjustable gas and brake pedals
- Heat/Air conditioning
- Dual O.E.M. cloth-covered captain's chairs with inside folding arm rests
- Carpeted floor covering in lieu of rubber floor covering
- Second generation driver and passenger air bags
- An AM/FM stereo/CD/MP3 player with two (4) speakers installed in cab
- Factory gauges for oil pressure, fuel capacity, and water temperature.
The converter-added digital display shall provide both ammeter and voltmeter.

ADDITIONAL APPOINTMENT STANDARDS:

- XLT trim package
- Chrome front bumper and grille surround
- Tinted glass
- Sliding rear window
- Rear view mirror
- Dual electric horns
- Interval wipers
- ICC lights
- Halogen jewel effect headlights
- Daytime running lights
- Sound reduction package
- Integrated auxiliary idle control
- Auxiliary heat/AC connections
- Front license plate bracket
- Under hood service light
- Two front tow hooks
- SOS Post Crash Alert System
- Perimeter Anti-theft alarm
- Remote keyless entry
- Shift on the fly
- SYNC system

BATTERIES:

The vehicle shall be equipped with two (2) 750 cca batteries located in the OEM location under the chassis hood. The total cca rating for this vehicle shall be 1,500 cca.

ALTERNATORS:

Dual O.E.M. Motorcraft alternators shall be installed by the chassis manufacturer. These alternators shall be internally regulated and shall provide a total of 357 amps.

WARRANTY:

The chassis manufacturer's standard vehicle warranty policies shall apply.

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CHASSIS INTERIOR COLOR:

The chassis interior shall be O.E.M. gray.

CHASSIS HARDWARE AND ACCESSORIES:

The items to follow represent chassis modifications, hardware, and accessories that are required. Failure to provide these features will be cause for rejection of the bidder's proposal as being non-responsive.

DIESEL EXHAUST FLUID (DEF):

The fill for the DEF tank will be located in the driver's side of the body forward of the rear wheels. A label stating "DIESEL EXHAUST FLUID (DEF)" will be installed next to the fill neck.

STAINLESS STEEL WHEEL COVERS

Stainless steel DOT approved wheel inserts shall be installed on all four outside wheels of the chassis.

MUD FLAPS, REAR:

The vehicle converter shall install individual rubber mud flaps behind each rear wheel. The mud flaps may incorporate the converter's corporate logo provided that the logo is incorporated into the rubber material and not a separate piece.

DOCK BUMPERS:

The rear step end caps shall include two heavy duty rubber dock bumpers installed on the outer face of the diamond plate. These bumpers shall serve to protect the diamond plate from damage due to minor contact. Each bumper is to be approximately 6"W.

REAR STEP/BUMPER REINFORCEMENT:

The standard rear step shall be reinforced with 2" x 2" steel angle for added impact protection.

DIAMOND PLATE RUNNING BOARDS:

Diamond plate running boards shall be installed on each side of the cab at the cab entry points. The running boards shall be .125" thick 3003-H14 alloy polished aluminum diamond tread plate. The design of the running boards shall include multiple perforations for better foot traction and drainage. They shall include a splash shield at the forward end to protect the vehicle from spray and road debris.

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REAR STEP/BUMPER ASSEMBLY:

The rear of the vehicle shall be equipped with a step/bumper assembly to be fabricated from .125" polished aluminum diamond Treadplate. The assembly shall be spaced out from the rear kick plate a minimum of 1.5". The center section of the assembly shall pivot up and over center on two (2) .5" bolts to stay in the 'up' position. This section shall be a minimum of 9.5" deep and shall be constructed with grip-strut on the stepping surface to provide for better footing. The ends of the assembly shall be fixed diamond tread plate. The distance between the top of the step and the ground shall not be less than 16". The fold-up portion of the step shall be firmly held down with two (2) pin and socket holders to prevent rattling while the vehicle is in motion.

LIQUID SPRING REAR SUSPENSION WITH DUMP:

The vehicle shall include a Liquid Spring system installed in the rear suspension. The system shall include a dump feature, wired through the left rear entry door magnetic switch, to lower the cot loading height.

SUSPENSION OVERRIDE SWITCH:

A manual dump override switch shall be installed as noted below. This switch shall override the dump activated by opening of the left rear patient compartment entry door.

Switch Locate: CURBSIDE REAR ENTRY DOOR PANEL ABOVE DOOR LOCK BEZEL.

BATTERIES:

The battery system, as noted in the "Chassis" section of this specification, shall include two OEM batteries to be installed in the manufacturers under hood battery tray locations.

BATTERY HEAT SHIELDS:

Battery heat shields will be provided and installed for all batteries located beneath the hood.

REVERSE ALARM:

An audible alarm shall be installed to activate when the vehicle is placed into reverse gear. There shall be, installed on the front console and wired through the vehicle electrical system, a momentary cutoff switch to disable the alarm. This switch shall automatically reset each time the vehicle is placed into reverse gear.

CAMERA SYSTEM:

A camera system shall be installed as noted below and wired for use with the front electrical system control screen. A separate monitor will not suffice to fulfill this requirement.

Quantity: (1)

Locate: ABOVE REAR DOORS.

Note: CAMERA TO BE A SAFETY VISION #SV-625B (COLOR).

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AM/FM/CD PLAYER:

The OEM AM/FM/CD player shall be installed in the cab and wired to the OEM cab speakers. This unit shall also be capable of being wired to patient area speakers should they be required within this specification.

CONVERSION:

The following section describes the required body design, manufacturing process, and materials. Adherence to this section is of extreme importance to this purchaser due to space requirements and safety concerns. The bidder must meet this section as closely as possible without utilizing experimental or prototype designs in order to be considered for bid award.

MINIMUM BODY DIMENSIONS:

The completed vehicle shall have the following minimum dimensions:

(Exterior)

-Height: 91"
-Width: 96.25"
-Length: 173"

(Interior)

-Height: 74"
-Aisle 48"
-Length: 169"

OVERALL DIMENSIONS (Including Chassis, Module and Step):

-Height: 114"
-Width: 100"
-Length: 307"

!!! VEHICLE TO HAVE A PASS-THROUGH CONFIGURATION WITH SLIDING WINDOW PROVIDED VIA OEM SLIDING WINDOW IN CAB.

!!! CRASH BARRIER CONFIGURATION.

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MODULAR BODY STRUCTURAL DESIGN REQUIREMENTS:

The module body shall be designed and fabricated with the following key elements in mind:

1. The greatest possible load carrying capacity is desired.
2. The safety of all vehicle occupants is of paramount concern.
3. The body design, including construction materials and fabrication techniques shall be proven to be durable.
4. The body shall be easily retrofitted to a new chassis should that need ever arise.

With these concerns in mind the following requirements have been established for the purposes of this specification:

The vehicle converter shall design and construct its own module bodies, and maintain an engineering staff at its manufacturing facility to handle any custom body changes that may be necessitated by this design. It is the intent of this purchaser to receive a finished product of the highest standards of quality available. Vehicle manufacturers who design and build their own bodies and who have the expertise of an engineering staff will possess a greater capacity as far as handling a custom project of this type than manufacturers who purchase their bodies from an outside vendor. Accountability and quality of the design and construction of the body are enhanced when the vehicle converter manufactures the body.

GENERAL BODY DESCRIPTION:

The construction process described within this specification will ensure that the body shall remain structurally intact. However, to achieve this level of quality and durability, the module body, including all doors, must be constructed correctly initially. This specification requires that the module body, including all doors, be built within a tolerance of one five-thousandths of one inch. To achieve this the vehicle manufacturer must use, as standard practice, precision computerized equipment such as found in Strippet machines and microprocessor controlled milling machines and chop saws. Use of precision equipment will ensure that all door openings, door handles and latches, body windows, and warning light assembly installation locations are of the correct size and square to the body. Cutting done by hand, such as with a jigsaw, is not desired unless it involves the chassis, or unless a warning light assembly must be located in such a way that it depends on the layout of the finished vehicle. (E.g. when a light must be centered within a paint stripe since the exact stripe location will not be determined until the module is built and mounted.) In addition, utilization of computerized equipment will simplify the production of replacement body panels in the event of an accident since the computer can duplicate a given part exactly. This includes documentation of all body light locations.

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PAYLOAD REQUIREMENTS:

The vehicle payload shall meet or exceed that called for in the current KKK-A-1822 specification. The vehicle manufacturer shall, upon notice by this purchaser, provide a written statement from an independent engineer that the model being offered has met this set of criteria. Before delivery of the completed unit the manufacturer shall weigh the vehicle. A written statement of those weights shall be affixed to the inside of the street side front #1 compartment door. This purchaser reserves the right to have the finished vehicle weighed independently upon delivery. If it is found that the written statement of weight provided by the manufacturer is inaccurate beyond what may be reasonably explained as a slight difference in the calibration of the scales, then the vehicle will be rejected. It should be noted that this purchaser, while interested in attaining the greatest possible payload, is unwilling to compromise on the structural requirements of a strong, durable, and safe body. All bidders must understand these factors supercede concern over payload, and that the lightest body (greatest payload) will not necessarily be deemed sufficient to meet the stringent quality and safety requirements set forth herein.

MODULE BODY CONSTRUCTION AND WARRANTY:

The module body shall be constructed per the following detailed specifications. Generally speaking the body shall be of all-aluminum construction. Aluminum is shown to reduce weight over several other materials. It also possesses anti-corrosion properties that are essential for a vehicle of this type. The exact aluminum material requirements are explained in further detail below. The choice of materials and the design shall allow the manufacturer to warrant the materials and workmanship of the module body for a period of thirty (30) years as set forth in the warranty section of this specification. The manufacturers structural warranty shall specifically cover:

- The continued and correct alignment of both compartment and access doors.
- Seam or joint separation in door construction.
- Aluminum interior cabinetry.

The warranty shall be fully transferable to a new owner should the vehicle ever be sold. In addition, should the manufacturer bidding this proposal re-chassis the vehicle within the period of the initial structural warranty, then an additional 5 years shall be added to the remaining amount of warranty coverage left at the time of re-chassis. This warranty shall be revalidated in five-year increments each time the body is mounted to a new chassis provided that the warranty has not expired, that this purchaser authorizes any necessary repairs, and provided that the original manufacturer performs the re-chassis.

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CORNER POST SUPPORTS:

The body structure must be able to support the loaded weight of the vehicle in the unlikely event of a rollover. A structure is required that will enhance the safety of both patients and attendants in the event of an accidental collision. The foundation of a solidly built module body is the utilization of strong corner posts in both the sidewalls and the roof. A one-piece 90-degree radius post is required. The posts shall include a full length W shaped extrusion that forms a fully encased web inside the post for strength. This reinforcing member shall angle inward just before it joins the radius to form a small slot where the edges of the aluminum skin will be inserted prior to the final welding. Because the structural integrity of a body is derived from the corner posts, subfloor, and framework, corner posts that are a part of the exterior body skin (e.g. rolled corner posts) will not be considered, nor will corner posts which do not have an integral center reinforcement as part of the extrusion.

CORNER POST STRENGTH:

The corner post extrusions shall possess a minimum ultimate tensile strength of 27,000 psi (6063-T5).

ROOF EXTRUSIONS:

The horizontal roof extrusions shall conform to the same construction description as the vertical wall extrusions. They will, however, include an extruded drip rail as a part of the one-piece posts. Because the drip rail is a part of the post itself there will be no seams between the rail and the body above the rail. In addition there shall be drip rails installed above all body doors that are not full height. These rails shall attach via a durable adhesive.

WALL AND ROOF SKIN SUPPORTS:

The exterior wall and roof skins shall be supported on the inside by 2" square tubing with .125" wall. These structural supports shall be strategically located at the load bearing points of the module body. The roof structural support beams shall be spaced on minimum 12" centers for adequate load support. Wall tubing of .125" thickness or less will not be acceptable.

HORIZONTAL WALL SUPPORT:

In addition to the vertical wall supports there shall be a horizontal beam, located in the beltline area, to provide additional protection in the event of a side body collision.

GUSSET ENHANCEMENT:

Gusset supports, made from 2" square tubing, shall be installed throughout the vehicle for added strength. Each gusset shall be a minimum of 5" long at its longest point. A minimum of twenty-four (24) of these gussets shall be welded into the vehicle support structure. Areas of installation shall include but not be limited to: all door openings, all body corners, and above all wheel wells. Designs that utilize no gussets, or gussets of lesser material size or strength, are not acceptable.

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EXTERIOR BODY PANELS:

The materials selected for the body skin have been chosen because of this vehicles expected heavy-duty cycle and the good wear characteristics that this material has shown in the field. The material shall be a minimum 5052-H34 alloy with an ultimate tensile strength of 38,000 psi. This material has been chosen because it is less prone to fail due to stress than other weaker materials such as 5052-H32 alloy.

EXTERIOR BODY PANELS (PART 2):

The thickness required for exterior body panels is:

-Side, front, and rear walls: .125"

-Ceiling and floor panels: .090"

Note: The roof shall be constructed with a single sheet of 5052-H34 .090" thick aluminum. This one-piece construction is preferred over a multiple piece design. The roof shall incorporate a 3/8" crown designed to allow water to drain.

FLOOR CONSTRUCTION:

Floors that are uneven or are incapable of adequately supporting the load being carried on the vehicle are unacceptable. For that reason thin floor panels and/or a lack of floor supports are not desirable. To prevent buckling, sagging, oil canning or any other structural breakdown of the flooring system a detailed description of the required construction process is provided.

The body subfloor shall be constructed of .090" 5052-H34 aluminum. The floor, from the front to the rear and from curbside to street side shall be supported by a minimum 2" x 3" tubular beams with a .25" wall. The floor just behind the axle shall be supported by a minimum 1.5" x 3" tubular beam with a .25" wall. All beams shall be strategically located at the load bearing points of the floor and welded into place. The interior of this vehicle shall contain no wood or wood products of any kind. The subfloor, above the aluminum sheet shall be specially constructed to provide both acoustic and thermal protection for the patient interior. It shall consist of the .090" aluminum with tubular understructure as noted above. The underside of this area is to be sprayed with a sound reduction coating. In addition, a .125" damping pad, a .125" sound barrier sheet, and a .625" aluminum composite floor panel shall be installed prior to installation of the vinyl floor covering. The purchaser reserves the right to inspect the process proposed by the bidder and to make determinations regarding the acceptability of that process. The resultant subfloor shall have no organic, wood, or wood products and shall be guaranteed against rotting or water absorption for a minimum of fifteen years. It shall not support or attract mold or fungus.

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SKIN TO SUPPORT ATTACHMENT:

All exterior aluminum body panels shall be attached to the underlying structural supports via high performance polyurethane two sided tape. The tape shall have a polyurethane foam core for environmental resistance and an acrylic adhesive for a durable bond. The tape will be used as an insulating agent to hold the panels tightly against the structural supports, thus eliminating vibration and oil-canning. In addition to the tape attachment system, all panels shall be welded to structural members at the perimeters only. Welding in the center of the panels is not desired as the process will cause heat distortion of the body panels and lessen the overall quality of the finished appearance. Use of the tape, as described here, will eliminate heat distortion without damaging the structural integrity of the module body.

SKIN TO SUPPORT ATTACHMENT (PART 2):

Each body panel shall be welded to all horizontal frame members, including the roof extrusions. In addition, the panels shall be welded to the vertical corner posts. In the case of the roof, the perimeter of the one-piece roof sheet shall be stitch welded. This method of attachment shall provide a total welding application to the entire perimeter of the body skin and a taped/insulating application to the interior surfaces of all walls. Methods of panel attachment that utilize rivets will not be acceptable.

STRUCTURAL INTEGRITY VERIFICATION:

Structural integrity, as stated elsewhere in this specification, is of extreme importance to this purchaser. As such, it is required that the manufacturer maintain a program of simulated crash tests. The manufacturers Hygee sled testing program must be current and have been maintained on a continuous basis for a period of time not less than ten years. In addition, the sled testing shall have subjected a body, built to the above-written specifications, to a minimum of 30 G's in both side and frontal impact conditions. Neither photographs of vehicles that have been involved in accidents, nor statements or observations relevant to an accident, be it from a customer or a manufacturers representative, shall suffice as a substitute for this requirement. The sled testing must take place in a controlled environment whereupon meaningful engineering data can be gathered and applied to the structural design of the module body. Accidents that take place outside of this controlled environment do not yield any meaningful data. Therefore, real world accidents are considered anecdotal and cannot realistically be used by the manufacturer to judge the safety of a design.

MODULAR DOOR DESIGN:

Door panel separation, dirt accumulation at seams, paint imperfections, misalignment, and even malfunctions whereupon the door cannot be operated have been observed in many styles of door construction. These problems, along with the expected rugged use of the vehicle doors, shall be eliminated with a good overall design and construction process. With these thoughts in mind the modular doors shall be constructed as follows:

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OUTER DOOR SKIN:

The door facing and edges shall be formed from a single sheet of aluminum. The aluminum used for the doors shall not be less than 5052-H34 alloy with an ultimate tensile strength of 38,000 psi. The material shall be .125" thick. All module doors shall be flush fit to the body side. The door panels must be welded at the corners.

INNER DOOR REINFORCEMENT:

Each door shall include an internal extrusion for added reinforcement. The extrusions shall extend around the entire perimeter of the door. Additional reinforcement shall be installed through the center of the door and around each window where applicable. In addition to the extrusions reinforcing each outer door pan, the extrusions themselves shall be reinforced through a dual joining method. First, each mitered corner, where the frame corners join, shall be fitted with a one-way solid aluminum insertable key. This key shall prevent the corner from pulling apart, and shall act as a solid aluminum internal gusset. Secondly, each corner where the frame joins shall be welded to further prevent any separation. The end result will be a rigid door that will not bend or flex and that will eliminate the other commonly seen structural defects described above.

INNER DOOR PAN:

An inner door pan shall fit flush with the inner edges of the door. Inner door pans that do not fit flush will have sharp or ragged edges exposed and will not be acceptable. The panels must be attached to the door structure with machine screws and "T" style Nutserts to prevent spinning stripping. Sheet metal screws or rivets will not be accepted. Lastly, a closed cell cross-linked polyolefin foam tape shall be used beneath the inner door panels to isolate the panels from the door frames. This process will prevent door rattling.

DOOR SEAL:

All module doors shall incorporate an extruded rubber seal located around the perimeter of the door. The seal shall insert into a groove in the inner door extrusion. Seals that are installed around compartment openings will be easily torn by the movement of equipment across them. In addition, glue will not be permitted except in the case of a double door compartment. The requested design does not include a groove on the underlying door edge of a double door compartment. That edge alone will require an adhesive. Glue for all seals is not desirable because of increased replacement time and insufficient durability.

DOOR JAMB:

All doorjamb must be separate from the body skin and must be welded to the 2" x 2" tubular body frame members so as to ensure continued door alignment and proper latching. The compartment frame shall be designed in such a manner as to provide extra protection around the compartment openings. The reinforcement tube shall be at least 1" wide. For added strength, the frame shall be at least .188" thick where screws are attached.

Prior to door installation the doors shall be true fit to the doorjamb. The fitting, prior to installation, shall provide added assurance that the door aligns properly with the doorjamb.

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HINGING:

All doors shall have full-length stainless steel hinges. The hinges shall be .070" thick and shall incorporate a .25" diameter pin.

All hinges shall have un-slotted mounting holes for an exact and permanent installation. Hinges that utilize slotted mounting holes are unacceptable because of the continued adjustments that they require.

There shall be an anti corrosive material installed along the length of the hinge where the hinge meets the door frame to separate the stainless hinge from the aluminum body. This material shall be transparent so as not to be visible at any point while the door is being used.

HOLD-OPEN DEVICES:

The following door hold-open devices shall be installed:

- Compartment doors: Gas filled, 100-degree extension actuator
- Side access door: Gas filled, 110-degree extension actuator
- Rear doors: Cast Products grabber style devices

Spring-loaded devices are not desired because of their weaker holding capabilities and a lack of smooth door operation.

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DOOR HANDLES AND LATCHING SYSTEM:

A door latching system is required that provides safety to all on-board personnel and security to all stored equipment. The patient area must be capable of being quickly secured. The following minimum features are to be designed into the module door latching system:

- All door handles shall be rugged automotive style handles that are near flush with the outer door panel.
Each handle shall actuate a Nader rotary safety latch.
- The handle and latching system shall be designed by their manufacturer to accommodate electromagnetic activation. Paddle style or D ring style handles that must be retrofitted for this application are unacceptable.
- The entire exterior handle assembly shall be Tri/Mark Series 2200 cast metal that is chrome plated and buffed to a high luster.
- All doors shall have an exterior key lock.
- All three patient area access doors shall include both interior and exterior latch activators. The rear doors shall have an activator installed on the outside of each door. The interior activators shall be located in a recessed pan on the door. A manual lock/unlock device shall be located within the pan. This pan shall be powder coated cast aluminum for extra durability and for ease of decontamination. No plastic products shall be used for this application.
- Exterior double door compartments shall include two exterior latching devices, one on each door.
- The rear entry doors shall incorporate an emergency release lever located at each rotary latch. The emergency release handle shall allow emergency exit if a latch failure would occur.

The latching system shall be a proven system that has been subjected to the simulated sled tests as described elsewhere in this specification. Latching systems that have not been subjected to these tests will not have reliable data available as to installation and retention characteristics. Again, only controlled testing fulfills this requirement. Neither pictures of accidents, nor common observations gathered from damage surveys will suffice.

PATIENT AREA DOOR OPENINGS:

REAR DOORS:

Two (2) doors shall be provided at the rear of the module body. The overall opening of the access to be a minimum of 54.3" in height x 46.75" in width. Both inside and outside door handles shall be installed on each rear door. Left rear doors that can only be activated from the inside are not acceptable. These doors are to incorporate emergency release levers as described above.

SIDE DOOR:

One (1) side door shall be provided on the curb side of the module body. The opening shall have minimum overall dimensions of 71.1" in height x 30" in width.

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INSULATION:

The patient area, including the doors, shall be insulated with 2" Technicon polyfiber for both thermal and acoustic insulation. The headliner area of the vehicle shall also include a barrier insulation of Reflectix material for increased protection.

STREETSIDE FRONT COMPARTMENT (#1):

The compartment described above shall feature the following minimum dimensions:

Clear Door Opening:	18.7" wide x 59.1" high
Actual Compartment	24.2" wide x 62.1" high x 20.36"

This compartment shall be accessed through a single hinged door meeting the standards for door construction, hinging, and latching outlined within this specification. The compartment shall house the vehicle's primary O2 cylinder and shall be vented to the outside in such a way as to prevent moisture from entering the compartment. Under no circumstances shall vents be installed within the compartment door. The compartment itself shall be constructed as an individual box and welded into the body structural framing. The material used shall be .125" polished aluminum diamond plate that is continuously welded at all seams. The compartment shall include two strips of LED lights, one to either side of the compartment door, to provide lighting inside the compartment.

STREETSIDE INTERMEDIATE COMPARTMENT (#2):

The compartment described above shall feature the following minimum dimensions:

Clear Door Opening:	51.8" wide x 26.3" high
Actual Dimension:	55.5" wide x 29.5" high x 20.36" deep

This compartment shall be accessed through double hinged doors meeting the standards for door construction, hinging, and latching outlined within this specification. The compartment shall be utilized for storage of miscellaneous items as required by this purchaser and shall also house the manufacturer's power distribution panel on the LH wall. This compartment shall be vented to the outside in such a way as to prevent moisture from entering the compartment. Under no circumstances shall vents be installed within the compartment door unless they are required for airflow to equipment installed within this compartment. The compartment itself shall be constructed as an individual box and welded into the body structural framing. The material used shall be .125" polished aluminum diamond plate that is continuously welded at all seams. The compartment shall include two strips of LED lights, one to either side of the compartment door, to provide lighting inside the compartment.

TOOL MOUNTING PLATE:

Install an aluminum plate as noted below. The plate shall be .250" thickness and is to have a D/A finish. The plate is to be fastened to the wall of the specified area using aluminum 'C' channels that are welded into place.

Size:	24"W X 24"H.
Location:	CENTERED ON BACK WALL.

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ADDITIONAL ALUMINUM "C" CHANNEL:

Install aluminum 'C' channel as noted below. The channel shall be welded into place and is in addition to channel already present in the specified area for shelving and/or any other required equipment. Priced per c-channel. Standard tool boards require two c-channels.

Location: IN CONJUNCTION WITH TOOL PLATE.

STREETSIDE REAR COMPARTMENT (#4):

The compartment described above shall feature the following minimum dimensions:

Clear Door Opening:	32.0" wide x 58.3" high
Actual Dimension:	36.6" wide x 61.5" high x 20.36" deep

This compartment shall be accessed through double hinged doors meeting the standards for door construction, hinging, and latching outlined within this specification. This compartment shall be utilized for storage of miscellaneous items as required by this purchaser. This compartment shall be vented to the outside in such a way as to prevent moisture from entering the compartment. Under no circumstances shall vents be installed within the compartment door. The compartment itself shall be constructed as an individual box and welded into the body structural framing. The material used shall be .125" polished aluminum diamond plate that is continuously welded at all seams. The compartment shall include two strips of LED lights, one to either side of the compartment door, to provide lighting inside the compartment.

TOOL MOUNTING PLATE:

Install an aluminum plate as noted below. The plate shall be .250" thickness and is to have a D/A finish. The plate is to be fastened to the wall of the specified area using aluminum 'C' channels that are welded into place.

Size: 35"W X 24"H.

Location: ON BACK WALL IN UPPER PORTION OF COMPARTMENT.

ADDITIONAL ALUMINUM "C" CHANNEL:

Install aluminum 'C' channel as noted below. The channel shall be welded into place and is in addition to channel already present in the specified area for shelving and/or any other required equipment. Priced per c-channel. Standard tool boards require two c-channels.

Location: IN CONJUNCTION WITH TOOL PLATE.

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CURBSIDE REAR COMPARTMENT (#5):

The compartment described above shall feature the following minimum dimensions:

Clear Door Opening: 19.0" wide x 81.1" high
Actual Dimension: 23.7" wide x 82.1" high x 20.3" deep

This compartment shall be accessed through a single hinged door meeting the standards for door construction, hinging, and latching outlined within this specification. This compartment shall be utilized for storage of miscellaneous items as required by this purchaser. This compartment shall be vented to the outside in such a way as to prevent moisture from entering the compartment. Under no circumstances shall vents be installed within the compartment door. The compartment itself shall be constructed as an individual box and welded into the body structural framing. The material used shall be .125" aluminum that is continuously welded at all seams. The compartment shall include two strips of LED lights, one to either side of the compartment door, to provide lighting inside the compartment.

SWEEP OUT COMPARTMENT FLOOR:

The floor of the compartment(s), as noted below, shall be flush with the door frame so as to provide a sweep-out style compartment bottom. There shall be no lip at the forward edge of the compartment bottom, or any other obstruction, that may hinder the purchaser's ability to sweep the compartment free of dirt and/or debris.

Locate: CURBSIDE REAR COMPARTMENT.

Note: FLOOR TO BE FLAT ALUMINUM WITH A D/A FINISH.

DIVIDER FIXED, VERTICAL COMPARTMENT;

A 16" vertical divider shall be installed as noted below. The divider shall be non-adjustable and shall be fabricated from the same material used in the construction of the compartment in which it is to be installed.

Quantity: (1)

Locate: 11" FROM RH WALL.

FIXED SHELVING FOR VERTICAL EXTERIOR COMPARTMENT:

Install a fixed shelf in the location(s) noted below. Fabricate all shelves using 0.125" thick 3003-H14 aluminum diamond plate. Shelves will be mounted directly to the compartment side walls with (4) cast shelf brackets; (2) for right wall, (2) for left wall. Shelves to include a 2" integral lip to retain equipment on the shelf. Compartment lighting to meet the lighting criteria as described elsewhere within this specification.

Quantity: (1)

Locate: 40" ABOVE COMPARTMENT FLOOR ON RH SIDE OF DIVIDER.

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ADJUSTABLE SHELVING FOR VERTICAL COMPARTMENT:

A shelf shall be installed in the location(s) noted below. All shelving is to be fabricated from 3003-H14 aluminum diamond plate. This material is to be .125" thick. All shelving is to include a 2" integral lip to prevent equipment from sliding off of the shelf. The compartment light shall meet the lighting criteria as described elsewhere within this specification.

Quantity: (1)

Locate: ABOVE FIXED SHELF FOR I/O ACCESS USE.

STAIR CHAIR POCKET:

A pocket that is 40" high x 10" wide x 1.2" deep shall be recessed into the inner compartment door panel. The pocket shall be installed as close to the bottom and hinged side of door as possible unless otherwise specified.

Locate: HINGE SIDE OF CURBSIDE REAR COMPARTMENT DOOR.

CRASH BARRIER SAFETY CONFIGURATION:

In order to provide a fully welded stable crash barrier at the head of the squad bench, the vehicle shall be built with the side access door located at the forward most part of the curbside of the vehicle. The cabinet normally located in that position shall be relocated to the head of the squad bench.

Size: ACTUAL DIMENSIONS: 31.5"W X 60"H.

CLEAR OPENING: 28.8"W X 46.44"H X 20.3"D.

Note: MINIMUM 72" SQUAD BENCH LENGTH REQUIRED.

- !!! INSTALL A CUSTOM DIAMOND PLATE CRASH BARRIER COMPARTMENT BETWEEN THE SIDE ENTRY DOOR AND WHEEL HOUSE.
- !!! INTERIOR ACCESS TO EXTEND AS CLOSE TO FLOOR AS POSSIBLE.
- !!! INSTALL A SINGLE-HINGED DOOR WITH DUAL NADER PINS ON EXTERIOR.
- !!! INSTALL (3) ADJUSTABLE DIAMOND PLATE SHELVES IN COMPARTMENT WITH 1" LIPS. LOCATE (1) SHELF AT INTERIOR FLOOR LEVEL AND OTHER (2) SHELVES SPACED EVENLY ABOVE IN THE INSIDE/OUTSIDE ACCESS AREA.
- !!! INSTALL AN AISLE-FACING CARGO NET WITH ANKRA FASTENERS ON THE INTERIOR ACCESS.
- !!! INSTALL (1) AISLE-FACING DRAWER (IK040000) WITH BOTTOM MOUNTED SLIDES IN THE UPPER PORTION OF THE CRASH BARRIER PER DRAWING. SIZE: APPROXIMATELY 25.5"W X 5.5"H X 16"D (ALL I.D.)
- !!! TOP OF COMPARTMENT TO HAVE FLAT STAINLESS STEEL (NO LIPS) TO ALLOW FOR DEALER INSTALLATION OF A CORIAN COUNTERTOP.

KKK-A-1822 CERTIFICATION LABEL:

The vehicle shall have weight/payload, electrical load, and the current KKK-A-1822 certification stickers installed in the O2 compartment. Failure to provide these certification labels will be cause for rejection of the completed vehicle. Labels that are found to be falsified will also be cause for rejection of the completed vehicle. The purchaser reserves the right to request documentation showing that all required testing has been completed at the time of the bid opening. Failure to provide this documentation, if requested, will result in the bid being rejected without further consideration.

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SPECIAL BODY REQUIREMENTS:

The requirements set forth in the following section of this specification represent items and features that may not be offered as standard by the bidder. If the bidder is unable to furnish any items listed in this section, then that inability must be listed and explained in the bidder's list of exceptions. Failure to do so will result in rejection of the bidder's proposal as being non-responsive.

INTERIOR HEADROOM:

The interior headroom of the finished vehicle shall be: 74".

Note: REAR DOORS TO REMAIN AT STANDARD 72" HEADROOM HEIGHT TO ALLOW FOR BACK-UP CAMERA INSTALLATION BELOW M9 AMBER LIGHT.

6" DROP SKIRT DESIGN:

The curbside skirt, forward of the rear wheel well shall be dropped six (6) inches. Two integral aluminum diamond plate steps shall be installed within the side access door step well for improved accessibility to the patient compartment. Under no circumstances shall this be accomplished by bolting an additional step to the step well. The design must be such that all steps are integral. The use of bolts, rivets, or any other type of fastener is prohibited.

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SOUNDPROOFING:

To insure good working conditions and to create a stable patient environment, the vehicle shall be manufactured with particular attention paid to sound control. The following process must be performed throughout the manufacturing cycle of the vehicle:

1. Underbody shall be completely sprayed with sprayable, non-flammable latex sound control coating
2. Body Interior walls, roofs and interior compartment walls shall be sprayed with sprayable non-flammable latex sound control coating
3. The interiors of all access doors shall be sprayed with sprayable non-flammable latex coating
4. The backs of all interior cabinets shall be wrapped in antiphon damping material
5. Door interiors are to be lined with polydamp intefoam extensional damping pad
6. The body structural tubes shall filled with non-resonating dampening material
7. Side stepwell areas are to be backed with PT Damping Pad
8. All walls shall be insulated with 2" Technicon polyfiber acoustic insulation. Headliners shall be double insulated with 2" Technicon Polyfiber and a Reflectix barrier.
9. A .125" damping pad, a .125" sound barrier sheet, and a .625" composite floor panel sandwiched between aluminum sheets shall be installed prior to installation of the vinyl floor covering.
10. A chassis tuned VI-Tech mounting system shall be used to provide vibration reduction and structure borne noise attenuation.

ELECTRICAL COMPARTMENT:

A single door compartment, meeting all criteria for exterior compartments as described elsewhere within this specification, shall be installed above the streetside forward compartment. This compartment shall include a Cast Products vent in the door. This compartment is to house all converter-added electrical equipment such as vacuum pump, battery charger/inverter, etc.

MODULE BODY HARDWARE:

The following section lists hardware items that are to be installed on the vehicle body.

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WINDOWS, MODULE BODY ENTRY DOORS:

The rear module body access doors shall include windows. The two rear doors shall feature fixed windows. Each of these windows shall measure 16.5"h x 17"w and shall be glazed and tinted in accordance with FMVSS. The windows shall have dark privacy glass. The windows shall meet FMVSS glazing standards. No films are to be used. The windows shall be encased in extruded aluminum frames. Under no circumstances will RV style windows, windows that rely on rubber gaskets, windows that do not feature extruded aluminum frames, or windows that do not meet the above stated minimum dimensions be acceptable.

WINDOW, SLIDING FOR SIDE ENTRY DOOR

The side entry door shall have a sliding privacy window. The window shall measure 16.5"h x 17"w and shall be glazed and tinted in accordance with FMVSS. The window shall have dark privacy glass. The window shall meet FMVSS glazing standards. No films are to be used. The windows shall be encased in extruded aluminum frames. Under no circumstances will RV style windows, windows that rely on rubber gaskets, windows that do not feature extruded aluminum frames, or windows that do not meet the above stated minimum dimensions be acceptable.

EMERGENCY RELEASE PROVISION:

The manufacturer shall install emergency release latches at the top and bottom of the interior of the rear entry doors. These will allow egress in the event of a door latch failure. The release knobs are to activate the rotary at the top and bottom of each door. The knobs are to include black round ball-type surfaces for ease of grip and non-intrusive appearance.

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BODY MOUNTS:

This purchaser requires a mounting system that provides a stable and durable attachment of the module body to the chassis frame. To accomplish this requirement the following body attachment method shall be used:

A minimum of (5) five mounting platforms shall be attached along the outside of each chassis frame rail for a total of (8) eight. Each platform shall consist of (1) top plate of .375" thick steel and (2) side reinforcement plates made of .25" steel. There shall be a .375" full backing plate where the mount attaches to the frame rail. The plates shall be welded along all seams with a heavy continuous weld. The body substructure shall include a 1" by 3" solid aluminum tie down bar welded to each sub structure cross member. To complete the body to chassis attachment, a tuned mounting system shall be used. The elastomer mount shall be custom-tuned to the specific chassis type for vibration reduction, structure borne noise attenuation and to provide low profile, low frequency isolation necessary for ideal patient compartment conditions. Standard chassis furnished mounting donuts will not meet the requirements of this specification. The mount shall be attached to each platform by (2) .625" Grade 8 bolts with washers and locking nuts. The platform shall be attached to the chassis frame rail with a minimum of (3) .625" diameter Grade 8 bolts with washers and locking nuts. The fail safe elastomer isolation mount shall then attach to the aluminum body tie down bar with a .75" diameter Grade 8 bolt, a washer, and a locking nut.

The mounting system must have been subjected to a documented Hygee dynamic frontal impact test of at least 30 G's to verify the integrity of the mounting system in the event of a serious accident. No exceptions to this requirement are permissible.

SPLASH SHIELDS:

Stainless steel splash shields are to be installed on the lower front face of the module body just aft of the cab access doors. These shields are to have a brushed finish and shall match the height of the diamond plate corners guards that are to wrap around the lower corner posts on the side of the body.

RUBBER FENDERS:

Extruded rubber fender flares shall be installed above each wheel well opening. The mounting of these flares shall provide for no contact between the fender fasteners and the aluminum body skin. This is done to eliminate any contact between dissimilar metals and the electrolysis that may result.

RUB RAILS:

Rubber lower body rub rails shall be installed on each side of the module body. Each rail shall be securely installed yet simple to remove and replace in the event of damage.

REAR ACCESS DOOR HOLD-OPEN DEVICES:

Cast Products "Grabber" style rear door hold-open devices shall be installed to maintain the rear access doors in the 'open' position. One loop shall be installed on each door, and the appropriate socket shall be installed on the body. These devices are to be chrome finish in lieu of Cast Products' standard finish.

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ELECTRIC LOCKS, ACCESS DOORS:

Power activated door locks shall be installed on patient area access doors. Locks shall be activated by switches located at each patient area access door. Locks may be overridden by a manual slide lever or by the door key. A separate lock switch will be located in the front radio console if option DL48D000 (wired to OEM locks) is not selected.

CONCEALED DOOR LOCK SWITCH:

A concealed weatherproof switch shall be installed as indicated below to operate the power door locks specified above. The switch shall be wired to unlock only.

Locate: REAR LICENSE PLATE BRACKET.

DOOR LOCKS WIRED THROUGH OEM SWITCHES:

The power door locks specified above are to be wired to the O.E.M. chassis door lock switches.

RECESSED LICENSE PLATE BRACKET:

A Cast Products recessed license plate bracket shall be installed as noted below. The bracket will include lighting in the top to illuminate the license plate.

Locate: CENTER OF REAR RISER.

REFLECTORS ON ENTRY DOORS:

Red reflectors shall be installed on the inside on the patient area doors.

DOOR REFLECTION:

Red Scotchlite strips, 2" x 12", shall be installed horizontally across the top of each entry door. This material is in addition to the reflectors listed above.

RUBBER MATTING IN EXTERIOR COMPARTMENTS:

Black rubber matting material shall be cut to size and installed on the bottoms of all exterior compartments and shelves. The material shall feature integral ridges to help equipment to stay in place.

RUBBER-COVERED WALLS IN BACKBOARD COMPARTMENT:

The walls of the backboard compartment shall be covered with self-adhesive textured rubber matting to protect the walls and the equipment stored in this area from any damage.

Color: BLACK.

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PAINT AND STRIPING:

A paint process is required that provides the highest possible gloss as well as superior color and luster retention characteristics. In addition, the paint process must provide a high resistance to chemical sprays, salt sprays, humidity, and temperature changes. Lastly, this process, given the expected life of the vehicle and its heavy-duty cycle, must resist chipping. The final paint application shall be free of material application imperfections such as orange peel, streaking, or a dull finish. Once painted, the vehicle shall be inspected under a black light to bring any small imperfections, not seen with the naked eye, to attention. Any such imperfections shall be repaired prior to the conclusion of the paint inspection process. The final application shall provide a high gloss finish.

PREPARATION:

To produce an acceptable paint finish, the following paint process must be used:

All body doors and hardware must be removed prior to any wash, prime, or final paint application. All material impurities and oils must be removed from the bare aluminum body. The entire module body, excluding the underside, will have all visible welds ground down and all material imperfections filled. The entire body, including the compartment doors, must be finished with a DA. It is also required that all door jamb areas be sanded to insure that no areas are missed with the DA process mentioned above. All holes (e.g. for hinge mounting, etc.) shall be plugged at this stage to prevent any cleaning agents from entering the module body framework. The body shall be prepared for paint by spraying with a high strength detergent followed by a water rinse. Next the body and compartment doors are to be sprayed with a phosphoric acid-based cleaner to remove dirt and oil and to etch the body for superior paint adhesion. The application of the acid-based cleaner shall be followed with a water rinse. Next, a conversion coating shall be applied to the body to enhance paint adhesion and to prevent corrosion. The body shall be rinsed with de-ionized water to prevent salts from accumulating on the surface. The body will, then, be baked dry prior to the application of Sikkens sealer/primer. Again, all module doors, though handled separately from the body, shall undergo the same process as described above.

PAINT:

Immediately after application of the sealer/primer the manufacturer shall apply a finish color coat of Sikkens BTLV 650 paint. The color coat is to be immediately followed by a clear coat. Once all coats are applied the unit is to be baked dry. This base coat/clear coat 'wet on wet' process is required over any other proposed process. Processes not meeting these specific requirements must be explained thoroughly within the bid.

POST-PAINT PROCESS:

Following the drying of all paint coats the unit is to be thoroughly inspected as noted above. The unit is to be polished and any blemishes repaired. All paint lines must be sanded and cleaned.

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PAINT MANUFACTURER'S INSPECTIONS:

The manufacturer shall maintain an outside paint audit system. As part of that audit the paint manufacturer shall regularly receive and test sample paint panels that are painted along with module bodies. The paint manufacturer shall also provide regular onsite inspections of the vehicle manufacturers paint process to assure a consistent level of quality. Audit reports from these inspections shall be provided to management.

ADDITIONAL CORROSION PREVENTION MEASURES:

All locations where fasteners penetrate the outer skin of the module body shall be coated with ECK anti-corrosion agent. In addition, all fasteners that penetrate the outer skin of the module body shall be treated with an anti-corrosion agent to assure the maximum protection against vehicle corrosion and electrolysis.

NON-METALLIC HOLE INSERTS:

All locations where light heads and fenders attach to the aluminum body shall utilize threaded Nylon inserts to isolate the fasteners from the aluminum module body skin and structure. This practice, along with the other measures described above, shall act to minimize the threat of electrolysis.

PAINT WARRANTY:

The paint warranty provided by the converter must meet all minimum warranty standards as described below. Bidder must submit a manufacturer's paint warranty certificate with the bid. Failure to do so will result in automatic rejection of the bidder's proposal. The paint finish is warranted for 7-years or 84,000 miles, prorated as follows:

0-4 years = 100%
5 years = 50%
6 years = 35%
7 years = 20%

BODY PAINT COLOR:

The final paint application to the vehicle body shall be made with Sikkens Autocryl acrylic urethane paint.

Paint Color: RED.

Paint Number: CROSSMATCH OEM RACE RED TO SIKKENS EQUIVALENT.

Paint Color: WHITE.

Paint Number: FLNA4002.

!!! CHASSIS TO BE RECEIVED IN OEM RACE RED. DO NOT REPAINT CHASSIS.
!!! DO NOT PAINT MODULE UNTIL CHASSIS ARRIVES. PAINT BODY TO MATCH
OEM CHASSIS COLOR.

!!! ALL FLAT PORTIONS OF MODULE ROOF TO BE PAINTED WHITE. NO WHITE PAINT
TO BE VISIBLE FROM THE GROUND.

***CUSTOMER NOTE: IF A SPECIFIC COLOR OF RED IS REQUIRED, REQUIRING THE
CHASSIS TO BE REPAINTED, THEN ADD \$1343.00.

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SAMPLE PAINT PANEL:

The successful bidder shall provide a painted test panel, prepared with the specified paint color and number. This process shall confirm correct paint selection, and acceptable color match with the purchaser's current vehicles.

CABINET DOORS, PLEXIGLAS, HANDLES AND HARDWARE:

Information relative to interior door materials, handles, and hardware is provided below:

HANDLES FOR PLEXIGLAS DOORS:

All interior sliding Plexiglas doors are to include extruded pull handles.

LATCHES FOR HINGED DOORS:

The hinged doors within the patient compartment are to utilize Southco Stainless Steel flush-style latches unless otherwise noted. These latches shall feature recessed pull ring style handles. The latches shall be both positive (mechanical latching) and passive (latches automatically).

LATCHES FOR HINGED DOORS:

Install spring loaded lever style latches on the interior cabinets as described below.

Quantity: (4)

Locate: (1) ON EACH DOOR OF LINEN CLOSET, INCLUDING ELECTRONICS
CABINET. TOTAL OF (3).

(1) ON DRAWER ABOVE CRASH BARRIER COMPARTMENT.

PLEXIGLAS COLOR:

The Plexiglas interior cabinet doors shall be a light gray tint.

STAINLESS STEEL COUNTERTOP:

The patient area countertop(s) shall be constructed of stainless steel. The countertops shall be pan-formed and shall incorporate a 1" retention lip around the perimeter of the material. Areas where the stainless steel material meets the vehicle walls shall be sealed to eliminate seams. The front lip on the countertop shall be protected with grip-lock material.

!!! INSTALL FLAT STAINLESS STEEL COUNTERTOPS (NO LIPS) TO ALLOW FOR DEALER INSTALLATION OF CORIAN COUNTERTOPS.

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INHALATION PANEL:

To prevent contamination of the inhalation panel the panel shall be fabricated using no wood or wood based products. The material used shall be an aluminum composite material. This material shall not absorb liquids and shall not attract bacteria, molds or fungi. The material is to be covered with CG-Tech material in a color matching that required within this document.

!!! PANEL LENGTH TO BE REDUCED BY 16" TO ALLOW FOR ZOLL MONITOR STORAGE ON REAR PORTION OF COUNTERTOP.

!!! INSTALL A RECESSED POCKET FOR SMART PHONE STORAGE. LOCATE IN THE INHALATION PANEL BETWEEN THE CONTROL PANEL AND THE RADIO HEAD.

SIZE: 7"W x 4.5"H x 8"D. (ALL I.D.)

!!! INCLUDE A 1" RETAINING LIP ON THE OUTBOARD EDGE.

!!! INCLUDE (2) 12V OUTLETS IN THE BACK WALL FOR CHARGING JACKS PER PAL31C00.

INTERIOR COLORS, UPHOLSTERY AND SEATING:

The patient area interior design is specified below:

INTERIOR WALLS/DOOR PANELS:

The interior wall and door panels in the patient area are to be finished with a non-laminate material possessing the following properties:

- Finger print resistant
- Resistant to yellowing
- Temperature resistant between -40 degrees and 212 degrees F.
- Resistant to oils, greases, weak acids and salts
- Verified tensile strength of 34mpa

No wood or wood-based products are to be used within the specified material.

In addition, the risers and the lower door panels are to be covered with brushed stainless steel.

Color: POLAR WHITE.

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INTERIOR COLOR SCHEME:

The patient area interior shall feature the materials and colors listed below.

FLOOR: TO BE DETERMINED.
RISERS: STAINLESS STEEL.
WALLS: CG-TECH POLAR WHITE.
CABINET: WHITE PAINT - SMOOTH (NON-TEXTURED).
WHITE ALUMINUM COMPOSITE - MAIN WALL AND SQUAD BENCH CEILING CABINET.
UPHOLSTERY: EVS VACUUM FORMED - COLOR TO BE DETERMINED.

- !!! ALL CUSHIONS, INCLUDING THE ATTENDANT SEAT, SHALL BE EVS VACUUM FORMED MATERIAL.
- !!! INSTALL ALL SEAT CUSHIONS WITH FULL PERIMETER VELCRO - DO NOT USE PINS OR CLIPS.
- !!! ALL BACKREST CUSHIONS AND CLOSEOUT PANELS SHALL BE COVERED WITH A MATCHING EVS MATERIAL.
- !!! INSTALL BACKREST CUSHIONS AND CLOSEOUT PANELS USING STANDARD FASTENERS - DO NOT USE VELCRO.
- !!! PROVIDE A .500" OVERHANG ON THE SQUAD BENCH AND CPR SEAT CUSHIONS (IF EQUIPPED).

INTERIOR STORAGE AREAS:

All interior storage cabinets, including the interior of the squad bench, shall be painted for ease of cleaning. Under no circumstances shall carpet be used within these storage cabinets as it is impossible to decontaminate. The paint color is listed within the "Interior Color" section of this specification. The paint shall be treated with an antimicrobial agent.

RISERS:

The interior of this vehicle shall be constructed without the use of wood or wood-based products. The risers shall be constructed of a reinforced structural composite consisting of a high density polypropylene core laminated between two layers of .024" aluminum skin. The composite shall then be covered by a Formica laminate to match the interior of the vehicle. The finished riser panels shall be impervious to water or other forms of moisture and must be guaranteed against rotting or decomposition. Formica laminate will not be installed on the risers if stainless steel riser options are selected.

STAINLESS STEEL STREETSIDE RISER:

The street side riser beneath the main cabinet wall shall be covered with a single sheet of stainless steel. If the "Interior Color" section of this specification calls for an accent stripe, then the stainless steel shall stop at the bottom of this stripe. The stainless material shall be installed flush with the riser and trimmed at the top and both sides so as to cover the edges. The bottom of the material shall be formed at a 90 degree angle so that, upon installation, the 4" rolled floor will seal against the stainless steel. Designs that do not include this stainless steel riser will not be considered as they will not be capable of protecting the riser from damage due to cot movement, Etc. In addition, stainless steel risers that have exposed edges, or that are more than one piece, will not be acceptable. Drawer and tip-out faces will be constructed with brushed aluminum laminate. Actual stainless steel faces must be specified as an additional requirement and at additional cost.

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STAINLESS STEEL CURBSIDE RISER:

The curbside riser beneath the squad bench shall be covered with a single sheet of stainless steel. The stainless material shall be installed flush with the riser and trimmed at the top and both sides so as to cover the edges. The bottom of the material shall be formed at a 90 degree angle so that, upon installation, the 4" rolled floor will seal against the stainless steel. Designs that do not include this stainless steel riser will not be considered as they will not be capable of protecting the riser from damage due to cot movement, Etc. In addition, stainless steel risers that have exposed edges, or that are more than one piece, will not be acceptable. Drawer and tip-out faces will be constructed with brushed aluminum laminate. Actual stainless steel faces must be specified as an additional requirement and at additional cost.

STAINLESS STEEL LOWER DOOR PANELS:

The lower portion of the patient area entry doors shall be covered with single sheets of stainless steel. If the "Interior Color" section of this specification calls for an accent stripe, then the stainless steel shall stop at the bottom of this stripe. Designs that do not incorporate this feature will not be considered as they will not be capable of protecting the door panels from damage due to cot movement, foot traffic, Etc.

!!! INSTALL 2-PIECE INTERIOR DOOR PANELS FOR ALL THREE ENTRY DOORS.
INSTALL LOWER STAINLESS STEEL DOOR PANELS SIZED TO COVER THE
2-PIECE DOOR PANEL SEAM.

VACUUM-FORMED EVS CHILD SAFETY/ATTENDANT SEAT:

An attendant's seat base shall be fabricated from aluminum and shall be installed in a position at the head of the cot. The base shall house the vehicle's heat/AC unit as described in that section of this specification. The material shall be perforated to promote airflow to the unit. A vacuum-formed EVS bucket type seat with a built-in child safety seat and 3-point occupant restraint shall be installed on the seat base in the rear-facing position. The seat shall be upholstered to match the vehicle interior, and shall be capable of adjustment from front to rear. Under NO circumstances shall this seat be installed in any manner that allows it to swivel due to the lack of stability and weaker structural characteristics inherent in such designs. The entire seating assembly, as described here, shall be subjected to a Hygee sled test of at least 30 G's to test the structural integrity of the design, as well as seatbelt retention characteristics, in order to assure a certain level of safety for the vehicle occupants.

!!! SEAT BELTS TO BE RED.

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SEAT BELTS:

Each seating position shall include RED seat belts as follows:

Attendant seat: Three point seat belt.

CPR seat: Three point seat belt with removable third point latch.

Ends of bench: Three point seat belt with removable third point latch.

Center of bench: Two point seat belt.

Each seat belt shall have been tested to verify its latching capabilities and performance as well as the extent to which it allows movement by the "spooling effect" within the retractor. Those tests shall verify that this spooling effect allows less than three inches (3") of belt travel before latching.

ALUMINUM COMPOSITE INTERIOR CABINETS:

This specification requires an antimicrobial all aluminum composite modular cabinet design. Aluminum composite, a minimum of .5" thickness, is required over wooden cabinetry due to its lighter weight, greater durability, and the ease with which it can be decontaminated. The main cabinet wall shall be of modular construction. All individual cabinets shall be of similar construction. The main cabinet wall may not be constructed of any wood or wood product. Wooden cabinetry can warp, expand, contract, splinter, separate, or crack. Wood will also harbor bloodborne pathogens whereas aluminum composite can be easily cleaned. Aluminum composite will remain stable and securely mounted (no fibers to compress) over many years and miles of continuous service. For these reasons, wooden cabinets, even when laminated with another material, will not be acceptable. Bids received that utilize any material other than that which is specified above will be considered non-responsive and will be rejected without further consideration.

Color: WHITE.

CABINET TRIM:

The interior of the ambulance module shall have radius corner extrusions. The radius shall be a minimum of one and a half inches. The radius trim shall also include a removable cap that can be field replaced if damage occurs. (No Exception)

INTERIOR CABINETS, STREET SIDE

Methods of cabinet construction that utilize rivets or adhesives of any type will not be considered.

ALUMINUM COMPOSITE CABINET WARRANTY:

The aluminum composite cabinet construction, as described within this section, shall be warranted against any structural defects for a period of time not less than 15 years. This warranty shall be stated within the manufacturer's structural warranty document, and shall not be subject to any mileage limitations.

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CABINET BEHIND ATTENDANT SEAT:

A vertical storage cabinet shall be located behind the attendant seat. The upper storage area shall house the primary electrical distribution area. The lower section shall be used for miscellaneous storage. The left and right cabinet walls shall be fitted with Unistrut shelf track for an aluminum adjustable shelving as noted below. Each area shall be accessed through hinged doors. The entire cabinet shall be fabricated from aluminum. The interior of the cabinet shall be painted and trimmed as described in the cabinet construction section of the specification.

Number of Doors: 3 (Including Electrical Cabinet)
Number of Shelves (Adjustable): 1
Number of Shelves (Fixed): 3

Include the following provisions beneath the electronics cabinet (positioned from top to bottom):

- LOCKABLE STORAGE WITH (1) ADJUSTABLE SHELF. INCLUDE A 110-V AND A 12-VOLT OUTLET SURFACE MOUNTED ON UPPER BACK WALL - LH SIDE.
- AN APPROXIMATE 12"H STORAGE AREA TO HOUSE A SMITHWORKS IV WARMER. INCLUDE A SOLID RH HINGED DOOR.
- NORCOLD REFRIGERATOR IN BASE OF CABINET.

!!! ALL (3) DOORS OF THE LINEN CLOSET SHALL HAVE C2 LEVER LATCHES.
!!! THE ELECTRONICS CABINET AND THE CABINET IMMEDIATELY BELOW IT SHALL HAVE LOCKABLE LATCHES.

INTERIOR OCCUPANT PROTECTION:

For the safety of the attendants working in the patient area, the vehicle shall be equipped with an interior occupant protection system incorporating an emergency inflatable airbag system at both the attendant and the CPR seat locations. In the event of a side impact rollover collision, the bags shall be triggered by an electronic sensor to inflate and protect the occupants against severe head strikes typical of such collisions.

ATTENDANT'S SEAT PROTECTION:

The attendant seat location shall be protected by an inflatable head cushion technology as well as a unique inflatable tubular system to prevent the attendant from impinging into the danger zones of the inhalation area.

CPR SEAT PROTECTION:

The CPR seat location shall be protected by a combination of an inflatable tubular system at the forward side to protect against entry into the inhalation area as well as a system of progressive resistance head protection cushions.

TESTING:

All airbag seating locations shall have been tested with a variety of occupant sizes. Those tests shall include Hybrid III fully instrumented test mannequins including 5% child (115 lbs.), 50% female (163 lbs.), and 95% male (195 lbs.). Testing shall have included at least fourteen (14) fully instrumented destructive dynamic roll crashes and an additional six (6) side impact destructive crashes. Roll crash testing shall be performed at 17-19G's while side impacts shall be approximately 27G's.

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CERTIFICATIONS:

The vehicle must have been certified as compliant to standards ECE R29, SAE J2420, and SAE J2422.

STREET SIDE CABINET WALL:

The street side main cabinet wall shall be constructed from aluminum as described above. Each cabinet within the cabinet wall shall be designed and constructed as an individual welded aluminum box. Each box shall be insulated and soundproofed. The boxes shall then be bolted together to form the main cabinet wall. This design will allow for future modifications to the cabinetry should equipment storage requirements be updated. The cabinet wall assembly shall be further insulated against noise and temperature extremes. The entire assembly shall be bolted to the module body structure. Cabinets that are welded or otherwise permanently affixed to the module body structure will be unacceptable. Such permanent installation methods limit the ability to make design updates at a later time. They also increase the time and cost involved with regard to remounting the body onto a new chassis should that occasion ever arise. Likewise, cabinets mounted with the use of either rivets or adhesives of any kind will not be considered without exception.

Configure: PER DRAWINGS.

- !!! STREETSIDE CABINETS TO BE 18"D UNLESS OTHERWISE SPECIFIED.
- !!! UPPER INHALATION CABINET TO HAVE (1) FIXED CENTER DIVIDER AND (1) ADJUSTABLE SHELF ON EACH SIDE OF THE DIVIDER.
- !!! UPPER REAR CEILING CABINET TO BE APPROXIMATELY 51.75"W X 22"H.
- !!! CPR SEAT WIDTH TO BE 34" TO CREATE A 14" WIDE REAR COUNTERTOP AREA.
- !!! INCLUDE A LOWER INHALATION AREA CABINET WITH SLIDING PLEXIGLAS DOORS FORWARD OF PULL-OUT WRITING SURFACE.

CABINET SHELVING:

All interior cabinet shelves shall be fabricated from aluminum. The shelves shall utilize mini Unistrut adjustable shelf track.

Quantity: (2)

Locate: BOTH UPPER CEILING CABINETS.

CPR SEAT HEAD PROTECTION:

The progressive resistance head protection cushions shall incorporate layers of foam of increasing densities. Should a head strike occur, then the increasing density of the cushion as the impact progresses shall lessen the likelihood that the head will reach the aluminum cabinet material behind the cushions. It should be noted that standard single density foam cushions will not meet the requirements of this section. The bidder must have performed both actual impact tests as well as computer simulations in order to test the efficacy of this material in reducing head strike intensities to a survivable rate.

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RESTOCKING CABINET FRAMES, STREETSIDE:

The street side cabinet(s) listed below shall feature sliding Plexiglas doors that hinge upward for cleaning and restocking of the cabinet in addition to the normal sliding mode of operation. The extruded door frame shall be installed at the top with a full length piano hinge. This will allow the entire frame to flip upwards providing complete access to the cabinet. The door and frame shall be held in the "up" position with two gas-charged cylinders, and in the down position with two sliding dead bolt type latches. The remainder of the door construction shall adhere to the appropriate section of this specification.

Locate: BOTH UPPER CEILING CABINETS.

RECESSED-PULL OUT ASPIRATOR STORAGE:

The vehicle aspirator shall be install in a pull out drawer in the main cabinet riser. A recessed pocket will provide the storage for the pull out drawer and the pocket will be trimmed with aluminum diamond plate inside the exterior compartment.

Locate: IN RISER BELOW AIRWAY DRAWERS - JUST FORWARD OF CPR SEAT.
Door Style: PLEXIGLAS WITH NOTCH IN UPPER RH CORNER FOR SUCTION TUBING.
Size: 8.75"W x 9"H x 11"D (ALL I.D. DIMENSIONS).
Note: INSTALL USING ACCURIDE SIDE MOUNTED SLIDES.

PULL OUT WRITING TABLE:

A pull out writing table will be installed below the inhalation counter top. The writing surface is to be positioned for use at the attendant seat location.

Locate: THROUGH THE MAIN WALL RISER FOR USE FROM THE ATTENDANT SEAT.
POSITION AS HIGH AS POSSIBLE.

!!! INCLUDE A SMALL PULL RING LATCH (NOT SOUTHCO) AND A LARGE BALLCATCH LATCH TO HOLD IT IN THE CLOSED POSITION. INSTALL WITH LOCKING SLIDES TO HOLD IT IN THE OPEN POSITION.

!!! WRITING TABLE SIZE: 13.5"W x 16"D. (I.D.)

!!! INSTALL A SMALL TRIM LIP ON THE REAR FACING EDGE TO HELP HOLD THE COMPUTER IN PLACE.

CPR SEAT STORAGE:

The CPR seat cushion shall be hinged upward to allow for access to the area beneath it. This area shall provide miscellaneous storage. The storage area shall be fabricated with aluminum diamond plate and shall be as large as is possible given the location of the exterior compartmentation and wheel house.

!!! CUSHION OVERHANG TO BE .500".

RECESSED PADDLE LATCH FOR LIFT UP CPR CUSHION

A recessed paddle latch will be installed in the cabinet riser to provide access to the CPR seat storage area.

GAS HOLD OPEN FOR LIFT UP CPR SEAT CUSHION

A gas holdopen shall be installed to keep the CPR seat cushion in the upright position when opened.

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PULL OUT DRAWER IN MAIN CABINET WALL:

The main cabinet wall shall incorporate an aluminum drawer(s) as noted below. The drawer shall be painted to match the interior cabinets and shall include a face covered with CG-Tech material.

Locate: (2) DRAWERS STACKED ABOVE THE SUCTION DRAWER JUST FORWARD OF THE CPR SEAT PER DRAWING.

Size: 9.25"W x 3.625"H x 16"D (ALL I.D. DIMENSIONS)

Note: INSTALL USING ACCURIDE SIDE-MOUNTED SLIDES.
ALL SURFACES OF DRAWER TRAYS TO BE NON-PAINTED WITH A D/A FINISH.

LOCKABLE DOOR:

A single lockable door shall be installed on the cabinet noted below and designated in the vehicle drawings. The door shall include a Southco locking/latching device, and shall be constructed of Formica-covered material.

Locate: UPPER CENTER PORTION OF LINEN CLOSET.

Note: INCLUDE A LOCKING C2 LEVER LATCH ILOS.

ADJUSTABLE SHELVES:

The interior cabinet adjustable shelving, as noted below, shall be installed. This shelving shall be in addition to the shelving noted in the cabinet description elsewhere within this document. All adjustable shelving shall be aluminum and shall include 1" integral retention lips to prevent equipment and supplies from moving from its designated area.

Quantity: (1)

Locate: UPPER INHALATION CABINET.

CABINET DIVIDER:

An aluminum cabinet divider shall be installed as noted below. The divider is to be fabricated out of aluminum and painted to match the cabinet interior.

Quantity: (1)

Locate: CENTER OF UPPER INHALATION CABINET.

SHARPS/WASTE DISPOSAL ON MAIN WALL:

An aluminum cabinet shall be installed on a tip-out door in the main cabinet wall per the instructions listed below. This cabinet will house a sharps/waste disposal location. Both the sharps and the waste containers shall be easily removable from this area. The tip-out door will include a Southco latching device to hold it in the 'closed' position.

Locate: BELOW REAR COUNTERTOP PER DRAWING.

Size: 14"W X 17.5"H X 18"D (O.D. DIMENSIONS).

TO HOUSE A WINFIELD #185 SHARPS CONTAINER AND A RUBBERMAID #2953 6-QUART WASTE CONTAINER.

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INTERIOR CABINETS, CURB SIDE:

All of the cabinets located within this section shall meet the same standards for construction, design, materials, and testing as designated in the previous section. Failure of the bidder to provide cabinets meeting these criteria shall be grounds for rejection of the bid as being non-responsive.

SQUAD BENCH STORAGE:

A storage area, fabricated from .125" 5052-H32 aluminum, shall be installed beneath the squad bench cushions. This storage area shall be painted and trimmed per the cabinet construction section of this specification. Access to this area shall be gained by raising the bench cushion. This area shall be as large as possible given the presence of the wheelhouse directly beneath this area. Note that storage areas made of wood, whether or not they are laminated or otherwise covered with another material, will not be acceptable.

!!! MINIMUM 72" SQUAD BENCH LENGTH REQUIRED.

SQUAD BENCH:

A minimum 22" wide x 72" long split squad bench cushion shall be provided on the curb side of the patient area. The cushion shall not include posts or wheel cups for a stretcher. These cushions shall include air vents on the underside to allow air in and out as the cushion expands and contracts. Three seat belts shall be installed in this area for the seated position, and for backboard retention for a secondary patient. These belts shall meet all applicable testing requirements as set forth in the latest federal 'K' specs. In addition, these belts shall have been subjected to a Hygee sled test of at least 30g's. Squad bench restraints that have not been subjected to this testing will not be acceptable.

!!! CUSHION OVERHANG TO BE .500".

BENCH HEAD END:

The following bench restraint shall be installed at the head end of the squad bench.

NET AT HEAD OF BENCH:

A woven cargo type net shall be installed at the head of the bench. The net shall be anchored top and bottom and be removeable for cleaning purposes.

Locate: FOR INTERIOR ACCESS TO CRASH BARRIER COMPARTMENT.
INCLUDE ANKRA FASTENERS.

BENCH HOLD OPEN:

24 lbs. Gas piston style hold-open devices shall be installed on the flip-up squad bench cushion. These devices will provide for smooth and simple operation. For that reason substitute hold-open devices, such as ratchet style devices, will not be acceptable.

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BENCH HOLD DOWN:

Paddle style latches shall be installed on each flip-up bench cushion to hold the cushions in the 'closed' position. The operation of these latches shall be passive and shall require intentional unlatching in order to raise the squad bench cushion. Each latch is to be flush mounted in the face of the squad bench riser.

BENCH BACKREST:

A full backrest cushion shall be installed on the wall over the squad bench. The cushion is to extend the full width of the squad bench and shall be trimmed to match the interior of the vehicle. The upholstery shall be as described in the upholstery section of this document.

BENCH WALL DRAWER:

A drawer shall be installed on Grant slides on the bench wall as noted below. This drawer is to be .063" 5052-H32 welded aluminum. All welds are to be continuous. The drawer shall include a Southco latch to hold it in the 'closed' position.

Locate: UPPER PORTION OF CRASH BARRIER COMPARTMENT - AISLE FACING.

Size: APPROXIMATELY 25.5"W X 5.5"H X 16"D (ALL I.D. DIMENSIONS).

Note: INCLUDE A C2 LEVER LATCH AND ACCURIDE BOTTOM MOUNTED SLIDES.

INCLUDE BINNING STRIPS AND (5) ADJUSTABLE PLEXIGLAS DIVIDERS RUNNING FRONT TO BACK.

ALL SURFACES OF DRAWER TRAY TO BE NON-PAINTED WITH A D/A FINISH.

SHARP/WASTE DISPOSAL AT HEAD OF BENCH:

An aluminum drawer shall be installed in the face of the squad bench per the instructions listed below. This drawer will house a sharps/waste disposal location. Both the sharps and the waste containers shall be easily removable from the drawer. The drawer will include a Southco latching device to hold it in the 'closed' position.

Locate: BELOW FORWARD PORTION OF BENCH - FACING AISLE.

Size: 9.5"W X 10.625"H X 16.5"D (ALL I.D. DIMENSIONS)

TO ACCOMMODATE A #185 SHARPS CONTAINER AND A #8970 2-GALLON WASTE CONTAINER.

Note: INSTALL USING ACCURIDE SIDE-MOUNTED SLIDES.

ALL SURFACES OF DRAWER TRAY TO BE NON-PAINTED WITH A D/A FINISH.

BENCH CEILING CABINET:

A cabinet shall be installed at ceiling level over the full length of the squad bench. This cabinet is to be fabricated from .063" 5052-H32 welded aluminum. The interior of the cabinet shall be painted per the cabinet construction description listed elsewhere within this specification. The cabinet is to be accessed through hinged Plexiglas doors that are held in the 'open' position by gas piston hold-open devices. This cabinet is to be a maximum of 9"H with 72" headroom or 11"H with 74" headroom to allow enough clearance between the bottom of the cabinet and the top of the seat below to meet KKK-F requirements.

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INSIDE/OUTSIDE ACCESS:

An inside/outside dual access storage area shall be provided as indicated below. This area shall be accessible through both an exterior compartment door, and an interior cabinet door.

Size: TO ACCESS SHELVING IN CURBSIDE REAR COMPARTMENT.

Door style: FLUSH CLOSING PLEXIGLAS WITH CONTINUOUS HINGE.

ADDITIONAL CURBSIDE CABINETRY:

Provide and install a ceiling cabinet forward of the squad bench ceiling cabinet to house the following:

!!! FORWARD PORTION TO PROVIDE STORAGE FOR (3) GLOVE BOXES MOUNTED VERTICALLY. INCLUDE A HINGE-UP PLEXIGLAS LID WITH OVAL ACCESS OPENINGS.

!!! GLOVE BOX SIZE: 10.5"H X 5.5"W X 4"D (UNLESS OTHERWISE SPECIFIED).

!!! REAR PORTION TO HOUSE A CUSTOMER-SUPPLIED MED-VAULT 2 WITH WI-FI CAPABILITIES.

!!! MED-VAULT TO BE WIRED TO A FUSED BATTERY HOT CIRCUIT.

!!! SIZE: APPROXIMATELY 15"W X 11.625"H X 11"D (O.D.)

MODULE INTERIOR ACCESSORIES AND TRIM:

The following section addresses interior accessories and trim features. All installation locations, as noted below, shall be strictly adhered to by the bidder. The items in this section will directly influence the quality of care given to the patient, as well as the safety of the attendants. For these reasons the installation locations listed below must be met without exception.

IV HOOKS:

Cast Products recessed swing-down IV hangers shall be installed per the instructions listed below. These hangers are to be near flush mounted into the patient area ceiling to reduce their interference with the walkway when not in use. The arms of each hanger shall be rubberized so as to reduce the possibility of injury that may occur if contact is made with them. This style IV hanger shall be sufficient to meet Federal KKK-1822-F.

Quantity: (2)

Locate: (1) EACH SIDE OF COT IN CENTER POSITION AT APPROXIMATELY WAIST LEVEL.

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ANTI-MICROBIAL COATED COT CEILING GRAB RAIL:

A grab rail shall be installed in the ceiling as noted below. This rail is to be constructed of stainless steel. Integral stanchions shall be welded into place at fixed points along the length of the rail for attachment to the ceiling. The rail shall attach through aluminum mounting plates that are welded to the module roof structure for strength and durability. Because contamination occurs most often as a result of contact, this feature must be treated with an anti-microbial agent consisting of an inorganic ceramic coating embedded with silver ions. This coating shall be effective against a broad range of microbes including bacteria, molds, algae and fungi.

Locate: (1) 6' RAIL ABOVE COT IN CENTER POSITION ON CURBSIDE EDGE OF CENTER CEILING PLATE.

VERTICAL GRAB RAIL WITH ANTI-MICROBIAL COATING:

A vertical grab rail shall be installed as noted below. This rail is to be constructed of stainless steel with a brushed finish. The rail is to attach to the mounting location at each end. The attachment points shall be reinforced for added strength and durability. Because contamination occurs most often as a result of contact, this feature must be treated with an anti-microbial agent consisting of an inorganic ceramic coating embedded with silver ions. This coating shall be effective against a broad range of microbes including bacteria, molds, algae and fungi.

Locate: (1) ON REAR OF MAIN WALL FOR ENTRY ASSIST.
(1) ANGLED ON THE CRASH BARRIER COMPARTMENT - FACING THE STEPWELL.

PATIENT AREA DOOR GRAB RAILS:

Angled door handles shall be installed on the interior door panels of each access door. The handles shall be one-piece and shall be constructed of stainless steel. The handles shall feature smooth radius corners and flange mounts at each attachment point. Because contamination occurs most often as a result of contact, this feature must be treated with an anti-microbial agent consisting of an inorganic ceramic coating embedded with silver ions. This coating shall be effective against a broad range of microbes including bacteria, molds, algae and fungi.

COVE MOLDING:

A radius cove molding shall be installed at all areas of the floor that may have seams.

PROTECTIVE EDGE TRIM:

The 90 degree edges of the squad bench, the attendant seat riser, and the front wall cabinet shall be protected by a chamfered trim angle.

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CEILING:

The patient area ceiling shall be constructed of a bright white Alcopla aluminum composite material consisting of a polyethylene core laminated between two sheets of coated aluminum. The headliner shall be smooth, impervious to moisture, easy to clean and durable. It shall have the same rate of expansion and contraction as the aluminum body. Headliner that is padded or upholstered in any way will not be considered, nor will any headliner made of wood or wood products due to the lower degree of durability and the risk of contamination inherent in such materials. Plastic, fiberglass or ABS headliner material is not acceptable due to the cracking commonly causing by the differing rates of expansion. Lastly, the headliner material shall be treated with an antimicrobial agent. The bidder, at the request of the purchaser, may be required to submit proof of the application along with a detailed description of the agent used and the types of organisms that it effects.

FIRE EXTINGUISHER:

Two (2) 5# ABC fire extinguishers, with mounting brackets, shall be supplied on the completed vehicle per the notations below.

Locate: SHIP LOOSE.

PATIENT AREA RADIO SPEAKERS:

Two (2) speakers shall be installed in the patient area. The speakers shall include a volume switch that is integral to the rear action area switch panel. The volume shall be controlled by individual up/down switches of the type described in the electrical section of this specification.

Locate: (1) FRONT AND (1) REAR OF PATIENT AREA CEILING NEAR CENTER LINE.

COT MOUNTS AND ACCESSORIES:

The following cot mounting hardware shall be installed per the instructions listed below. The installation shall meet the hardware manufacturer's installation guidelines. In addition, the installation process shall have been subjected to Hygee sled testing as outlined elsewhere within this document.

COT MOUNT:

One (1) Stryker cot mount shall be installed per the instructions and recommendations of the hardware manufacturer. The cot mount shall be installed as noted below. The cot mount installation must meet all requirements for cot retention as set forth in the current federal specification.

Cot Model: POWER PRO.

Locate: CENTER POSITION - SPACED EVENLY BETWEEN RISERS.

Note: FLOOR TO BE DESIGNED TO ACCOMMODATE THE POSSIBLE FUTURE INSTALLATION OF A STRYKER POWER LOAD SYSTEM.

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INSTALL CUSTOMER SUPPLIED TRAFFIC EMITTER:

The manufacturer shall install a purchaser-supplied traffic emitter as noted below. Emitter shall be wired to a separate switch in the front console and shall automatically deactivate when the unit is placed into 'Park'.

Model: GTT - EXACT MODEL # TO BE DETERMINED.
Location: CENTERED ON FRONT FACE JUST BELOW WHITE LED.
Configure: INCLUDE SEPARATE ON/OFF SWITCH LABELED "OPTICOM".
UNIT TO ALSO BE WIRED INTO THE CHASSIS TURN SIGNAL CIRCUITRY.

ELECTRICAL EMERGENCY VISUAL WARNING SYSTEMS:

Warning lights are to be installed per the following instructions:

L.E.D. WARNING LIGHT ON ACCESS DOOR:

Install custom made L.E.D. light strip(s) on the side and rear patient area access door interior door panels. Each light strip shall include an array of L.E.D lights centered behind a integral lens for maximum light disbursement. Each light strip is to be twelve inches in length. When mounted the lens shall be flush with the inner door panel. The light strip shall flash when the access door is open, and the red flashing light circuit is activated. The lens shall be designed to provide a bright and intense light from a distance. As distance is decreased the light shall become less intense, preserving night time visibility for people working near the vehicle. L.E.D. lamps shall be used due to their "cold" operating temperature, low amp requirement, and long life expectancy. The lights are to provide additional lighting to warn traffic and pedestrians of a stationary emergency vehicle with open access doors, and to provide additional safety for the attendants.

Quantity: (1)
Color: RED.
Locate: LOWER PORTION OF CURBSIDE ENTRY DOOR.
Activate: WITH DOOR OPEN AND AND VEHICLE RUNNING.

WHELEN M SERIES LIGHTING:

!!! ALL M-SERIES LIGHTS TO HAVE BLACK FLANGES ILO CHROME FLANGES.

M6 SERIES RED L.E.D. LIGHTING:

Whelen M6 Series Red L.E.D. lighting shall be installed per the quantity and location requirements listed below. All lighting is to include the optional black flange.

Quantity: (2)
Location: IN REAR RISER - IN LINE WITH TAIL LIGHTS.
Lens color: RED.

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M7 SERIES RED L.E.D. LIGHTING:

Whelen M7 Series Red L.E.D. lighting shall be installed per the quantity and location requirements listed below. All lighting is to include the optional black flange.

Quantity: (4)
Location: (2) ON UPPER BAR OF CHASSIS GRILLE.
(2) ON LOWER BAR OF CHASSIS GRILLE.
Lens color: RED.
Note: FLASH LIGHTS IN AN "X" PATTERN.

HOUSING FOR 700 SERIES LED INTERSECTION LIGHTS

Cast Products #LH37774-1 & LH37775-1 polished housings for Whelen 700 series LED's shall be installed on the chassis fenders.

!!! INCLUDE HORTON FABRICATED ADAPTER PLATE TO ACCOMMODATE M7 LIGHTS.

M7 SERIES RED/WHITE SPLIT L.E.D. LIGHTING:

Whelen M7 Series Red/White side/side split L.E.D. lighting shall be installed per the quantity and location requirements listed below. All lighting is to include the optional black flange. Clear lens is standard for split color light color light heads.

Quantity: (4)
Location: (1) EACH SIDE ON CHASSIS FENDER - WHITE PORTION FORWARD.
(1) EACH SIDE ABOVE REAR WHEEL WELL - WHITE PORTION FORWARD.

M9 SERIES AMBER L.E.D. LIGHTING:

Whelen M9 Series Amber L.E.D. lighting shall be installed per the quantity and location requirements listed below. All lighting is to include the optional black flange.

Quantity: (1)
Location: ABOVE REAR DOORS.
Lens color: TO BE DETERMINED.
Note: LIGHT TO FLASH IN "PARK" ONLY PER WI TRANS 309 REQUIREMENTS.

M9 SERIES RED L.E.D. LIGHTING:

Whelen M9 Series Red L.E.D. lighting shall be installed per the quantity and location requirements listed below. All lighting is to include the optional black flange.

Quantity: (12)
Location: (4) ON FRONT FACE - INBOARD OF RED/WHITE SPLIT M9s.
(2) STREETSIDE - UPPER OUTBOARD CORNERS.
(2) CURBSIDE - UPPER OUTBOARD CORNERS.
(2) REAR FACE - UPPER OUTBOARD CORNERS.
(2) REAR FACE - WINDOW LEVEL.
Lens Color: RED.
Front Layout: RW/R/R/W/R/R/RW
Note: FRONT FACE RED LIGHTS TO BE PROGRAMMED FOR TOP/BOTTOM FLASH.
WIRE TO FLASH IN AN "X" PATTERN - STREETSIDE INNER UPPER WITH CURBSIDE OUTER LOWER.
REAR FACE LIGHTS TO FLASH IN AN "X" PATTERN WITH SOLID FLASH.

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M9 SERIES RED/WHITE SPLIT L.E.D. LIGHTING:

Whelen M9 Series Red/White side/side split L.E.D. lighting shall be installed per the quantity and location requirements listed below. All lighting is to include the optional black flange. Clear lens is standard for all split color light heads.

Quantity: (2)

Location: FRONT FACE - OUTBOARD POSITIONS.

Note: LIGHTS TO FLASH IN AN "X" PATTERN. CURBSIDE RED WITH
STREETSIDE WHITE.

M9 SERIES WHITE L.E.D. LIGHTING:

Whelen M9 Series White L.E.D. lighting shall be installed per the quantity and location requirements listed below. All lighting is to include the optional black flange.

Quantity: (1)

Location: CENTER OF FRONT FACE.

Note: FLASH LIGHT TOP TO BOTTOM USING ALTERNATING DOUBLE FLASH
PATTERN.

WHITE LIGHT CUTOFF SWITCH:

A switch shall be installed in the front control panel that will deactivate all forward facing white flashing lights. This includes wig-wag flashers if ordered.

SECONDARY MODE WARNING LIGHT PROGRAMMING:

Program all White emergency lighting and the pre-emption system to be "OFF" in "Park" (Secondary Mode).

M-SERIES CONTROL WIRE:

The unit shall include a control wire designed to allow for changing the flash patterns on the specified M-Series lighting.

Lights: PROGRAM ALL M-SERIES LIGHTING, UNLESS OTHERWISE NOTED, FOR
ALTERNATING STANDARD RATE DOUBLE FLASH (75 FPM) WITH ABILITY TO
SET THE INTERNAL FLASH PATTERNS AT THE LIGHT HEADS.

AUDIBLE EMERGENCY WARNING SYSTEMS:

The following audible emergency warning features shall be installed on the vehicle:

AIR HORNS:

Buell air horn trumpets shall be installed as described below. The air horn system is to include a Buell #1602-2 heavy-duty 12V air compressor and a Velvac #035105 air storage tank.

Configure: INSTALL (1) #1061 (10") AND (1) #1062 12" TRUMPET BELOW
BUMPER PER QVM REQUIREMENTS.

Activate: VIA (2) CHROME SWITCHES IN FRONT CONSOLE.

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FEDERAL EQ2B SIREN:

The vehicle manufacturer shall supply and install a Federal EQ2B 200 watt siren as noted below. The siren shall be fully electronic and shall feature digital output control.

Locate: FORWARD PORTION OF CONSOLE FACEPLATE - LH CORNER.
SIREN AMPLIFIER ON FLOOR OF ELECTRONICS CABINET.

SIREN INSTALLATION:

The electronic siren specified above shall be installed in the designated location and wired for operation through the speakers noted below.

SIREN SPEAKERS:

Cast Products SAD3806 and SAP3806 speakers shall be installed on the chassis per the instructions listed below. The speakers shall be wired for operation through the siren listed above.

Quantity: (2)

Locate: OUTBOARD PORTIONS OF FRONT BUMPER.

SIREN SPEAKER INSTALLATION:

The siren speakers specified above shall be bumper mounted and wired for operation.

LIGHTING:

Lighting information is noted below:

M6 SERIES SIDE BODY RUNNING LIGHTS:

One Whelen M6 Series L.E.D. light with a red lens and a chrome flange shall be installed on each side of the vehicle towards the rear of the body. These lights shall function as both running lights and turn signals.

Locate: ON REAR COMPARTMENT DOORS PER DRAWINGS.

EXTERIOR COMPARTMENT LIGHTING:

The compartment lighting for the exterior compartments noted above shall consist of LED lighting strips. Strips of LED lights shall be installed on each side of the compartment opening on the inside of the door jamb and shall direct the light back into the compartment. These light strips shall fit securely into clips installed in this location. These strips shall be semi rigid. Please note that rope lighting is not an acceptable alternative to this requirement.

ICC MARKER LIGHTS:

The required ICC marker lights for this vehicle are to be LED. Bidder should note that some lightbars have ICC lights already installed. In that case those lights shall be installed in lieu of the lights described here unless denoted within this document.

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TAIL LIGHTS:

Whelen M6 Series L.E.D. tail/brake, back-up, and turn signal lights shall be installed on the rear of the module body per the instructions listed below. All six of these lighting assemblies shall include the optional black flange.

Locate: STACKED ON BODY ABOVE RISER.
TURN ARROWS.
BRAKE/TAIL.
REVERSE.

BRAKE AND TURN SIGNAL INDICATOR LIGHTS:

Install a brake and turn signal indicator cluster on a removable panel in the header panel above the rear entry doors. Include (1) Red and (2) Amber Whelen OS LED marker lights with chrome flanges and clear lenses.

Locate: CENTERED BELOW INTELLITEC CLOCK.

WHELEN PIONEER SLIMLINE SCENE AND LOAD LIGHTS:

Provide and install Whelen Pioneer Slimline series lights #PSL1R5B lights as described below.

Quantity: (6)

Locate: (2) PER SIDE AS SCENE LIGHTS.

(2) ABOVE REAR DOORS AS LOAD LIGHTS.

Note: WIRE TO FUNCTION PER STANDARD LOAD AND SCENE LIGHT FUNCTIONS.

*****CUSTOMER NOTE:**

THESE LIGHTS HAVE A BLACK HOUSING BUT ARE NOT AVAILABLE WITH A BLACK FLANGE. IN ADDITION, THEY ARE A SEMI-RECESSED LIGHT.

ELECTRICAL POWER GROUP:

The vehicle electrical system is extremely important to this purchaser. The requirements for the onboard electrical system are noted in detail below. The bidder's electrical system, should it deviate in any way from that which is specified, shall be explained in great detail. This explanation shall present facts relative to the bidder's system only. The bidder shall not draw any comparisons between the electrical system being offered, and the system being specified. Any comparisons or decisions regarding one system versus another will be made solely by the purchaser and shall be based entirely on the written description as provided by the bidder at the time the proposal is submitted. All decisions made by the purchaser as to the merits of one system over another will be final and will not be subject to discussion, either verbal or written, at any point.

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ELECTRICAL CONTROL SYSTEM STANDARDS:

The electrical control system must meet all current ambulance design standards to include, KKK 1822 and AMD. A system is desired that is easy to use, simple in design and allows electrical problem diagnosis and repair time to be minimized. The electrical system must be thoroughly engineered and manufactured to allow simple personnel operation. Finally, the system must be designed so that the probability of experiencing dead batteries, shorted electrical components and engaging in lengthy troubleshooting procedures will be reduced. In some cases the electrical output provided by the chassis charging system can be marginal and under certain circumstances the electrical load can exceed the alternator output. In addition, some electrical systems have not provided proper circuit protection and at times have not provided adequate wiring for the load. To address the above objectives, the following minimum electrical system design is required:

CONVERTER ADDED CHASSIS CHARGING ENHANCEMENT:

The basic design for the chassis electrical output system must include equipment that provides adequate electrical needs to operate the vehicle's electrical components. In addition, a system is desired that continually monitors the chassis voltage and amperage outputs. The end result of the desired electrical output system is longer battery life, less down time associated with charging system repairs, and the fulfillment of each and every emergency response.

AUTOMATIC THROTTLE ADVANCE:

In order to reduce the number of component parts and unnecessary throttle linkages, the factory electronic throttle control shall be utilized to activate the throttle advance system. The controls shall require that the chassis be placed in Park or Neutral with the Module Disconnect switch in the On position and the Park Brake engaged before activation of the throttle advance. A digital display warning on the driver console, accompanied by an audible tone, must instruct the driver to Set Park Brake or Release Park Brake to engage or disengage the automatic throttle control. No Exceptions.

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AUTOMATIC LOAD MANAGEMENT:

In order to insure that onboard personnel attention is focused on victim care rather than being occupied with monitoring vehicle systems, an automatic load management system is required. The bidder must provide a system that continually monitors the vehicles charging system while it is sitting on scene. The system design shall have the ability to automatically shut down not less than ten pre-programmed electrical circuits to prevent a deficit charging condition while the vehicle is sitting at idle. The system shall be programmed to constantly scan the electrical system. If a deficit charging condition continues for more than one minute, a pre-programmed circuit shall shut down, correspondingly reducing the electrical draw. If the deficit condition continues, a second circuit shall automatically shut down. This process shall continue to repeat at one-minute intervals until at least ten circuits are shut down with corresponding load reductions. In the event any circuits are being controlled (disabled) by the load management system, the driver must be informed in two ways. First, a digital display warning shall appear on the driver information panel indicating Load Management Active. At the same time, the L.E.D. switch indicator light shall begin to flash for each specific circuit that is being disabled. Systems that cannot indicate specific circuits being affected by the Load Management System are not acceptable.

Load management systems must be programmed through a microprocessor based logic and memory system rather than a series of mechanical relays. Systems that require manual activation of Load Management will not be acceptable. Once the deficit condition ceases to exist, the system must be capable of restarting any disabled circuit without any action required by the driver.

The bidder is required to furnish a system that permits the end user, if he so desires, to determine prior to production the order of priority for shedding loads. Although the entire system must function automatically, it must also be designed so that it can be set by the end user to a mode for restocking, training, or maintenance convenience. The System Off setting shall not be merely a switch which would permit the operator to easily turn off Load Management. The intent is to keep the system active at all times when the vehicle is in operation.

LOW AMPERAGE SWITCHING:

Electrical devices that are not activated automatically shall be controlled from either the cab or patient area control panels through the use of manual switches. A low amperage switch that sends only an on/off signal to the central electrical distribution area is required. The switches provided shall have documented durability ratings at a minimum of fifty million (50,000,000) cycles. The switch design shall include magnetic technology to attain the required durability ratings. Switches that are rocker style will not be acceptable due to their tendency to degrade and fail in continued field use. To eliminate loose or poor contacts, it is unacceptable to have soldered or terminal type connections for the switches. The switches must be an inherent part of the panels.

SWITCH "ON" INDICATOR LIGHT:

All switches (unless otherwise noted) on the panels described below shall include a red L.E.D. indicator light that will indicate when power is being applied to a circuit. Designs that have indicator lights that activate to indicate switch position only are not acceptable. In addition, the indicator lights shall be independently programmable to flash or steady burn as required to meet the end user specification.

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SWITCH PANEL DESIGN:

Both the driver and the patient area switch panels must be designed so they can be easily decontaminated. Current designs make decontamination impossible when an attendant must use a contaminated glove to operate the switch panel while treating a patient. These areas become breeding grounds for bacteria. For this reason, the switch panels must be built in such a manner that there are no openings or crevices on the panel faces. The entire switch panel must be sealed with a protective overlay material. There shall be no printing or labeling on the face of this material. Holes in the panel through which switches, backlighting, or legends are inserted will be unacceptable. The panels must be cleanable with any commercially available spray type cleaner or disinfectant commonly used by EMS systems with no damage created by fluids leaking through openings onto the circuit boards or switch contacts.

The panel surface must be covered with a polyester film laminate for enhanced solvent resistance, strength, and durability. Both front and rear switch panels shall have been tested to at least a 24 hour exposure under DIN 42 115 Part 2 for the following commonly used chemicals: hydrogen peroxide <25%, bleach <20%, glycol, isopropanol, xylene, benzene, phosphoric acid <30%, ammonia <2%, hydrochloric acid <10%, acetic acid <50%, sulphuric acid <10%, diesel fuel, silicone oil, linseed oil, Windex, Formula 409, Fantastic, Wisk, Downey, washing powders, fabric conditioner, Ajax, and glycerin. The bidder shall be required, if asked, to provide the appropriate documentation showing that the above chemicals produced no visible damage after at least a 24 hour exposure. Bidders should be cautioned that commonly used polycarbonate or vinyl membrane fascia and nameplate substrates for electrical panels will not meet this requirement.

The panels shall be spill resistant to shed accidental moisture from spilled soft drinks or coffee cups. In addition, the surfaces of the panels shall be antimicrobial. This antimicrobial property is to be inherent in the surface material itself and shall not need to be reapplied at any point in the future. Products offered that include aftermarket treatments of the panel surfaces will not be considered.

SWITCH PANEL BACKLIGHTING:

All switch perimeters shall be lighted and raised for ease of switch location at night. In addition, the control panels shall include a red color LED indicator -to further distinguish switch activation. The switch panels shall include, on each panel, an individual intensity control. Switch panel lighting that operates at the same level as the cab instrument panel or that illuminates both the front and rear panels at the same intensity will not be considered. The bidder must provide totally independent control for each panel.

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CAB CONTROL SWITCHING AND LCD DISPLAY:

Switch Activation:

The cab control center shall include 8 critical buttons installed in protective enclosure with proper ventilation to maintain temperature. The following minimum circuits shall be provided on the switch panel:

- Module Disconnect
- Emergency Master
- Emergency
- Interior Lights
- Exterior Lights
- Home
- Apps
- Options

The following displays will appear on the Home screen of the cab console:

- Voltage (to the nearest 0.10 volt)
- Amperage (to the nearest amp)
- Outside Temperature
- Inside Patient Area Temperature
- Access or Compartment Door Open Warning Message and Display
- Electrical System Diagnostics
- 24 Hour Clock
- HVAC
- Emergency Brake Warning
- Accent Lighting and Backlighting Controls
- Patient Warning

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PATIENT AREA CONTROL SWITCHES AND LCD DISPLAY:

Switch Activation: Home

The patient area control center shall include 8 mission critical buttons installed in a backlight aluminum control panel. The following circuits shall be provided on the Home screen of the switch panel:

- Rear Heat/AC Activation and Separate Temperature Control
- Rear Heat/AC Fan Speed Control
- Power Vent
- Interior Lights
- Oxygen and Suction
- Patient Status
- Stop Clock
- Oxygen Line Pressure
- Oxygen Cylinder Pressure
- Exterior Lights

Digital Message Center:

The following digital displays shall appear on the faceplate of the patient area control console when selected:

- Patient Area Temperature
- Thermostat Setting
- Oxygen Tank Pressure
- Oxygen Line pressure
- Oxygen Warning

MODULE COMPARTMENT AND ACCESS DOOR SWITCHES:

Exterior circuits such as loading lights, side scene lights and compartment lights shall be activated by low amperage, non-mechanical switches. The type of switch desired is a magnetic sensitive switch that activates the circuit when the magnetic plane is broken. Plunger type switches are not acceptable because of their short useful life and higher amperage requirements.

DOOR OPEN INDICATOR:

A vehicle graphic door open warning indicator, with accompanying audible chime shall be installed in both the cab and patient area. A digital display shall appear on both consoles indicating which specific door has been left ajar.

Under no circumstances will red flashing lights or systems that do not specifically pinpoint a specific open door be acceptable.

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CENTRAL ELECTRICAL DISTRIBUTION AREA:

The electrical system smart modules shall be independent and include their own logic. They must include RAM memory to execute commands without having to rely on a central CPU. The system must not be centered around the use of a logic-controlled microprocessor built into a single circuit board. This logic control system is required to maximize reliability of the electrical system and to minimize downtime. It must be provided in order to match the type of control system used in the chassis and to prevent communication problems caused when dissimilar systems are employed. The design of the system must totally separate chassis operation from converter feature installations. In the unlikely event of converter component failure, the chassis must still remain operable.

The computer based electrical system must utilize components similar in design to the computerized chassis functions such as the OEM cruise control system, fuel feed system, transmission control system and braking system.

MULTIPLEXED ELECTRICAL COMMUNICATIONS SYSTEM:

Because most chassis manufacturers have chosen multiplex electrical communication technology to operate the chassis system, this purchaser requires the same technology for the converter-added systems. A standardized electrical control and wiring system is required. The vehicle manufacturer must own and control all rights to the electrical system. Standard systems controlled by outside vendors and modified for a specific vehicle or manufacturer will not be acceptable due to the unpredictability for future parts or service. Switch panels or modules that are not standard in design and are not interchangeable from one unit to another will not be considered. Since solid state logic-controlled technology is commonly available and not proprietary to any one manufacturer and has been proven to be more reliable with greater benefits, a blanket exception or clarification regarding the electrical specification is not acceptable and will be cause for automatic rejection of the bid.

In addition, the system will consist of a series of input / output control modules to manage and feed information and to control the various circuits required by this specification. Each smart module must have 32 outputs and 10 inputs. The smart modules shall have a chassis gateway interface with a 120 amp max output. Mate-Lock connectors shall be used for all load connections. Molex connectors shall be used for data transmission lines. Under no circumstances will systems be acceptable that utilize screw type terminals or card connectors due to their susceptibility to working loose due to vibration normally encountered on a vehicle.

Under no circumstances may the operation of the central processing unit or the input or output modules be based upon the operation of mechanical relays. Relay based systems require higher amperage operating current and rely on mechanical contact points designed to degrade with use, creating short duty cycles for the vehicle electrical system. Relay based systems, due to those limited short duty cycles, will not be acceptable for the requirements of this specification.

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UNIT FUNCTION:

The electrical control system shall be fully programmable and shall control a number of functions. The minimum functions to be controlled are as follows:

- No Load Starting Circuit (as defined in subsequent sections of this specification)
- Load Management
- Sequenced Start Circuit Activation
- Electrical System Diagnostics
- Climate Control Heat/AC operation
- Intensity Controls for Patient Dome Lights
- Oxygen Warning System (high and low pressure)
- All Warning Light Flashers and Flash Patterns
- Patient Status System
- Electrical Diagnostics

UNIT FUNCTION OPTIONS:

The electrical control system shall be capable of adding the following options:

- Up to four cameras
- Emergency GPS
- Pulse width Modulation
- USB port for field upgradability
- Seat belt monitor display
- Remote system activation from a mobile device
- Record ambulance PM schedule

The electrical control system shall include the ability to manage user defined maintenance issues. It shall also allow for the notification of critical care issues such as oil changes and tire rotations.

CIRCUIT PROTECTION:

Each converter added electrical circuit must have circuit protection for both over current limit and over temperature condition. The circuit protection shall be provided by solid-state circuit breaker/switching devices (MOSFETS) for both the input and output wire feeds for each circuit. The circuit protection shall require no user intervention such as that required for circuit breakers or fuses. The system shall be able to indicate an output fault warning.

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FIELD PROVEN AND TIME TESTED ELECTRICAL SYSTEM:

The converter-added electrical system represents the most important system in the design of this ambulance. Reliability and proven performance is essential. Therefore, the bidder must be able to demonstrate that they have at least ten years experience with solid state logic-controlled electrical systems installed in emergency vehicles. Further, the bidder must be capable of all programming required by the system without turning to outside vendors. This includes custom-programmed items as may be delineated in this specification.

The bidder may be required to demonstrate an in production or in service vehicle in order to guarantee compliance with this requirement. Prototype or first of a kind electrical systems are not acceptable. The purchaser may require the bidder to furnish specific references to further document compliance.

WIRING:

The following minimum wiring standards are required:

Identification:

By color, by itemized number, and by actual circuit name, stamped every 4-6"

Size:

Size will vary and will be dependent upon each wire being able to carry a minimum of 125% of the actual circuit load.

Protection of Wiring:

All wiring must be run in breakaway wire loom for protection against abrasion or chafing.

ELECTRICAL SYSTEM DIAGNOSTIC CHECK:

The electrical system must have built-in capability to self check each converter-added circuit and identify a short or open circuit by means of a single diagnostic switch. . The diagnostic system shall be operable from the driver's seat without exiting the vehicle. Diagnostic systems that are incorporated into exterior compartments, patient area interior cabinets, or remote locations will not be acceptable. The relevant information shall be displayed on the digital display on the cab switch panel. When the operator activates the Run Diagnostic switch, the unit will initiate the systems check. The digital display shall flash the message Running Diagnostics while the check is in progress. The system must go through all outputs for the vehicle to check for malfunctions. If a malfunction is found, the display shall stop flashing and steady burn to indicate the message Module #, Output #, Fail. This message will direct the service staff to the correct output module and the correct wire number in order to troubleshoot and repair the system. Once a failure is identified, the operator may continue to run the remainder of the diagnostic by pressing the Warning Reset switch. The bidder shall furnish with the vehicle a detailed diagram indicating each input and output module number and identifying each circuit controlled by the module.

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ELECTRICAL SYSTEM SUPPORT DATA:

Being able to service the electrical system should the need arise is of the utmost importance. To reduce the down time associated with servicing, the following information shall be provided at the time of delivery:

1. Electrical system operating instructions
2. Patient area heating/AC schematic and parts list
3. Oxygen and vacuum system schematic, parts list and leak check instructions
4. Battery and alternator schematic and system description
5. Radio communications installation instructions
6. Wire description list for converter added wiring
7. Individual schematics for all converter added electrical circuits

MODULE DISCONNECT DEFAULT:

The 'Module Disconnect' circuit shall default to the "ON" position when the battery switch is activated. Manual activation of the switch is not acceptable.

THREE SWITCH CONTROL PANEL:

The manufacturer shall install a 3-switch electrical control as noted below. The panel is to match the style of switches used on the main control panels.

Location: ON CRASH BARRIER COMPARTMENT - REAR FACING AT HEAD OF BENCH.

Switch Functions: (1) SWITCH FOR EACH BANK OF DOME LIGHTS.
(1) SWITCH FOR OXYGEN ACTIVATION.

AUTOMATIC BATTERY SWITCH:

An Inpower electronic battery switch shall be installed. The switch is to activate battery power through the vehicle ignition and is to include an automatic shutdown timer to deactivate battery power after the ignition is turned off. There shall be, incorporated into the vehicle, a blue LED push button power wake/timer switch to activate the electrical system with the vehicle turned "off".

Shut down timer interval: (5) MINUTES.

INVERTER INSTALLATION:

The vehicle converter shall furnish and install an inverter as noted below:

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VANNER INVERTER/CHARGER WITH 20-AMP AUX. POWER:

The vehicle converter shall furnish and install a Vanner #20-1050CUL inverter with charger and 20-amp auxiliary power. The inverter shall be located as noted below and shall power each onboard electrical outlet. There shall also be a Vanner Interface Module, Inverter Status Panel, Charger Status Panel, and Control Switch furnished and installed at the location noted below. The "Charger" portion of this unit shall be wired to the vehicle shoreline circuit. This unit shall meet KKK specification.

Locate: STREETSIDE FRONT ELECTRICAL COMPARTMENT.
 CHARGER STATUS PANEL ABOVE SHORELINE INLET.
 INVERTER STATUS PANEL ON INHALATION AREA WALL.

Activate: VIA SWITCH IN REAR CONTROL PANEL.

110-VOLT INTERIOR OUTLETS:

Duplex 110-Volt interior electrical outlets shall be installed. Quantity and location information is noted below. Each outlet shall be GFI protected and shall illuminate when powered.

Quantity: (3)

Locate: (1) ABOVE REAR COUNTERTOP - LOWER LH CORNER.
 (1) IN INHALATION AREA - LOWER RH CORNER.
 (1) ON CURBSIDE WALL ABOVE CRASH BARRIER COMPARTMENT - UPPER LH CORNER.

12-VOLT OUTLETS:

12-volt cigarette style electrical outlets shall be installed within the vehicle. Quantity, location, and adapter type are provided below. All 12 volt outlets shall be protected by a Schottky medical isolator. In addition, the 12 volt outlets shall be wired through a 20-amp manual reset circuit breaker. All 12-volt outlets shall be labeled.

Quantity: (6)

Locate: (1) ABOVE REAR COUNTERTOP - LOWER RH CORNER.
 (1) IN CENTER PORTION OF LINEN CLOSET. (SMITHWORKS IV WARMER)
 (2) IN RECESSED POCKET IN INHALATION PANEL.
 (1) IN INHALATION AREA - LOWER RH CORNER.
 (1) ON CURBSIDE WALL ABOVE CRASH BARRIER COMPARTMENT - UPPER LH CORNER.

Configure: SHORELINE AND IGNITION ACTIVATION.

SHORELINE:

The vehicle shall be equipped with a Kussmaul Super Auto Eject non-arcing shoreline. The male shoreline inlet shall be installed as noted below. This inlet shall be a straight three-prong type and shall include the female adapter plug. The shoreline shall be designed so that the plug will automatically eject from the inlet in the event that the vehicle is started while still plugged in. The shoreline shall include a hinged cover to protect it from the elements. The shoreline system shall be designed to handle a 20-amp load, and shall also include a 20-amp inline GFI breaker.

Locate: STREETSIDE OF MODULE BODY AS FAR FORWARD AS POSSIBLE.
Color: RED.

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EZ PLATE:

The shoreline shall be installed as noted above using a Kussmaul EZ Plate.

Note: INCLUDES POWER INDICATOR LAMP.

BLOCK HEATER WIRED THROUGH SHORELINE:

The engine block heater shall be wired through the vehicle shoreline system. The wiring shall include a cutoff switch, to be installed with the onboard electrical components, to disable this feature for seasonal use.

EXTRA CIRCUIT BREAKER:

A spare 15-amp manual resetting circuit breaker shall be installed as a provision for the possible installation, at a later time, of additional equipment. This feature is in addition to any prewire that may be included elsewhere within this vehicle specification. The total number of spare breakers is listed below:

Quantity: (1)

CAB SWITCH PANEL INSTALLATION:

The cab control panel for the converter-added electrical circuits shall be flush mounted in the upper face of the cab console. The mounting surface shall be angled downward so that the LCD screen and switches are visible to both the driver and passenger positions.

REVERSE ACTIVATED REAR SIDE SCENE LIGHTS:

The rear scene lights on either side of the vehicle shall be programmed to be activated when the vehicle is placed into reverse gear. This is in addition to the other modes of operation as described elsewhere within this document. This feature shall be attained through the programming of the onboard electrical system. Systems that require additional wiring in order to provide this feature are not acceptable.

SIDE DOOR ACTIVATED CURB SIDE SCENES:

The curb side scene lighting shall be programmed to be activated when the patient compartment side access door is opened. This is in addition to the other modes of operation as described elsewhere within this document. This feature shall be attained through the programming of the onboard electrical system. Systems that require additional wiring in order to provide this feature are not acceptable.

REVERSE ACTIVATED LOADING LIGHTS:

The load lighting on the rear of the vehicle shall be programmed to be activated when the vehicle is placed into reverse gear. This is in addition to the other modes of operation as described elsewhere within this document. This feature shall be attained through the programming of the onboard electrical system. Systems that require additional wiring in order to provide this feature are not acceptable.

AUDIBLE LOW VOLTAGE ALARM::

An audible alarm shall be programmed to warn the operator should the vehicle's voltage drop below 11.8 volts for 120 seconds.

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EMERGENCY BRAKE WARNING:

When the vehicle is placed into 'Park' or 'Neutral' with the "Module Disconnect" switch 'On' and the "Red Flashing Light" switch 'On', then an audible alarm, accompanied by a visual readout on the cab console digital display, shall warn the vehicle operator to engage the emergency brake. Likewise, when the vehicle is placed into gear, then the same alarm will sound with a visual display warning the operator to disengage the emergency brake.

TECNIQ #E13 SERIES RED/WHITE REPORT LIGHT:

Report lighting, as described below, shall be installed within the patient area. The fixture shall be LED. The fixture shall be controlled via a switch(es) on the attendant control panel as noted below.

Quantity: (1)

Locate: ABOVE REAR CONTROL PANEL.

!!! LIGHT TO BE A TECNIQ #E13-WS0R-1 RED/WHITE FLUSH MOUNT LED LIGHT WITH STAINLESS STEEL TRIM RING.

!!! EACH COLOR OF LIGHT TO BE SWITCHED SEPARATELY FROM REAR CONTROL PANEL.

INTELLITEC LED CLOCK:

An Intellitec Time Manager clock shall be provided and installed as noted below:

Locate: ABOVE REAR DOORS.

TECNIQ #E06 SERIES STEP WELL LIGHT:

An LED light will be installed above the side step well to light the step well area when the side access door is opened. The light will be activated by a magnetic door switch installed on the door.

Locate: RH SIDE OF STEPWELL - BELOW FRONT WALL.

Note: LIGHT TO BE A TECNIQ #E06-WS00-1 4" ROUND LED LIGHT WITH STAINLESS STEEL TRIM RING.

CAB DOME LIGHTS:

Two (2) Signal Stat dome lights shall be flush mounted into the cab headliner. Each light shall have an individual activation switch on the light itself. The lens color is noted below.

Lens Color: RED.

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PROGRAMMABLE LIGHT TIMER:

A momentary switch shall be installed as noted below to operate the specified lighting with the battery switch in the 'Off' position and the shoreline plugged in. The switch shall activate a programmable timer that will automatically shut the lights off after the specified period of time. This timer shall be field-programmable to allow the time to be adjusted after the vehicle has been delivered. The initial time setting shall be as follows:

Locate: ON CRASH BARRIER COMPARTMENT FACING STEPWELL.
Light(s) Controlled: DOME LIGHT.
Configure: SET ELAPSED TIME FOR 15 MINUTES FOR INITIAL SET-UP.

REFRIGERATOR:

The vehicle shall be equipped with a Norcold NR740BB-H 12V refrigerator. The unit shall be wired through the vehicle's 12V electrical system. Location of installation is noted below.

Locate: BASE OF LINEN CLOSET PER DRAWING.

DOMES LIGHTS:

Whelen #80C0ECHR LED dome lights shall be installed in the patient area ceiling. Quantity and location information is listed below. The lights shall be recessed into the headliner and shall not protrude from the ceiling more than 1". All dome lighting shall have hi-low adjustability and shall be controlled via solid state switching at the patient area electrical control console.

Locate: (5) OVER STREETSIDE EDGE OF COT IN CENTER POSITION.
(5) OVER CURBSIDE EDGE OF COT IN CENTER POSITION PER DRAWING.

!!! LIGHTS TO OPERATE AT "HIGH" INTENSITY WHEN DOME TIMER IS MANUALLY ACTIVATED.
!!! WITH BATTERY SWITCH IN "OFF" POSITION, PROGRAM THE DOME LIGHTS TO ACTIVATE AT "LOW" INTENSITY WHEN A MODULE ACCESS DOOR IS OPENED. LIGHTS TO REMAIN ON FOR (5) MINUTES.
!!! INCLUDE SEPARATE DIMMER SWITCHES FOR EACH BANK OF LIGHTS.

PREWIRE FOR FUTURE STREAMLIGHT INSTALLATION:

Prewire shall be provided as noted below for future installation of Streamlight charger bases.

Quantity: (2)
Locate: UPPER LH CORNER OF STREETSIDE REAR COMPARTMENT.
Configure: BATTERY HOT.
Note: LEAVE 24" COILED AND CAPPED.

!!! FUSE LOCATIONS TO BE EASILY ACCESSIBLE TO CUSTOMER.

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AUXILIARY BRAKE LIGHTING:

The rear red flashing lights, as specified elsewhere within this document, shall be wired so that they illuminate when the brake pedal is depressed. This lighting is in addition to the specified brake/tail lights.

Lights: WINDOW LEVEL M9s.

Configure: FLASHERS TO OVERRIDE BRAKE LIGHT FUNCTION.

Note: FEATURE TO ACTIVATE REGARDLESS OF MODULE DISCONNECT SWITCH STATUS

HEATING AND AIR CONDITIONING:

A temperature control system is desired that provides quick and simple operation while maintaining a uniform temperature throughout the patient compartment. The unit itself must be located so that it is easy to access for service. This location must also be near the O.E.M. heat/AC connection points when provided so as to increase the overall efficiency of the unit. The following minimum design standards must be adhered to in order to best meet the needs of this purchaser.

SYSTEM CONTROLS:

The climate control functions shall be controlled through a primary location in the inhalation panel, and through a secondary location in the cab electrical control console. The switches used for the operation of this system shall be identical to the switches described in the "Electrical" section of this specification. Switches shall be present in the front console to select either 'Heat', 'A/C', or 'Off' functions and to select the desired temperature. Switches shall be present in the rear control panel to select either 'Heat', 'A/C', or 'Off' functions, 'Automatic' or 'Manual' mode of operation, and to select the desired temperature.

THERMOSTAT:

The temperature level shall be adjustable from both the front and rear electrical control panels for the 12V system. Two switches at each location shall be used to scroll through desired temperature settings on one degree intervals. Once the desired temperature is set, then the system shall retain that setting regardless of the position of the battery switch. The temperature sensor for the system shall be located at the inhalation panel so as to attain a true patient compartment temperature. The temperature setting and the actual temperature reading shall be viewable from both the front or rear digital displays.

This system is to be controlled through the converter-added electrical system. Under no circumstances shall household type thermostats be acceptable.

SYSTEM OPERATION:

The system shall allow for both automatic and manual operation. When set to the manual mode the fan speed shall be infinitely adjustable from the rear control panel for extra ventilation. When set to the automatic mode the fan speed shall be controlled by the thermostat setting. The temperature that is selected shall be continuously maintained. When the selected temperature has been reached, then the system shall automatically cycle the fan speed down to reduce unnecessary electrical load.

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HEATER WATER CONTROL:

The flow of hot water from the chassis to the converter-added heat/AC system shall be controlled by an electrically operated valve located under the hood. Water flow to the rear heater shall be activated when either the front or rear heater switch is turned to the 'On' position. It is a requirement of this specification that this type of valve be used unless the converter is supplying a self-contained heat-AC system. The term "self-contained" is defined as being a unit that does not require any water flow from the chassis. Under no circumstances will manual valves be considered. Manual valves are inconvenient and tend to leak.

UNIT LOCATION AND SERVICE:

It is required that the heat/AC unit be installed inside a custom-made aluminum box beneath the attendant's seat. This box shall be perforated to provide air flow to the heat/AC unit mounted beneath the seat. This is required for efficiency, serviceability, and safety.

Many O.E.M. chassis builders provide tap-in points for the converter-added heat/AC unit behind the driver's seat. Therefore, system efficiency is higher when the hot water from the chassis is pumped to the area beneath the attendant's seat. Efficiency is not lost by pumping the water over an extended distance or up to ceiling level. Such a condition would naturally result in reduced patient area temperature levels as excessive flow resistance would be present.

The attendant's seat shall be installed on a hinged top cover for the aluminum heat/AC system housing. This allows the seat to be hinged forward and out of the way for service work. The unit will be accessible by removing three bolts located behind the seat and lifting the seat forward as opposed to dismantling cabinetry, etc.

In the unlikely event of a system leak the specified installation location will allow the leakage to run out onto the ground. Systems that are installed above cabinetry may leak into the cabinets, thus ruining the cabinets (if they are wood) and the cabinet contents.

BRUSHLESS MOTOR:

The converter-added HVAC system, as noted below, is to include a brushless motor.

FILTRATION SYSTEM:

A replaceable carbon filter shall be installed at the air intake area of the heat/AC system. Replacement of the filter shall be simple, and shall require very little time so as to assure that the vehicle will not have to be taken out of service. Replacement filters shall be readily available and shall be capable of being cut to the proper size to fit the vehicle.

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AIR FLOW:

The installation of the heat/AC system shall include an air duct system to direct the airflow in such a way as to provide uniform temperature levels throughout the patient compartment. Air intake shall be from the floor level. The air shall be channeled through a duct that is aft of the heat/AC unit. The air shall exit through adjustable vents at the ceiling level above the attendant's seat. This design will allow for a circular flow of air throughout the patient compartment.

The specified design will separate the intake and exhaust ports. Separation of the intake and exhaust will decrease air turbulence and improve overall efficiency of the system. Systems that combine intakes and exhausts within the same grille work will not be acceptable.

12V HEAT/AC SYSTEM:

The 12V heat/AC system shall be installed per the instructions listed above. The BTU and CFM ratings on this unit shall be as follows:

Heat: 65,000BTU
A/C: 32,000BTU
CFM: 650

RECEIVER/DRYER KIT, ALL FORD F-SERIES CHASSIS

Receiver/dryer kit #63 000 744 w/pressure switch shall be installed in the air conditioning system.

VENTING SYSTEM:

Install a 400cfm exhaust and a static intake vent. Each vent cover is to the 9.5" square and is to feature a polished finish.

CAB CONSOLE AND COMMUNICATIONS:

The vehicle communications and console features are designated below:

ANTENNA COAX #1:

An RG 58U coax shall be installed. A removable access plate in the patient area ceiling shall be provided for access to the exterior termination point located on the module body roof. Under no circumstances shall the vehicle design necessitate disassembly of the interior finish work to access the coax termination point. The coax shall terminate at the following locations:

Exterior Termination: FRONT CENTER OF MODULE ROOF.
Interior Termination: ELECTRONICS CABINET.

!!! DEALER TO INSTALL A CUSTOMER SUPPLIED CRADLEPOINT DOME ANTENNA/COAX IN THIS LOCATION. COAX TO SERVE AS A "FISH-TAPE" FOR DEALER.

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ANTENNA COAX #2:

A second RG 58U coax shall be installed. A removable access plate in the patient area ceiling shall be provided for access to the exterior termination point located on the module body roof. Under no circumstances shall the vehicle design necessitate disassembly of the interior finish work to access the coax termination point. The coax shall terminate at the following locations:

Exterior Termination: SPACED EVENLY ON MODULE ROOF.

Interior Termination: BEHIND DRIVER'S SEAT.

ANTENNA COAX #3:

A third RG 58U coax shall be installed. A removable access plate in the patient area ceiling shall be provided for access to the exterior termination point located on the module body roof. Under no circumstances shall the vehicle design necessitate disassembly of the interior finish work to access the coax termination point. The coax shall terminate at the following locations:

Exterior Termination: SPACED EVENLY ON MODULE ROOF.

Interior Termination: BEHIND INHALATION PANEL.

ANTENNA COAX #4:

A fourth RG 58U coax shall be installed. A removable access plate in the patient area ceiling shall be provided for access to the exterior termination point located on the module body roof. Under no circumstances shall the vehicle design necessitate disassembly of the interior finish work to access the coax termination point. The coax shall terminate at the following locations:

Exterior Termination: REAR CENTER OF MODULE ROOF.

Interior Termination: BEHIND DRIVER'S SEAT.

K-94 ANTENNA BASE:

The ambulance manufacturer shall provide and install a K-94 antenna mount(s). The mount(s) shall be located per the quantity and location information listed below and shall utilize the coax cable(s) specified in antenna coax options listed above.

Coax Number(s): 2,3,4 ONLY.

RADIO PULL WIRE:

A pull wire shall be installed to aid radio cable installation and prevent removal of interior panels once the vehicle has been completed.

Location: BEHIND DRIVER'S SEAT.

Terminate: BEHIND INHALATION PANEL.

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CAB CONSOLE:

A console shall be installed in the cab. The console shall be installed at floor level and shall allow space for siren and radio head installation. The console shall be color coordinated with the cab interior. The top of the console shall be on a slant and shall house the recessed emergency control panel and integral digital display. Under no circumstances shall the console interfere with the OEM vehicle controls or gauges, nor shall the control panel be installed in such a manner as to interfere with either the OEM vehicle controls, gauges, or the driver's line of vision.

CONSOLE EXTENSION:

An aluminum console extension shall be fabricated and installed in the vehicle cab. The extension shall attach to the front console and shall include a location to mount siren and/or radio heads, as well as three slots for storage of map books and binders. The console extension shall be covered in black Scorpion material for ease of cleanup.

!!! CONSOLE TO INCLUDE (2) SEPARATE HORIZONTAL FACEPLATES.
FRONT PLATE TO BE APPROXIMATELY 15"L.
REAR PLATE TO BE APPROXIMATELY 13"L - LEAVING ROOM FOR CLIP BOARD
SLOT TO BE MOUNTED ON REAR CAB WALL.

Locate: EQ2B SIREN HEAD IN UPPER LH CORNER.
LEAVE ADDITIONAL SPACE REARWARD OF SIREN FOR A DEALER INSTALLED
MOTOROLA RADIO HEAD AND A FIRECOM CONTROL HEAD.

Note: ALL COMPONENTS OF FORWARD HORIZONTAL FACEPLATE TO BE ON LH
SIDE OF PLATE, LEAVING RH PORTION OF FACEPLATE CLEAR.

!!! THE FOLLOWING PROVISIONS SHALL BE INCLUDED ON THE REAR FACEPLATE.

Locate: (2) CUPHOLDERS FRONT TO BACK IN RH PORTION.
(1) KNOX SENTRALOCK IN UPPER LH CORNER - DEALER INSTALLED.
(1) KNOX KEY BOX IN LOWER LH CORNER - DEALER INSTALLED.

CUP HOLDERS:

Two (2) large cupholders shall be installed in the front console with easy access from both the driver's and the passenger's seats.

RADIO POWER/GROUND:

The vehicle manufacturer shall install heavy gauge cable B positive and ground for radio power. Termination is to be to insulated studs.

Locate: FRONT CONSOLE.
Configure: BATTERY HOT.

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RADIO POWER/GROUND:

The vehicle manufacturer shall install three heavy gauge cables for radio power. One cable to be wired battery hot, one cable to be wired ignition hot and one cable to be wired as ground. Termination is to be to insulated studs.

Locate: BEHIND DRIVER'S SEAT (AS LOW AS POSSIBLE).
BEHIND INHALATION PANEL.

Note: FUSES TO BE EASILY ACCESSIBLE TO CUSTOMER.

***CUSTOMER NOTE: EACH POWER STUD HAS A 50-AMP CAPACITY.

BLANK REMOVABLE RADIO PANELS:

Blank removable panels shall be installed for future installation of radio heads.

Locate: FORWARD PORTION OF INHALATION PANEL.

Note: EXACT LOCATION TO BE DETERMINED AT PRE-BUILD CONFERENCE DUE TO CONFLICTING LOCATIONS DESCRIBED IN RFP (PAGE 7).

CAB MOUNTED CLIP-BOARD HOLDER:

Install a (1) slot angled clip-board holder on the rear cab wall between the cab seats. Holder to be coated in Black Scorpion material.

Locate: CENTERED ABOVE THE REAR OF THE CONSOLE EXTENSION.

Size: 14"W x 8"H x 1.75"D (O.D. DIMENSIONS)

FIRECOM SYSTEM PRE-WIRE:

!!! HORTON TO RECEIVE A SPOOL OF 6C FIRECOM INTERCOM CABLE. INSTALL CABLE FROM THE FRONT CONSOLE TO THE INTENDED HEADSET LOCATIONS LISTED BELOW.

Locate: (1) ABOVE INBOARD CORNER OF EACH CAB SEAT.
(1) ON UPPER CURBSIDE WALL ADJACENT TO FORWARD SEATING POSITION.
(1) ON MAIN WALL BELOW UPPER REAR CEILING CABINET ADJACENT TO CPR SEAT.
(1) ON INHALATION AREA WALL PER DRAWING.

Note: TERMINATE ALL LOCATIONS IN MODULE WITH A STAINLESS STEEL COVER AND RUBBER GROMMET, LEAVING AN 18" SERVICE LOOP.
COIL CABLES IN CAB NEAR REAR OF HEADLINER FOR EASY ACCESS.

!!! HEADSET INSTALLATION AND FINAL HOOK-UPS TO BE HANDLED BY DEALER.

OXYGEN AND SUCTION SYSTEMS:

Reliability, safety, and ease of operation are essential characteristics of the onboard oxygen and suction systems. System design must meet the following minimum guidelines. Bidders are asked to respond to each section appropriately per the bid requirements and to explain any variations to these requirements.

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SWITCHING FOR OXYGEN AND SUCTION:

The rear switch panel shall contain two switches labeled "OXYGEN" and "VACUUM". Each of these switches shall electrically activate those respective systems. That activation shall be instantaneous. Systems that are not instantaneously responsive to their activation will not be considered.

SYSTEM DESIGN:

A single piece manifold assembly shall serve as the basis for the oxygen delivery system. The manifold assembly shall incorporate ports for installation of O2 lines to all specified outlets, an electrically activated oxygen delivery solenoid, and a manual bypass valve. The assembly shall be installed behind the inhalation panel and shall be easily accessible.

ELECTRICAL OXYGEN ACTIVATION:

The switch, located on the rear control panel and labeled "OXYGEN", shall activate the solenoid. This design will allow for the instantaneous flow of oxygen while eliminating the need to manually turn a valve to initiate oxygen flow.

MANUAL BYPASS:

The oxygen solenoid shall be equipped with a manual bypass valve. Located behind the inhalation panel, the valve shall be easily accessible so that, in the unlikely event of an electrical failure, administration of oxygen may continue.

SYSTEM REGULATION:

The patient area shall be free of high pressure oxygen lines. To accomplish this the vehicle converter shall install a KKK approved regulator at the oxygen cylinder. The regulator shall include an integral dial type gauge to monitor the cylinder contents. A single low pressure line shall be installed from the regulator to the O2 manifold assembly. This method shall insure that all high pressure is maintained in an exterior compartment away from the interior patient area.

OXYGEN LINES:

The O2 line connecting the regulator to the manifold assembly shall be rated at 200 psi working pressure and 1,250 psi burst pressure. The line shall be UL approved. There shall be NO connections installed in the line between the regulator and manifold assembly as these create a possibility for leakage. All connections shall be DISS style and shall be specific to the gas being supplied.

LINE PROTECTION:

The O2 line shall be protected from crimping through the installation of a flexible spring guard on the portion of the line in the cylinder storage compartment.

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SYSTEM MONITORING:

The condition of the oxygen system shall be continually monitored and reported to the vehicle operators through the vehicle's onboard electrical system. Readouts containing the information listed below shall be available primarily at the patient area control console. The secondary location for availability of this information shall be the cab console. The information available shall include the following:

- Cylinder Pressure
- Line Pressure

In addition, this system shall be designed to offer a warning, both audible and visual, if the condition of the oxygen system falls outside of the following pre-programmed parameters:

- Low Cylinder Pressure (500 psi or below)
- Low Line Pressure (40 psi or below)
- High Line Pressure (75 psi or above)

These oxygen system warnings shall immediately notify the personnel of a problem, again, via a readout and audible alarm. The system shall require the personnel to acknowledge receipt of the information.

PRELIMINARY SYSTEM TESTING:

The oxygen system shall be tested prior to installation in the vehicle. This test shall be performed by the vehicle manufacturer and shall subject the system to three times (3X) the working pressure. This test shall be conducted for a minimum of four (4) hours.

FINAL SYSTEM TESTING:

The completed system shall be tested again once it is installed in the vehicle. This test shall be performed at working pressure for a minimum of four (4) hours. After the system has passed the inspection process it shall be capped and tagged per Federal KKK specifications.

ADDITIONAL OXYGEN AND VACUUM SUPPLIES:

The oxygen and suction systems shall be complete upon delivery with the exception of the O2 cylinder. The cylinder shall be supplied and installed by the purchaser after delivery of the vehicle has taken place.

OXYGEN BOTTLE MOUNT, VERTICAL TRACK FOR QRM-V:

Vertical track for mounting of a QR-MV O2 bottle mount shall be welded on the back wall of the compartment in the right hand corner. The O2 bottle mount is adjustable for "M" or "H" size tanks.

ZICO QRM-V CYLINDER BRACKET:

A Zico QR-MV oxygen cylinder bracket shall be installed in the main O2 compartment.

Locate: STANDARD LOCATION.
Tank Size: "M".

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ACCESS TO CYLINDER VALVE FROM PATIENT AREA:

A Plexiglas door with a Southco non-locking flush pull ring latch shall be provided in the patient area wall for access to the oxygen cylinder valve. The door shall be hinged so that it swings into the patient care area of the module. The opening shall be trimmed with anodized aluminum edging.

OXYGEN OUTLETS:

Two oxygen outlets shall be installed in the rear inhalation panel unless otherwise noted below.

Locate: ON INHALATION AREA WALL DUE TO REDUCED LENGTH INHALATION PANEL.

ADDITIONAL OXYGEN OUTLETS:

Additional oxygen outlets shall be installed as noted below.

Quantity: (2)

Locate: (1) ON CURBSIDE WALL ABOVE REAR PORTION OF CRASH BARRIER
COMPARTMENT - UPPER RH CORNER.

(1) ABOVE HEAD OF COT.

NCG STYLE OXYGEN AND SUCTION OUTLETS:

The oxygen and suction outlets installed in the vehicle shall be NCG style outlets.

CYLINDER WRENCH:

A cylinder wrench shall be installed inside the oxygen compartment. The wrench shall be installed in such a way as it will not move or rattle. The wrench shall be chained to the compartment so that it cannot be removed, however, the chain must not interfere with the operation of the wrench.

Locate: ON RH COMPARTMENT WALL.

PRIMARY VACUUM OUTLET:

A single vacuum panel shall be installed in the inhalation area unless otherwise noted. The outlet shall be of the same style as those of the oxygen system and shall be connected to the onboard vacuum pump.

Locate: IN CONJUNCTION WITH SUCTION DRAWER LOCATION.

SSCOR ASPIRATOR:

The manufacturer shall furnish and install an SSCOR suction system. The system shall include a #22000 wall-mounted regulator, and a #23002 canister holder. The aspirator shall be accessible for use from the inhalation area per the attached prints and plumbed to the pump described below.

VACUUM PUMP:

The aspirator system shall be operated through a CFP #107CDC20 12V vacuum pump that is located inside an exterior compartment. The pump shall be activated by a switch on the inhalation area electrical control panel.

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LETTERING:

Lettering information is noted in detail below:

36" STAR OF LIFE EMBLEM:

The bidder shall supply and install the 'Star of Life' emblem(s) requested by the purchaser. The total quantity required is noted below. The emblems shall be made from 36" white Scotchlite that is silk screened in blue. A Mylar laminate shall then be applied to the emblem's surface in order to protect against scuffing, and to provide added longevity. Emblems that do not have this protective covering are not acceptable.

Quantity: (1)

Locate: CENTER OF MODULE ROOF.

HORTON OWNER'S MANUAL INFORMATION:

Two (2) copies of the Horton owner's manual shall be furnished with the completed vehicle.

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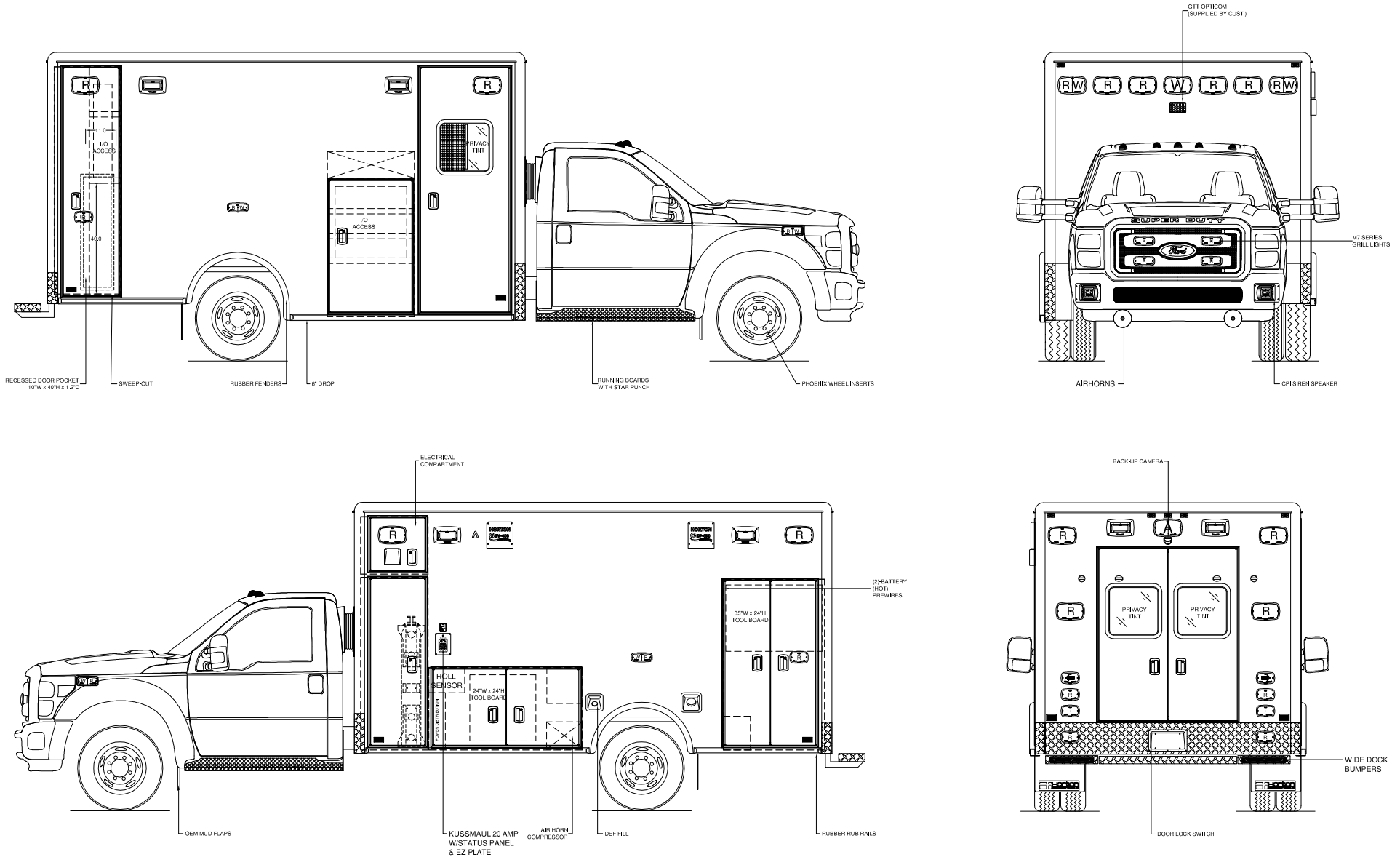
MISCELLANEOUS DEALER-SUPPLIED EQUIPMENT/SERVICES:

The following services and/or items shall be furnished and/or installed by Foster Coach Sales Inc.

- 1) (1) ADDITIONAL REMOTE KEY FOB - TOTAL OF (3). REQUIRES FORD DEALER PROGRAMMING.
- 2) WEATHER-TECH ALL-WEATHER FLOOR MATS FOR DRIVER AND PASSENGER'S SIDE.
***CUSTOMER NOTE: WEATHER TECH MATS ARE NOT DESIGNED NOR RECOMMENDED FOR USE ON VINYL FLOORS. CHASSIS TO BE ORDERED WITH CARPETED FLOORS.
- 3) INSTALL A CUSTOMER-SUPPLIED CAD MOUNT CONNECTED TO A FUSED 12-VOLT BATTERY HOT POWER SOURCE - INCLUDES HORTON PRE-WIRE.
- 4) INSTALL GLOVE BOX STORAGE IN PASSENGER SIDE CAB DOOR ONLY.
***CUSTOMER NOTE: DUE TO SPACE LIMITATIONS, NOT ALL GLOVE BOXES WILL FIT IN THE SPACE AVAILABLE IN THE DOOR PANEL.
- 5) (3) CORIAN COUNTERTOPS AND MONITOR SPACER.
- 6) (2) ZICO QR-D-2 CYLINDER BRACKETS INSTALLED BENEATH BENCH - BASE OF BRACKET TO BE TOWARD HEAD OF BENCH. EXACT LOCATION T.B.D.
- 7) (1) NCE H9000 MONITOR BRACKET FOR ZOLL E-SERIES MONITOR WITH CORIAN SPACER ON REAR PORTION OF INHALATION COUNTERTOP.
- 8) INSTALL CUSTOMER-SUPPLIED RADIOS AND KNOX SENTRALOCK INTEGRATED INTO MAIN VHF RADIO. RADIO TRANSCEIVERS TO BE LOCATED BEHIND DRIVER'S SEAT.
- 9) INSTALL CUSTOMER-SUPPLIED CRADLEPOINT DOME ANTENNA AND RELATED EQUIPMENT. BASE UNIT TO BE LOCATED IN ELECTRONICS CABINET. WIRE FOR OPERATION. FURTHER DETAILS MUST BE PROVIDED.
- 10) (2) FLOWTEC DIAL TYPE FLOWMETERS WITH NCG ADAPTERS.
- 11) INSTALL MISCELLANEOUS CUSTOMER-SUPPLIED EQUIPMENT NOT ALREADY SPECIFIED:
 - (2) PORTABLE RADIO CHARGERS IN FRONT CONSOLE - BATTERY HOT.
 - THERMOMETER - MOUNT ONLY, NO POWER.
 - 5-GAS METER MONITOR/CHARGER - LOCATION/POWER T.B.D.
 - HANDLIGHT CHARGER/BASE - STREETSIDE REAR. INCLUDES HORTON PRE-WIRE.
 - (3) SCBA BRACKETS - STREETSIDE REAR.
 - AX & HALIGAN BAR - STREETSIDE REAR. MOUNTS TO BE SPECIFIED.
 - THERMAL IMAGING CAMERA MOUNT - LOCATION/POWER T.B.D.
 - KNOX MABAS 3200 KEY BOX - FRONT CONSOLE. NO POWER REQUIRED.
- 12) STRIPING AND CHEVRONS PER SPECIFICATION.
DOES NOT INCLUDE LETTERING - LETTERING NOT REQUESTED IN RFP
- 13) (2) FACTORY INSPECTION TRIPS FOR (2) DEPARTMENT PERSONNEL. ONE TRIP TO BE UPON COMPLETION. THE OTHER TRIP INTERVAL IS T.B.D.
PRICED SEPARATELY ON QUOTATION DOCUMENT
- 14) PROVIDE EQUIPMENT AND COMPLETE INSTALLATION OF A FIRECOM 5200D INTERCOM SYSTEM PER FIRECOM QUOTE. HORTON TO PRE-WIRE HEADSET JACK LOCATIONS.
- 15) PHOENIX REAR VALVE STEM EXTENSION KIT.
- 16) DISABLE AUTOMATIC DOOR LOCK FUNCTION.

PLEASANT PRAIRIE FIRE & RESCUE DEPT

623 PASS THROUGH FORD CHASSIS



NOTE: DRAWINGS ARE FOR GRAPHICAL REPRESENTATION ONLY
DIMENSIONS ARE APPROXIMATE & MAY VARY DURING CONSTRUCTION

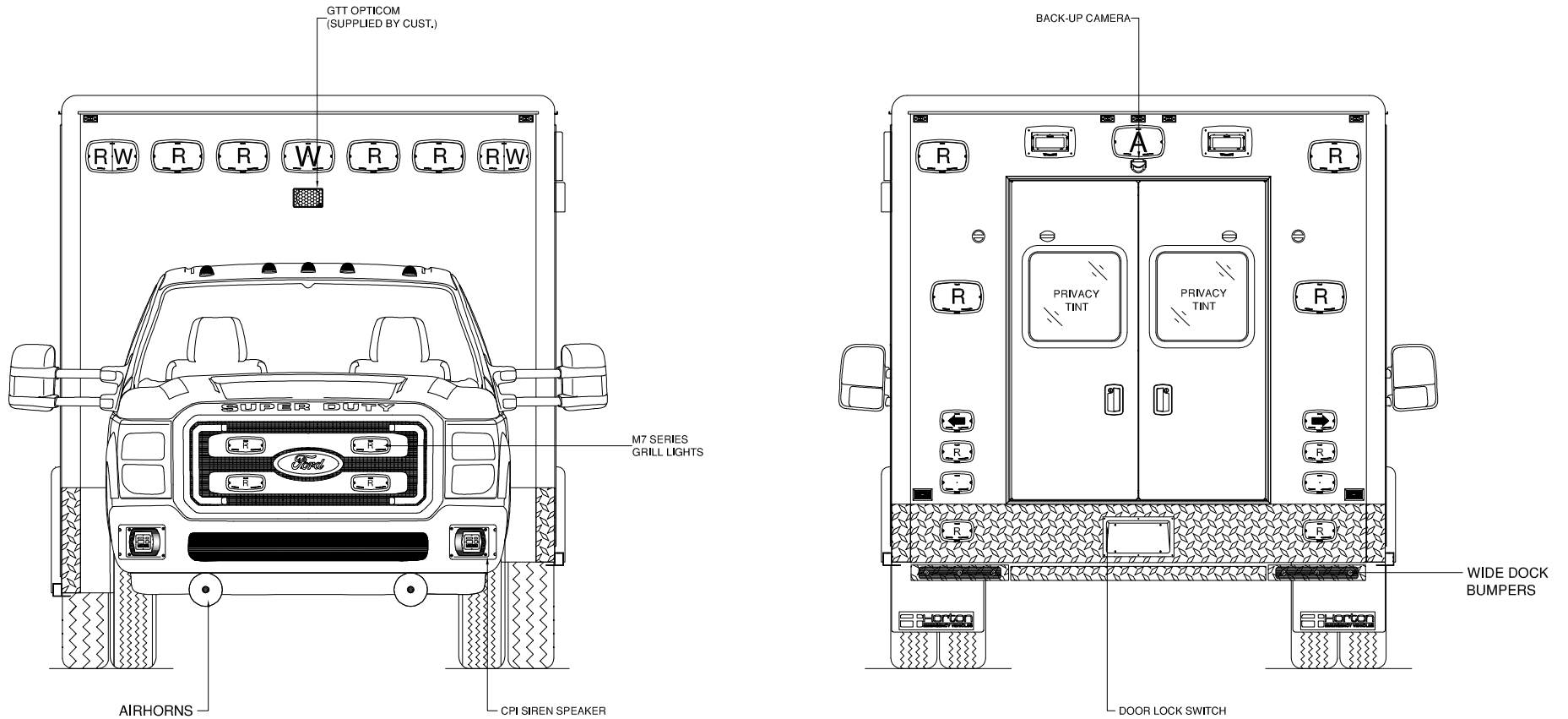
1505-112

PLEASANT PRAIRIE FIRE & RESCUE DEPT



DRAWN BY: MJL 5.27.15

623 PASS THROUGH FORD CHASSIS



NOTE: DRAWINGS ARE FOR GRAPHICAL REPRESENTATION ONLY
DIMENSIONS ARE APPROXIMATE & MAY VARY DURING CONSTRUCTION

PROPOSAL DRAWING

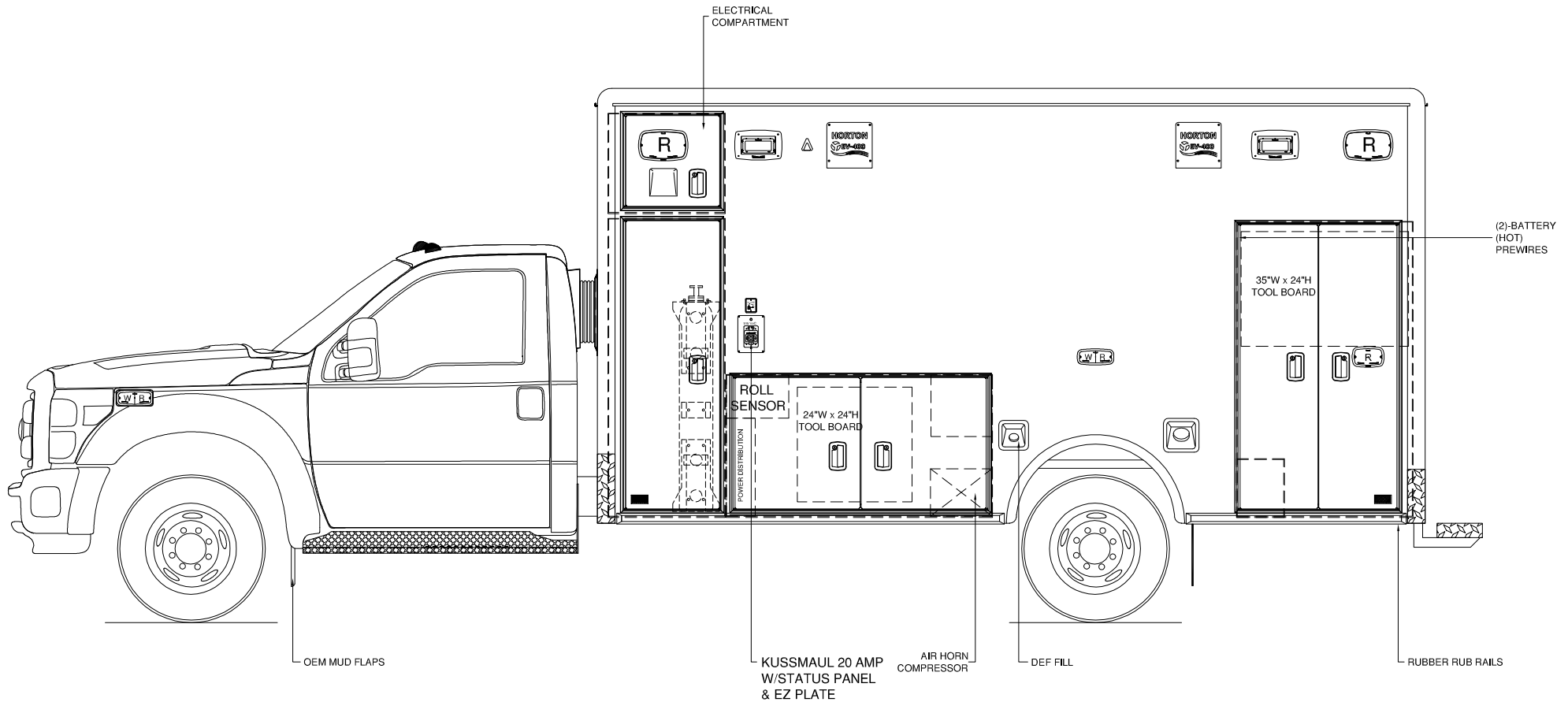
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PLEASANT PRAIRIE FIRE & RESCUE DEPT



DRAWN BY: MJL 5.27.15

623 PASS THROUGH FORD CHASSIS



COMP.	INSIDE WIDTH	INSIDE HEIGHT	INSIDE DEPTH	CLEAR OPENING WIDTH	CLEAR OPENING HEIGHT
SS FWD	24.25	62.13	20.36	18.74	59.16
SS INT	55.50	29.50	20.36	51.80	26.37
SS REAR	36.63	61.50	20.36	32.09	58.37

NOTE: DRAWINGS ARE FOR GRAPHICAL REPRESENTATION ONLY
DIMENSIONS ARE APPROXIMATE & MAY VARY DURING CONSTRUCTION

PROPOSAL DRAWING

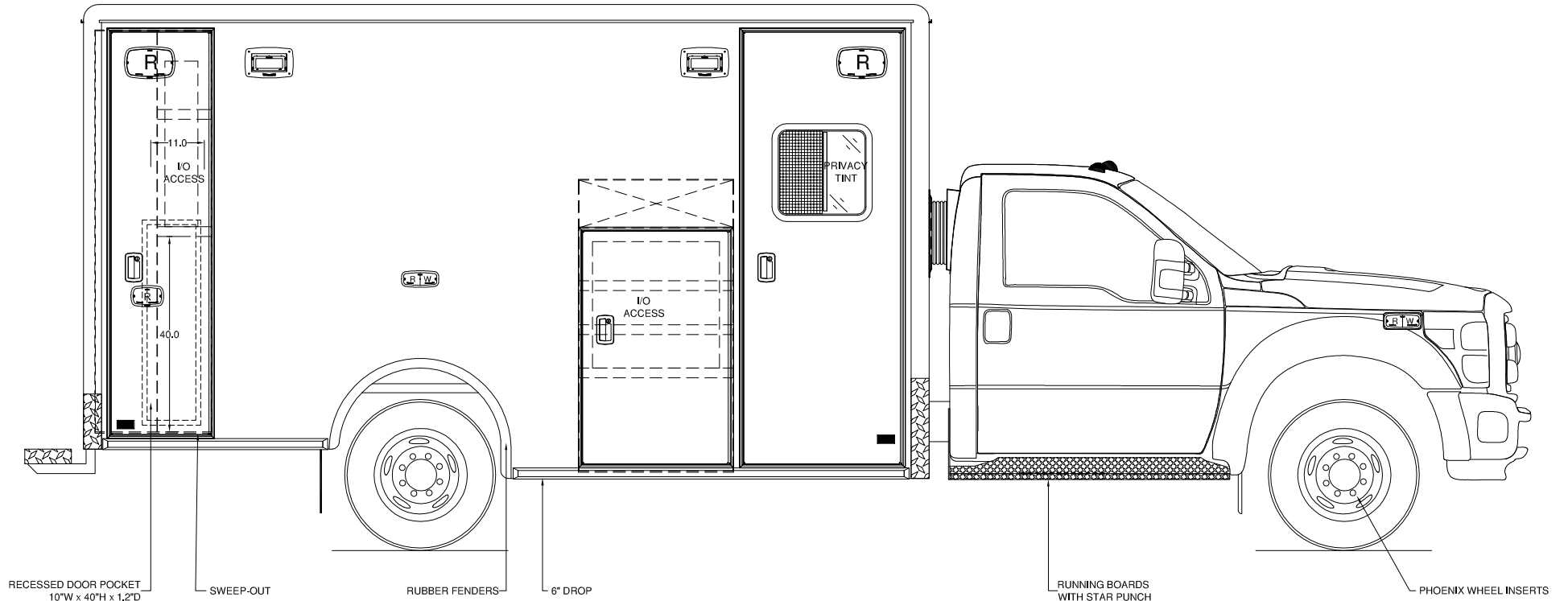
1505-112

PLEASANT PRAIRIE FIRE & RESCUE DEPT



DRAWN BY: MJL 5.27.15

623 PASS THROUGH FORD CHASSIS



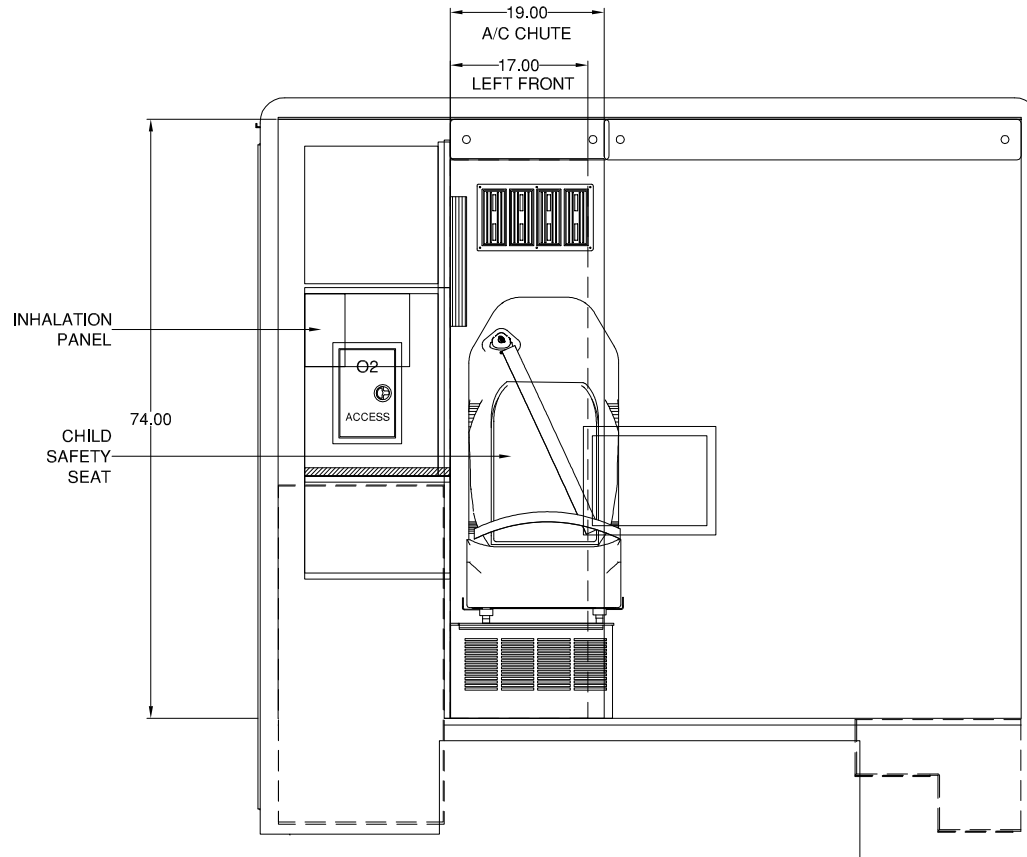
COMPT	INSIDE WIDTH	INSIDE HEIGHT	INSIDE DEPTH	CLEAR OPENING WIDTH	CLEAR OPENING HEIGHT
CS REAR	23.70	82.13	20.36	19.09	81.16
CS FWD	31.50	59.56	20.36	28.87	46.44

**NOTE: DRAWINGS ARE FOR GRAPHICAL REPRESENTATION ONLY
DIMENSIONS ARE APPROXIMATE & MAY VARY DURING CONSTRUCTION**

PROPOSAL DRAWING

1505-112

623 PASS THROUGH FORD CHASSIS



NOTE: DRAWINGS ARE FOR GRAPHICAL REPRESENTATION ONLY
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PROPOSAL DRAWING

1505-112

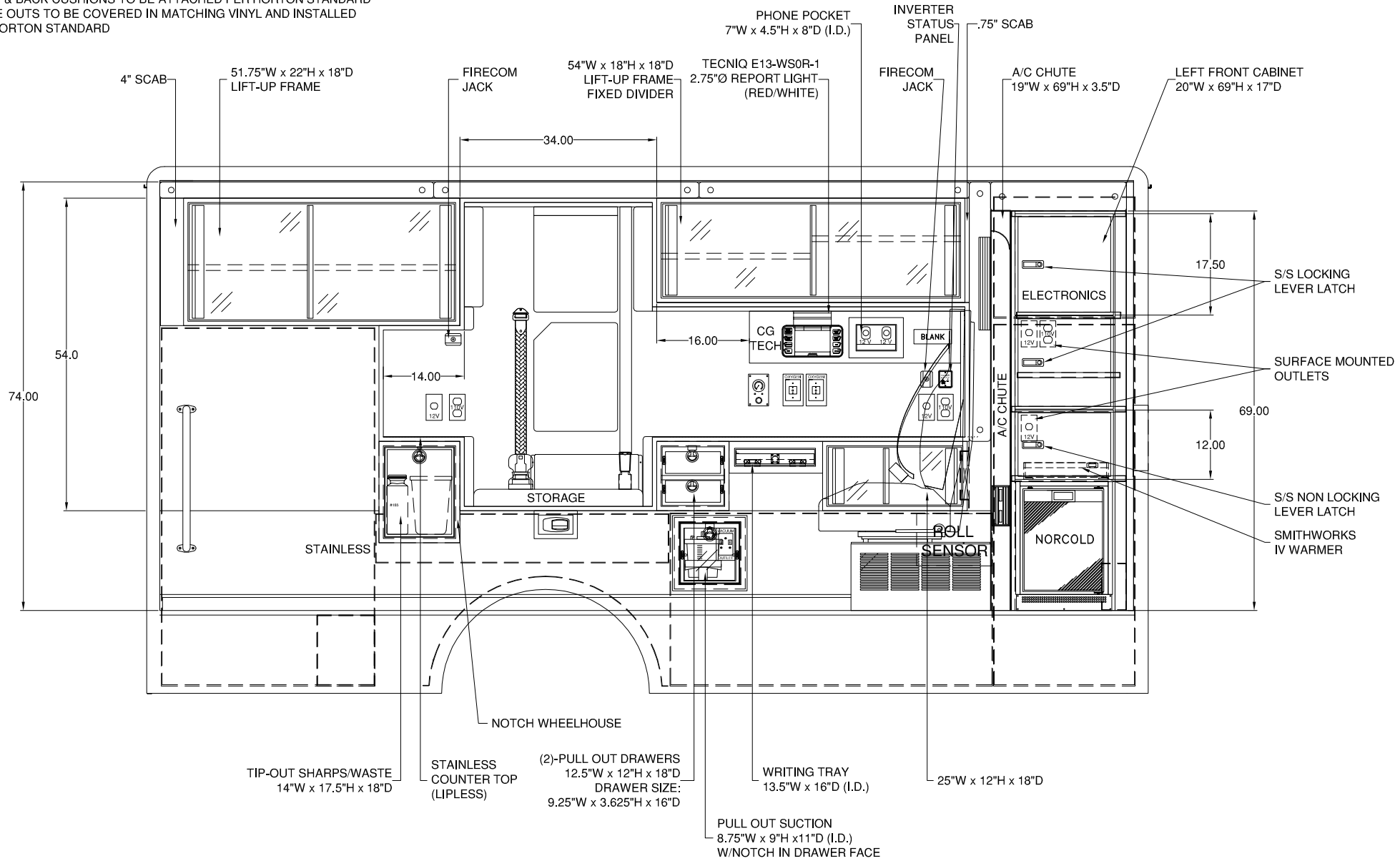
PLEASANT PRAIRIE FIRE & RESCUE DEPT



DRAWN BY: MJL 5.27.15

623 PASS THROUGH FORD CHASSIS

ALL SEAT, HEAD & BACK CUSHIONS TO BE EVS VACUUM FORMED
 SEAT CUSHIONS TO BE ATTACHED USING VELCRO
 HEAD & BACK CUSHIONS TO BE ATTACHED PER HORTON STANDARD
 CLOSE OUTS TO BE COVERED IN MATCHING VINYL AND INSTALLED PER HORTON STANDARD



NOTE: DRAWINGS ARE FOR GRAPHICAL REPRESENTATION ONLY
 DIMENSIONS ARE APPROXIMATE & MAY VARY DURING CONSTRUCTION

PROPOSAL DRAWING

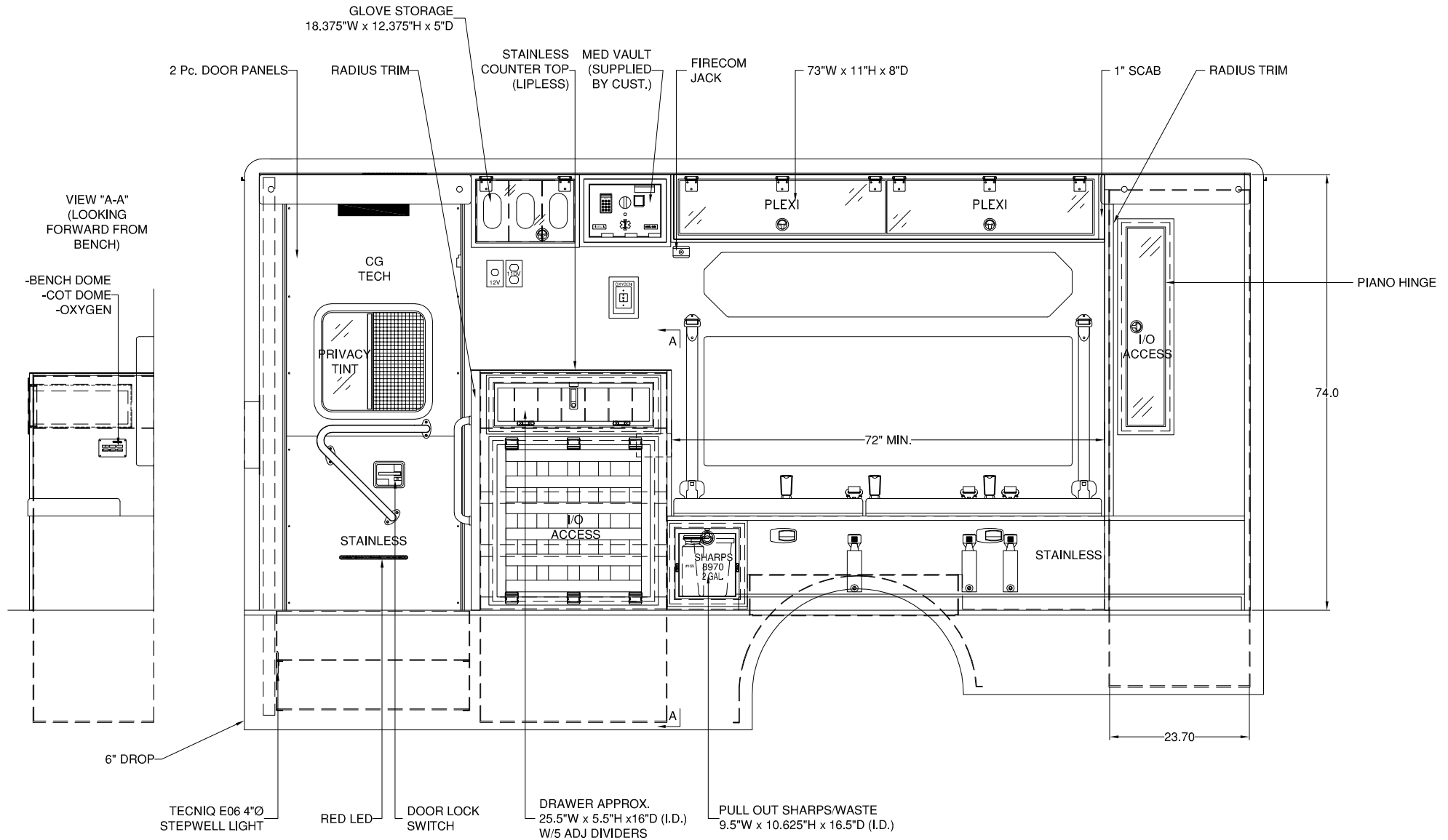
1505-112

PLEASANT PRAIRIE FIRE & RESCUE DEPT



DRAWN BY: MJL 5.27.15

623 PASS THROUGH FORD CHASSIS



NOTE: DRAWINGS ARE FOR GRAPHICAL REPRESENTATION ONLY
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PROPOSAL DRAWING

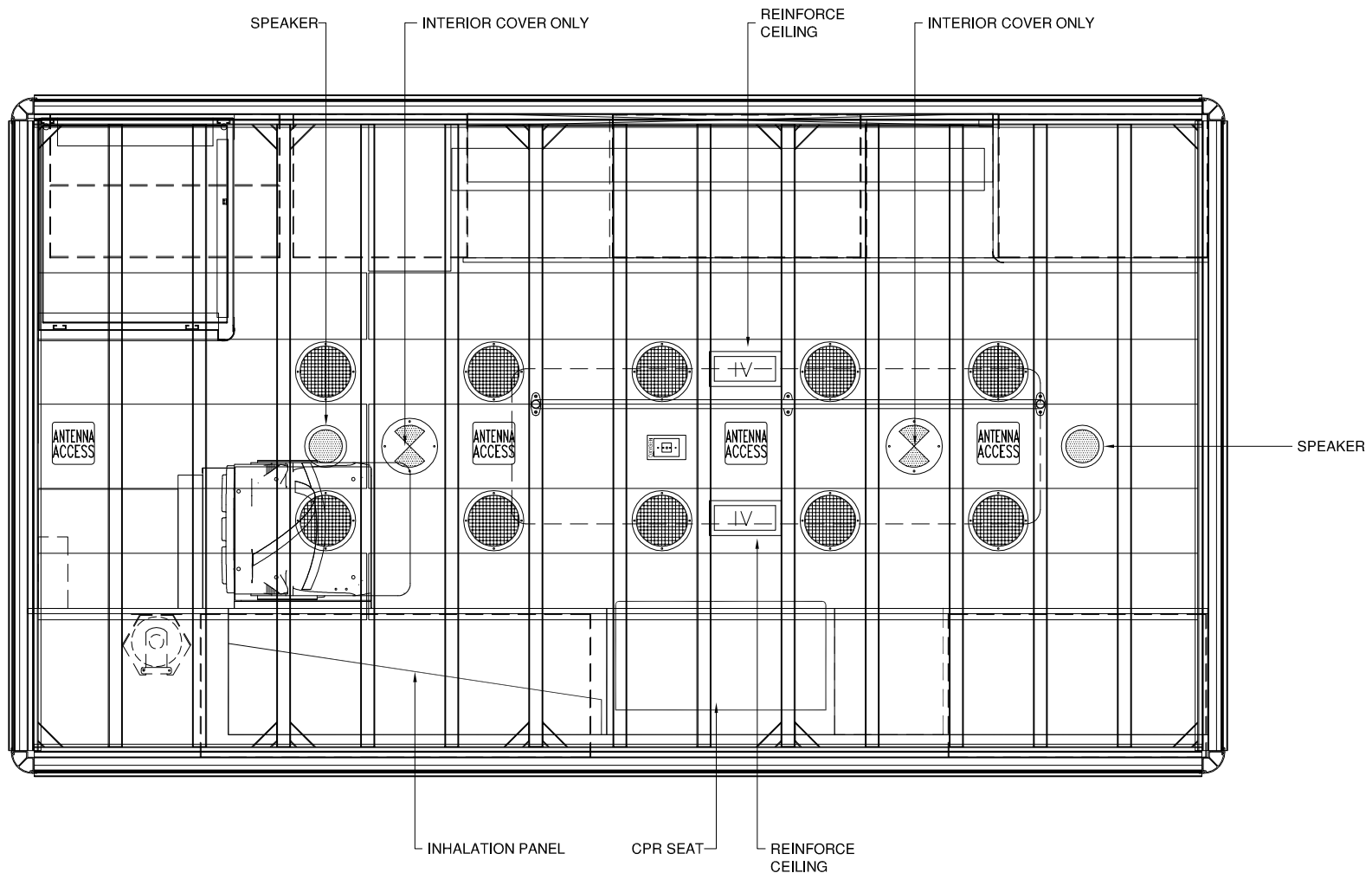
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PLEASANT PRAIRIE FIRE & RESCUE DEPT



DRAWN BY: MJL 5.27.15

623 PASS THROUGH FORD CHASSIS

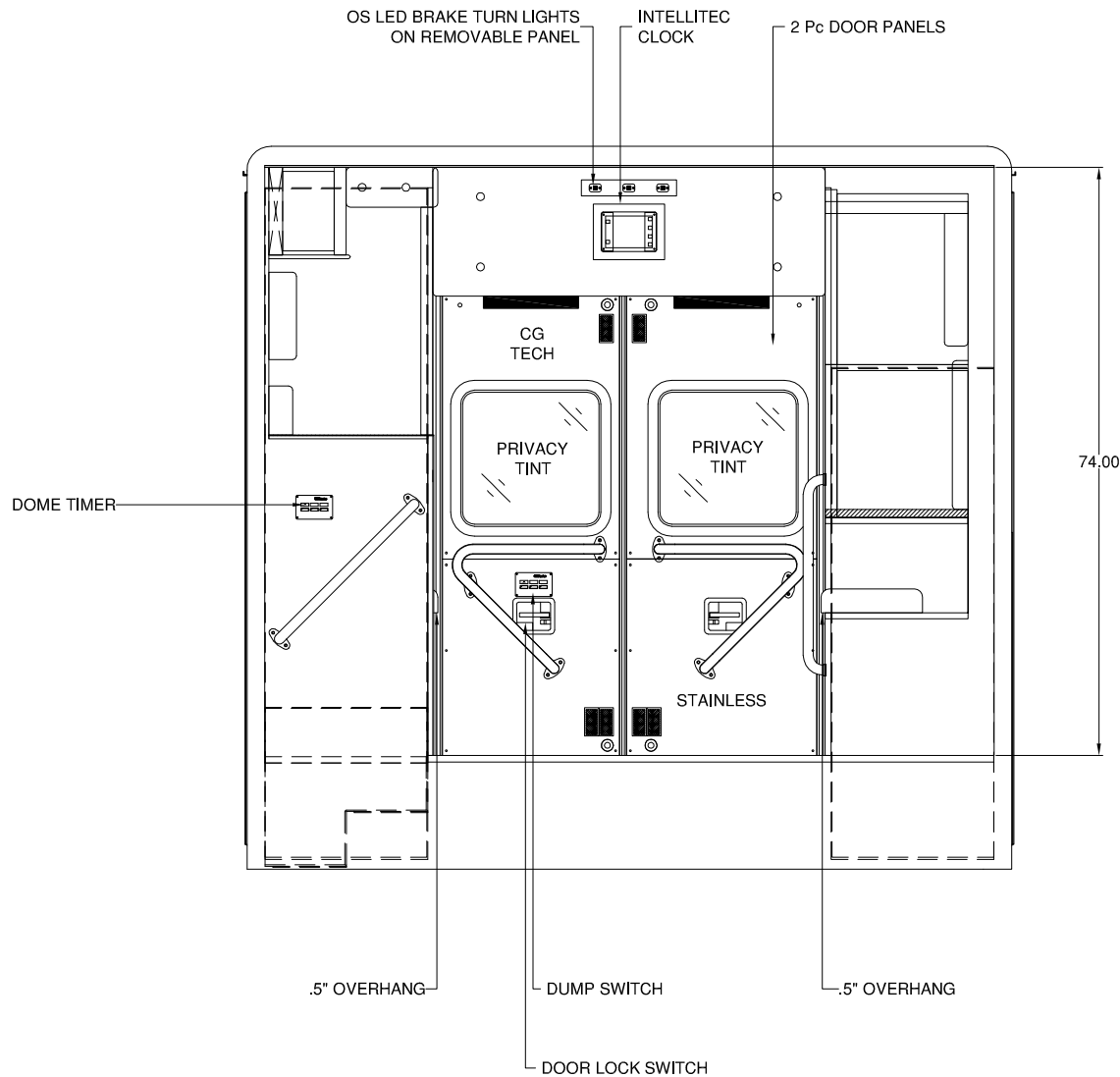


NOTE: DRAWINGS ARE FOR GRAPHICAL REPRESENTATION ONLY
DIMENSIONS ARE APPROXIMATE & MAY VARY DURING CONSTRUCTION

PROPOSAL DRAWING

1505-112

623 PASS THROUGH FORD CHASSIS



NOTE: DRAWINGS ARE FOR GRAPHICAL REPRESENTATION ONLY
DIMENSIONS ARE APPROXIMATE & MAY VARY DURING CONSTRUCTION

PROPOSAL DRAWING

1505-112



May 22, 2015

Re: Village of Pleasant Prairie Fire & Rescue Department
Request for New Ambulance Proposal – Explanation of Exceptions/Clarifications

The following information has been prepared to define any exceptions and/or clarifications required of this vendor pursuant to the Request for Proposal distributed by the Village of Pleasant Prairie Fire & Rescue Department.

General Statement – A Horton Model F623 (173") all-aluminum modular body with 74" headroom and a pass-through configuration has been quoted. The proposed F623 unit will be produced by the Horton Emergency Vehicles Company (HEVC), whose primary production facility is located at 3800 McDowell Road in Grove City, OH 43123. The HEVC has been producing custom designed ambulances since its inception in 1968, with current production output at approximately (500) units annually.

1. **Page 4 – Chassis, Item J:**

The skid plate option is NOT available on regular cab models. This option is limited to Super Cab and Crew Cab models only.

2. **Page 4 – Chassis, Item K:**

Per our Ford fleet contact, there is no published information available which identifies the rating of the (2) frame mounted tow hooks.

3. **Page 4 – Chassis, Item L:**

Ford Qualified Vehicle Modifier (QVM) guidelines do not allow tampering with the OEM exhaust tip. While some adapters are available through 3rd party sources, they do present potential warranty issues. As such, Foster Coach will not modify the OEM exhaust system. The OEM exhaust tip will be nearly flush with the body rub rails.

4. **Page 4 – Chassis Modifications, Hardware & Accessories, Item F:**

WeatherTech states their floor mats are NOT designed nor recommended for use on models with vinyl floors. As such, the chassis will be ordered with carpeted flooring. Should the WeatherTech mats not be desired, the chassis can be ordered with the carpet delete option, which provides vinyl flooring.

5. **Page 5 – Chassis Modifications, Hardware & Accessories, Item K:**
OEM GPS options are only available with the Lariat trim level. The SYNC system available with the XLT trim level does not provide navigational services. GPS specifically designed for emergency vehicle use is available through the Horton i4g electrical system. This system is available for \$700.00.
6. **Page 5 – Chassis Modifications, Hardware & Accessories, Item L:**
The red dome lights provided above the cab seats shall have replaceable bulbs ILO the requested LEDs. LED lights are available, but they do not include integral switches and would have to be switched through the control panel. These lighting options can be further evaluated at a pre-construction conference.
7. **Page 6 – Interior Wall & Cabinets, Streetside:**
The upper rear main wall cabinet shall be approximately 51.75"W x 22"H per the main wall drawing. Additional configurations can be provided if so desired.
8. **Page 6 – Interior Wall & Cabinets, Streetside:**
The inhalation area and the adjacent cabinetry shall be approximately 54"W per drawing.
9. **Page 7 – Action Area/Inhalation Panel:**
Horton has discontinued the use of Avonite products. The solid-surface countertop material proposed shall be manufactured by LG.
10. **Page 8 – Interior Accessories & Trim:**
The CPR seat area shall be approximately 34" wide as noted in the main wall drawing. Note: this is the dimension between the actual cabinetry (not accounting for the width of the cushions).
11. **Page 9 – Exterior Compartments, Streetside:**
Due to the requested electrical accessories compartment above the streetside forward compartment, the oxygen compartment shall be approximately 62"H. The standard 24.25" width of this compartment shall be maintained.
12. **Page 9 – Exterior Compartments, Streetside:**
Due to the interior cabinetry requested below the inhalation area countertop, the streetside intermediate compartment must be lowered 12" to 29"H.
13. **Page 10 – Exterior Compartment, Curbside:**
In order to provide a 72" squad bench area in conjunction with the proposed width of the crash barrier compartment, the width of this compartment shall be reduced to approximately 23.7". This configuration can be further evaluated at a pre-build conference.

14. Page 12 – Module Scene Lighting:

The requested Whelen Slim-Line flood light(s) is a semi-recessed light. It is not available as a surface mount light. The lights shall have a black housing, but are only available with a chrome flange.

15. Page 12 – Emergency Lighting General Comments:

- Clarification: Further description of the term "Emergency Mode 3" shall be obtained at a pre-build conference. Multiple programming options exist that can be tailored to the department's needs.
- Flash pattern adjustability shall be integrated into the front control panel as standard. Additional auxiliary switches shall not be required.

16. Page 13 – Antenna Coax Locations:

The antenna mounts shall be a K-94 antenna base in lieu of the requested NMO mount. Horton switched to K-94 mounts as a replacement of the NMO mounts for increased reliability. This should not affect the mounting of any antenna that previously fit an NMO mount.

17. Page 14 – Firecom System:

Due to the location of the attendant seat HOPS air bag, a Firecom headset jack cannot be installed above and behind the primary attendant seat as requested. The attendant seat area headset jack is currently shown on the inhalation area wall, pending further review at a pre-build conference.

18. Page 15 – Module HVAC Operation:

The module's heat/air conditioning system shall operate on 12-volts (chassis power) only. It shall not operate off the vehicle's shoreline.

19. Page 17 – Paint & Striping:

- The chevron striping shall be outlined with a .500" non-reflective pinstripe to seal the exposed edges of the Diamond Grade material. This material has been found to provide better adhesion to the chevron material, thereby increasing its overall effectiveness in sealing the edges.
- The use of the 3M #8115 seam sealer has been discontinued by Horton and will not be provided.
- This proposal includes the requested Chevron and beltline striping only. NO customer specific lettering is included with this proposal.
- All details of the striping shall be further reviewed at a pre-build conference.

20. Page 22 – Item #17:

Information and pricing regarding extended warranty options for the chassis have been provided (under Tab 3). No extended warranty options currently exist for the module and its related components.

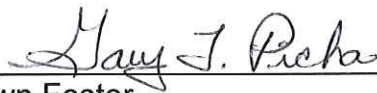
Closing Statement:

It is the intent of Foster Coach Sales Inc. and the Horton Emergency Vehicles Company to provide the Village of Pleasant Prairie Fire & Rescue Department with a vehicle that will fulfill its needs for many years to come. Foster Coach would welcome the opportunity to review this bid response with the staff of the Pleasant Prairie Fire & Rescue Department before a purchase decision is made.

Should it be determined that Foster Coach has overlooked a desired feature/option or perhaps included something that was not desired, please notify me at your earliest convenience. I can be reached at (800) 369-4215, extension #5 or via e-mail at shawn@fostercoach.com should you have any questions or concerns related to the information contained within this proposal.

Thank you for your consideration.

Respectfully Submitted;

pp: 
Shawn Foster
Regional Sales Manager

Item#	Bid Page/Reference	Option	Question/Review	Unit Cost	QTY	Sub-total
1	14 of 37	GK140000	Will 4-point restraint be available, spec asked for any greater restraint to be quoted	n/a		
2	20 of 37	ISP00001	Addition of glove location at rear either side of Intellitec clock ?	\$205	1	\$205
3	15 of 37	HK001000	Explain HOPS and credit deletion....	n/a		
4	20 of 37	KG02B000	Addition of IV hangers (2) aft of cot.	\$94	2	\$188
5	27 of 37	PAL30A00	Add 110v outlet at inhalation panel behind where Zoll monitor will be located.	\$107	1	\$107
6	27 of 37	PAL30A00	110v outlet drawing/quote mis-match, quote missing cabinet 110v shown on drawing.	\$107	1	\$107
7	22 of 37		Add wig/wag headlight	\$143	1	\$143
8	27 of 37		12v outlet drawing/quote mis-match, quote missing cabinet 12v shown on drawing.	\$70	1	\$70
9	27 of 37		12v in area of IV warmer. Is this needed for Warmer?	n/a		
10	29 of 37	PL11G000	Park Brake warning, is this typical ?	n/a		
12	36 of 37	Z15 #4	Glove note about size may not fit - clarify	n/a		
14	24 of 37		Air Horn Activation – Horn Ring select from Horn/Siren/Air Horn ?	n/a		
15	23 of 37	mm09r000	LED Light Front Layout: RW/R/R/W/R/R/RW. Can outboard RW match WR or WR – RW – I can explain at later time	n/a		
16	11 of 37		Paint: I know there is no lettering defined. What of side strip?	n/a		
			Additions post-quote			\$820
			Original Quote			\$195,682
			Adjusted proposal cost			\$196,502



TYPE I

*Dependable support.
Excellent ride quality.*

403 603
453 623
457

Widely recognized as the “workhorse” of the Fire/EMS world, Horton’s Type I ambulance offers all the added benefits of a truck chassis. Applicable for both large city fleets and small rural areas, Type I vehicles provide dependable support and optional rugged four-wheel drive power.



FORD



RAM

Horton Type I ambulances come standard with all of the features you know and trust to be part of the safest and highest-quality ambulance on the market:

- Superior construction, able to withstand a 90,000 pound load test
- More than 40 years of dynamic crash testing
- VI-Tech mounting to isolate vibration and noise
- Eight layers of insulation and sound suppression in the floor alone
- The most capable electrical system in the industry, Intelliplex i4G™
- HOPS for rollover protection
- Lowest cost of ownership*

Model	Chassis	Length	Width	Headroom
403	Ford Chevy Gas*	138"	96"	68"
453	Ford	146"	96"	72"
457	Ford RAM	157"	96"	72"
603**	Ford RAM	167"	96"	72"
623**	Ford RAM	173"	96"	72"

* Pending payload requirements

** Pending payload requirements, the 603 and 623 may also be mounted on an International 4300, TerraStar, Freightliner or Kenworth chassis

*factoring in purchase price, maintenance costs, operating costs, uptime, life expectancy and resale value



hortonambulance.com

info@hortonambulance.com

800.282.5113



DEPARTMENTS WITH HARSH WINTERS OR OFF-ROAD CALLS CAN CHOOSE A TYPE I AMBULANCE WITH FOUR-WHEEL DRIVE.

OPTIONAL CRASH BARRIER CONFIGURATION PROVIDES A TRUE SAFETY BARRIER AT THE HEAD OF THE SQUAD BENCH.



WITH A VARIETY OF LAYOUT OPTIONS TO SUIT ANY DEPARTMENT NEEDS, TYPE I IS THE MOST VERSATILE OF AMBULANCE OFFERINGS.

HOPS



Horton **O**ccupant **P**rotection **S**ystem
for rollover collisions

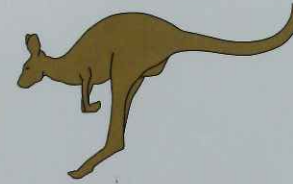


 **Horton**
EMERGENCY VEHICLES

Safety With Substance 

Introducing

HOPS

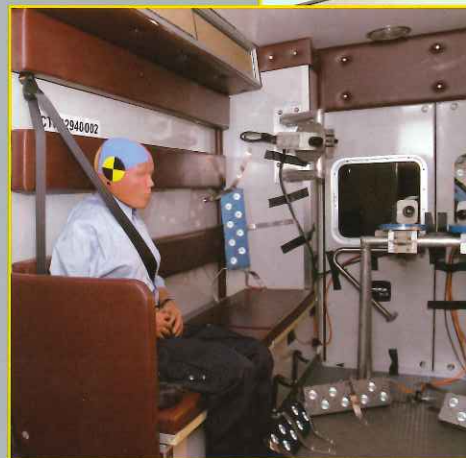


Horton Occupant Protection System

An emergency medical vehicle responding to a call presents a unique paradox. Medical personnel rush to help someone in need, but at the same time put their own lives at risk. Danger lurks at every intersection. Ambulance accidents are commonplace and deaths occur at a higher rate than in other professions. According to the National Highway Traffic Safety Administration, rollover accidents are the most violent of all types of motor vehicle crashes typically causing over 10,000 fatalities and 24,000 injuries per year. Head trauma is the most frequent type of fatal and non-fatal injury. That figure represents about 32 percent of all occupant fatalities. According to the NHTSA Crash Analysis Center, as a class, rollover injuries constitute one third of all vehicle accident injury costs.

Long known for its leadership in safety and testing in the ambulance industry, Horton Emergency Vehicles has invested years of research to develop a new advanced occupant protection system for these severe rollover crashes. Horton's Occupant Protection System (HOPS) has combined advanced research on airbag protection, head strike dissipation and occupant restraint devices into today's ambulance interior. As always, Horton has performed extensive, fully documented research to verify the efficacy of every step. And even more important, the new HOPS system is now standard equipment on every Horton Emergency Vehicle.

The Horton Occupant Protection System (HOPS) is a fully-tested system that combines advanced restraints, multi-density head protection, tubular airbags and head curtain airbags to protect attendants in a side impact rollover collision.



Like any collision protection system, HOPS is effective only when occupants are wearing seat belts. CPR, head and aft bench seat belts have a detachable third point belt to enhance mobility.





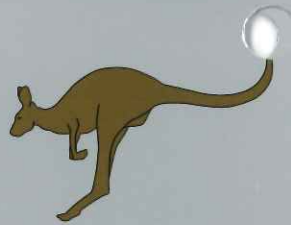
Two types of airbags are deployed in a side impact rollover collision: the Inflatable Head Curtain protects the attendant from the inhalation area cabinet and (optional) the attendant at each end of the bench seat. The Tubular Structure airbag is used with the attendant and the CPR seats for additional head protection.



The dynamic rollover test machine enables test engineers to simulate a rollover event while recording significant test data from inside the vehicle. The Hybrid III test dummies are fully instrumented to check head strike impacts, G loads, neck loading and seat belt forces. Comparisons are made to evaluate base line data against data collected with new experimental systems and to ensure the efficacy of the solution. The Horton body was subjected to 14 rollover impacts and retained its structural integrity through it all.



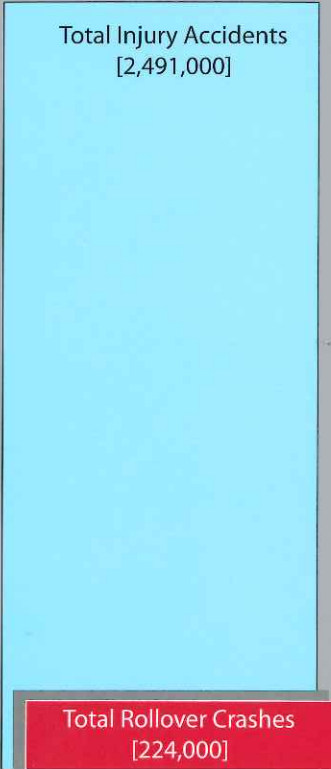
Why HOPS



The ambulance industry has long recognized the exposure to danger faced by vehicle occupants. NHTSA, NIOSH, GSA and other agencies are actively involved in investigating the problem of occupant safety in ambulances, as are various special interest groups. While nearly everyone is willing to acknowledge the problem, few have offered concrete solutions. The difficulty lies in the very nature of the ambulance patient compartment. It is a large unrestricted space around which attendants must move to perform their emergency functions. It is filled with hard cabinets and other obstacles and seating positions vary widely.

Rollover crashes and severe head injury potentials demand immediate solutions. But products presented to date are often merely “Band-Aid” approaches to the problem. Even worse, there is little or no testing undertaken to verify the efficacy of such offerings. By leading end users to a false sense of security with unverified solutions, manufacturers and vendors often provide a greater disservice than if they had done nothing at all.

As the industry leader, Horton Emergency Vehicles committed its resources to the development of substantive solutions backed by thorough and exhaustive testing programs.

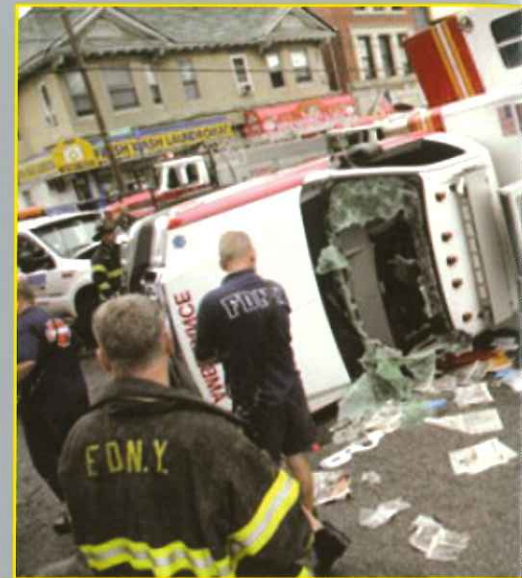


source: NHTSA, Traffic Safety Facts 2007, DOT HS 810 993

While total rollover crashes make up a low percentage of injury accidents, the severity of injury is much higher. Year after year fatal rollovers account for 20 to 25 percent of all fatal crashes.

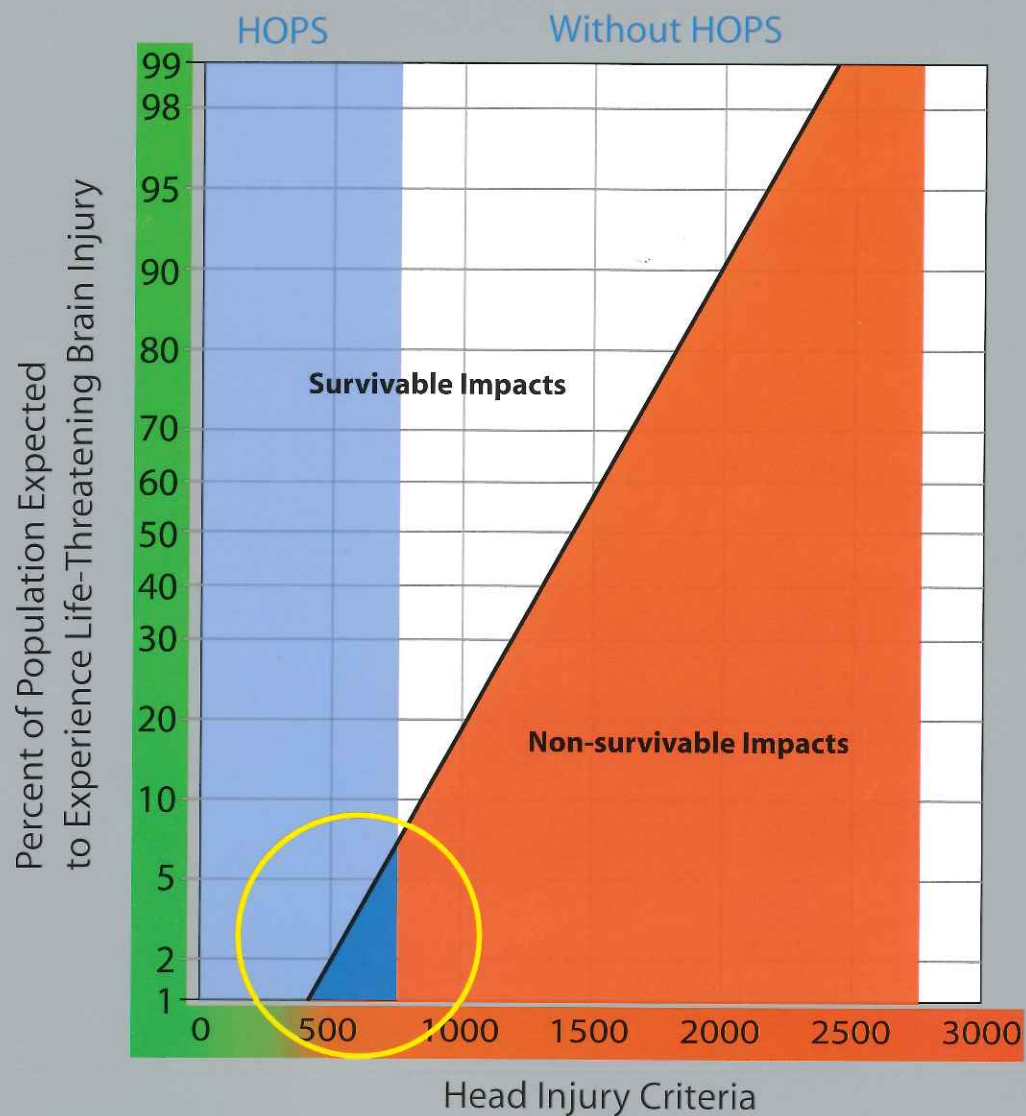


More than 30 years of impact testing did more than create a base-line for HOPS development. Horton testing validated the structural integrity of the body, as well as the patient area. This validation is unmatched by any other ambulance manufacturer.





Violent head strikes against hard surface cabinets represent one of the most significant dangers within the ambulance interior. Horton's baseline testing, as shown in red in the chart below, indicates these forces can be non-survivable in nearly 100 percent of the impacts. These same tests run with HOPS installed show the increased occupant protection reduces the fatality risk from head strikes by 95 percent. The yellow circle on the chart illustrates this reduction of risk for all but the most vulnerable 5 percent of the population who may be infirm, suffer from prior injury or who may be extremely feeble due to advanced age.



H. Mertz, "Injury Assessment Values Used to Evaluate Hybrid III Response Measurement", Hybrid III: The First Human-Like Crash Test Dummy, SAE PT-44, 1994

What's behind HOPS

Horton Emergency Vehicles has employed extensive dynamic impact rollover testing, Hyge Sled testing, direct impact crash testing, L-DYNA-MADYMO computer simulation models, cannon impact tests for head strike and restraint verifications with state of the art Hybrid III fully instrumented test dummies.

Tests were conducted to verify body to chassis mounting integrity, cabinet retention, seating stability, contact surfaces, and compliance to ECE and SAE cabin integrity requirements. For the first time, real solutions are being offered with the substance of solid engineering and verification testing behind a comprehensive safety system for ambulance rollover accidents.

In a side impact rollover collision, both computer modeling and high speed film show how occupants rise out of their seats changing head strike areas.



In addition to the inflatable cushion system, the HOPS system includes Horton's new progressive resistance headrests at all squad bench and CPR seating positions. While traditional foam padding feels quite soft, tests show that severe headstrikes are transmitted completely through the foam padding, literally "bottoming out" on the mounting surface and offering very little protection.

The progressive resistance product dissipates the energy throughout a laminated protective surface to eliminate the bottoming effect and offer additional protection much like that provided by high impact sports and racing helmets. Numerous combinations and materials were evaluated using high tech cannon fires at the test laboratory.



A head-strike cannon was used to test the HOPS cushion system.



Horton's detachable three-point harness system is barrier seat bolsters to confine the occupant in fair detachable feature on the over the shoulder portion of the squad bench and the CPR seat permits the EMT to the patient without removing his seat belt.

Seat belts are subjected to extensive safety and durability testing.





used in conjunction with open spaces. The of the harnesses on the move forward to access



European and SAE standards dictate a series of tests to address survivable space around its occupants. Reasoning that if a cab occupant is entitled to certain crash protection, then certainly someone in an ambulance patient compartment should enjoy the same protection, Horton applied these same tests.



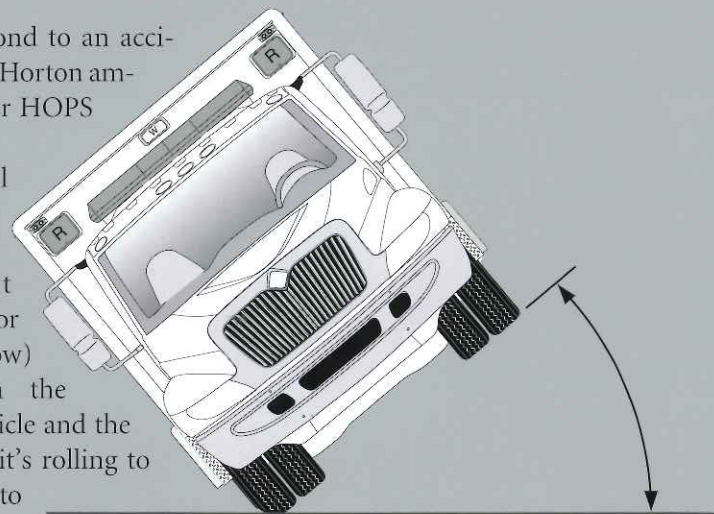
Far exceeding any structural requirements of the KKK specifications, this testing marked the first time in history such survivability testing was done on an ambulance body and clearly demonstrated the structural integrity of the Horton body. The tests were run with all cabinetry in place to demonstrate they would not detach and injure the occupants.



What you'll find in a HOPS equipped ambulance

If you respond to an accident involving a Horton ambulance, look for HOPS warning labels.

HOPS will not deploy during a frontal or side-impact crash. A roll sensor (pictured below) calculates both the angle of the vehicle and the speed at which it's rolling to determine when to fire the airbag restraints.



If occupants require emergency extrication after a frontal or side-impact crash, special caution must be taken by rescue personnel, as HOPS may still be active and ready to deploy.

The airbags are filled with a pressurized stored gas cylinder with a small quantity of a solid pyrotechnic fuel. When fired, the gas is warmed slightly and expands to fill the head curtain or tube airbag.



⚠ WARNING
 PRESSURIZED GAS AND/OR PYROTECHNICS contained behind this panel or in cylinder. Release of gas can cause serious injury.

- NEVER service, salvage or reuse.
- NEVER weld, apply heat, grind, puncture or drill.
- Contact manufacturer for disposal instructions.

15328

⚠ WARNING
 TO AVOID SERIOUS INJURY:

- Do not sit or lean unnecessarily close to the airbag.
- Do not place objects over the airbag or between the airbag and yourself.
- For maximum safety protection, you must always wear your seat belt.

AIRBAG 15614

DO NOT TAMPER

We didn't get here overnight

How does an ambulance company create the world's safest ambulance?

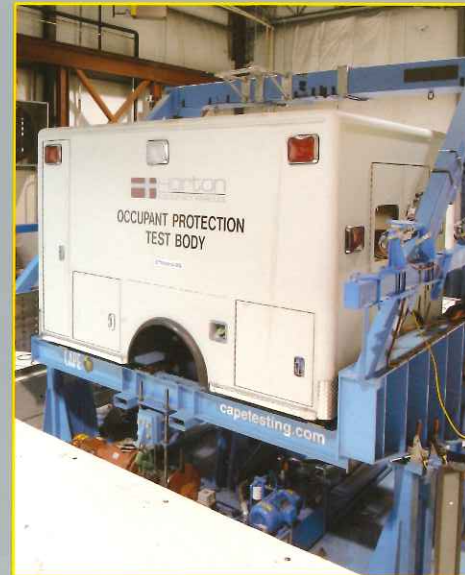
It didn't happen overnight. It started more than 30 years ago with a desire to build an ambulance body that would not only stand up to day-to-day driving rigors, but would protect its occupants.

Safety isn't a guessing game. "I *think* this cabinet will stay in place in an accident" is not an acceptable answer. "We *heard* about a crash with our vehicle where no one was hurt" is not an acceptable answer. "We've been doing it this way for years," is not an acceptable answer.

The only solution is to test. And now, our decades of testing has brought Horton to this point: two types of advanced airbag protection, custom restraint systems and sophisticated head protection. Modeled on computers and then tested with real impacts, HOPS is truly Safety With Substance.

Real world solutions backed by extensive research engineering and testing — it's what you have come to expect from us.

See your Horton dealer today and let him show you how you can use HOPS, the most advanced ambulance occupant protection system ever designed for rollover crashes, to protect your staff.



HOPS testing was conducted at the Center for Advanced Product Evaluation (CAPE[®]), a modern crash-test facility on the campus of IMMI. Above: a technician preps the sensors, high speed cameras and data collectors before a side impact test. Right: rollover impacts are created with this massive frame, the only one of it's kind in the world.



Safety With Substance **HOPS** 

3800 McDowell Road, Grove City, Ohio, 43123 • 614.539.8181 • fax 614.539.8165 • email: info@hortonambulance.com





Horton
EMERGENCY VEHICLES

Crash Test Documentation



March 14, 2007



Mr. Malley
Horton Emergency Vehicles
3800 McDowell Road
Grove City, OH 43123

Dear Mr. Malley

18881 US 31 North
P.O. Box 1020
Westfield, Indiana
46074-1020

Phone
(317) 896-9531
Fax
(317) 867-2305

This document is to confirm that on February 22, 2007, IMMI's Center for Advanced Product Evaluation (CAPE) witnessed the dynamic roof pre-load, static roof load and dynamic rear impact tests as regulated by ECE R29, SAE J2422 and SAE J2420 on a Horton Emergency Vehicles ambulance box. The events were documented with photographs, real-time and high-speed video, vehicle accelerometers, cart accelerometers and a laser speed trap.

This document also reports that the Horton Emergency Vehicles ambulance box is in compliance with the requirements and intent of standards ECE R29, SAE J2422 and SAE J2420. Whereby, the testing with regard to the above-mentioned standards was carried out by CAPE on February 22, 2007 at IMMI's Center for Advanced Product Evaluation in Westfield, Indiana.

Sincerely,

James R. Chinni P.E.
Director of CAPE

Ref: CTR02181



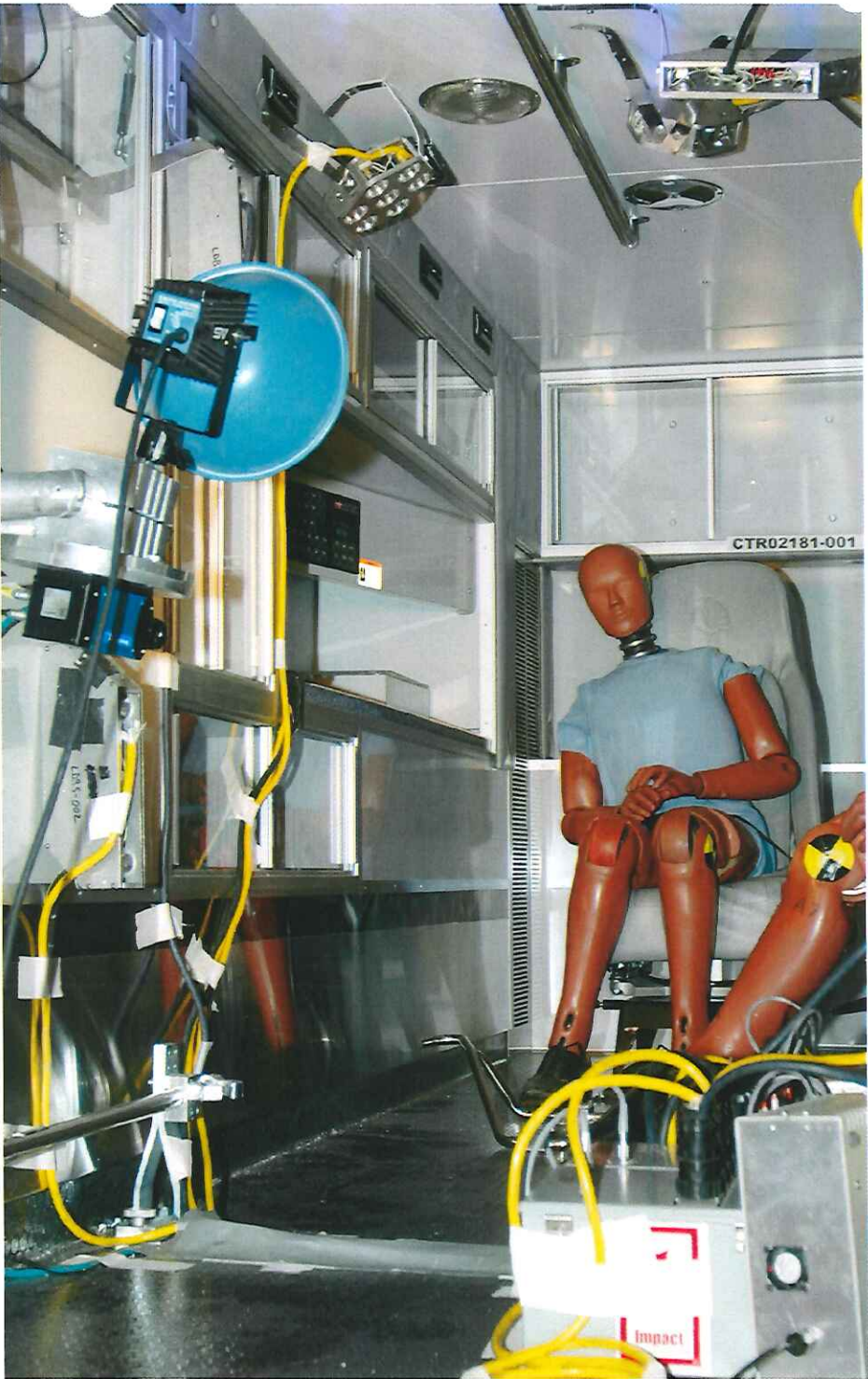
Corner Impact Test



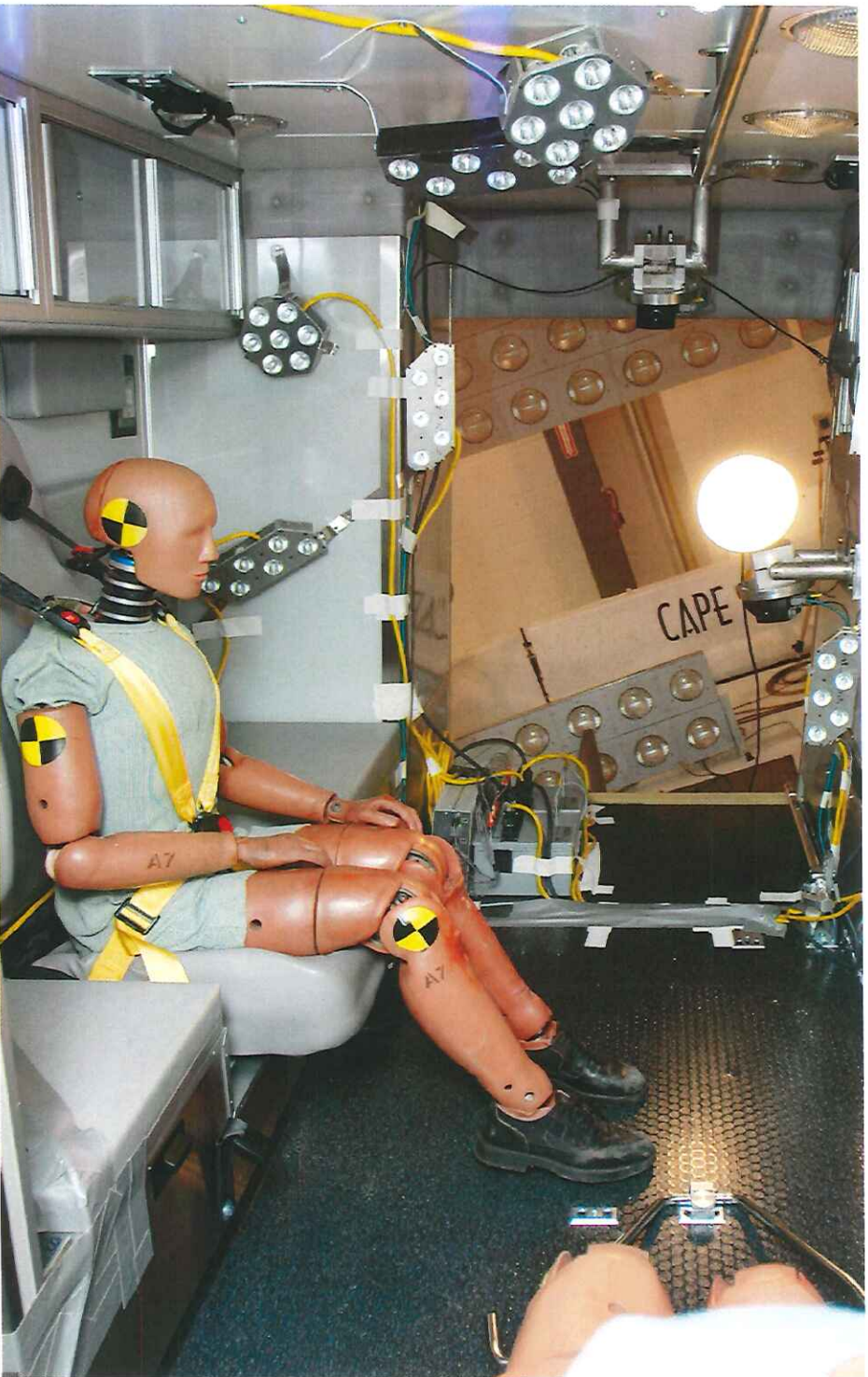
Rear Impact Test



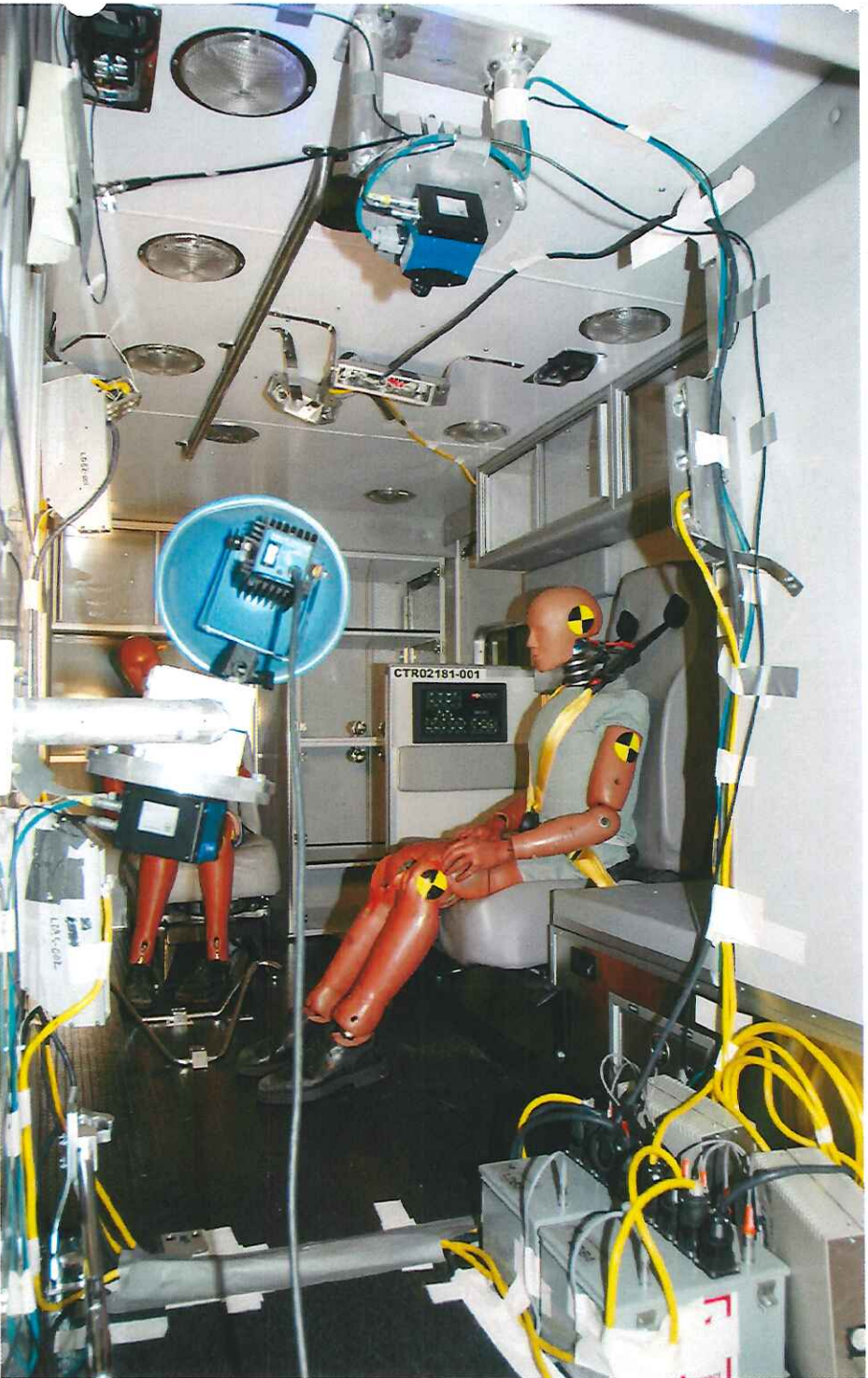
Dynamic Roof Pre-Load



Attendant Seat -- Impact Test



Five Point Harness, Bench Seat -- Impact Test



Five Point Harness, Bench Seat -- Impact Test

Item 1 Safety Certification

The bidder must perform and certify to dynamic sled or impact testing run on the ambulance body to a load of 30 G's. The body tested shall include normally installed components for each of the following areas of the vehicle. The body structure and installed components shall not show evidence of structural failure or separation from its mounted position as a result of the test. All test results must be witnessed and verified by a Registered Professional Engineer.

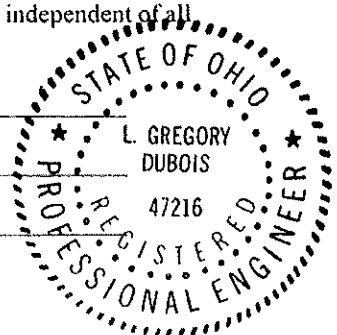
	Test Required	Date Tested	Body Test Force Applied	Signed By
1.	Body to Chassis Mounting	4/20/06	30 G's	<i>L. Gregory Dubois</i>
2.	Access Door Latching	4/20/06	30 G's	<i>L. Gregory Dubois</i>
3.	Oxygen Cylinder Mount (Main)	4/20/06	30 G's	<i>L. Gregory Dubois</i>
4.	Oxygen Cylinder Mount (Portable)	4/20/06	30 G's	<i>L. Gregory Dubois</i>
5.	Attendant Seat Mount	4/20/06	30 G's	<i>L. Gregory Dubois</i>
6.	Attendant Seat Belt	4/20/06	30 G's	<i>L. Gregory Dubois</i>
7.	CPR Seat Belt	4/20/06	30 G's	<i>L. Gregory Dubois</i>
8.	Squad Bench Seat Belt	4/20/06	30 G's	<i>L. Gregory Dubois</i>
9.	Retention of Main Cabinet Wall	4/20/06	30 G's	<i>L. Gregory Dubois</i>
10.	Crash Restraint Wall At Head of Bench	4/20/06	30 G's	<i>L. Gregory Dubois</i>

Certification of Registered Professional Engineer

I L. Gregory Dubois attest that I am a Registered Professional Engineer registered in the State of Ohio. My Registration Number is 47216.

I hereby certify that I and my company, its affiliates and subsidiaries are completely independent of all manufacturers, suppliers, and vendors in the ambulance industry..

Signature *L. Gregory Dubois*
 Company CTL Engineering, Inc
 Date June 22, 2006

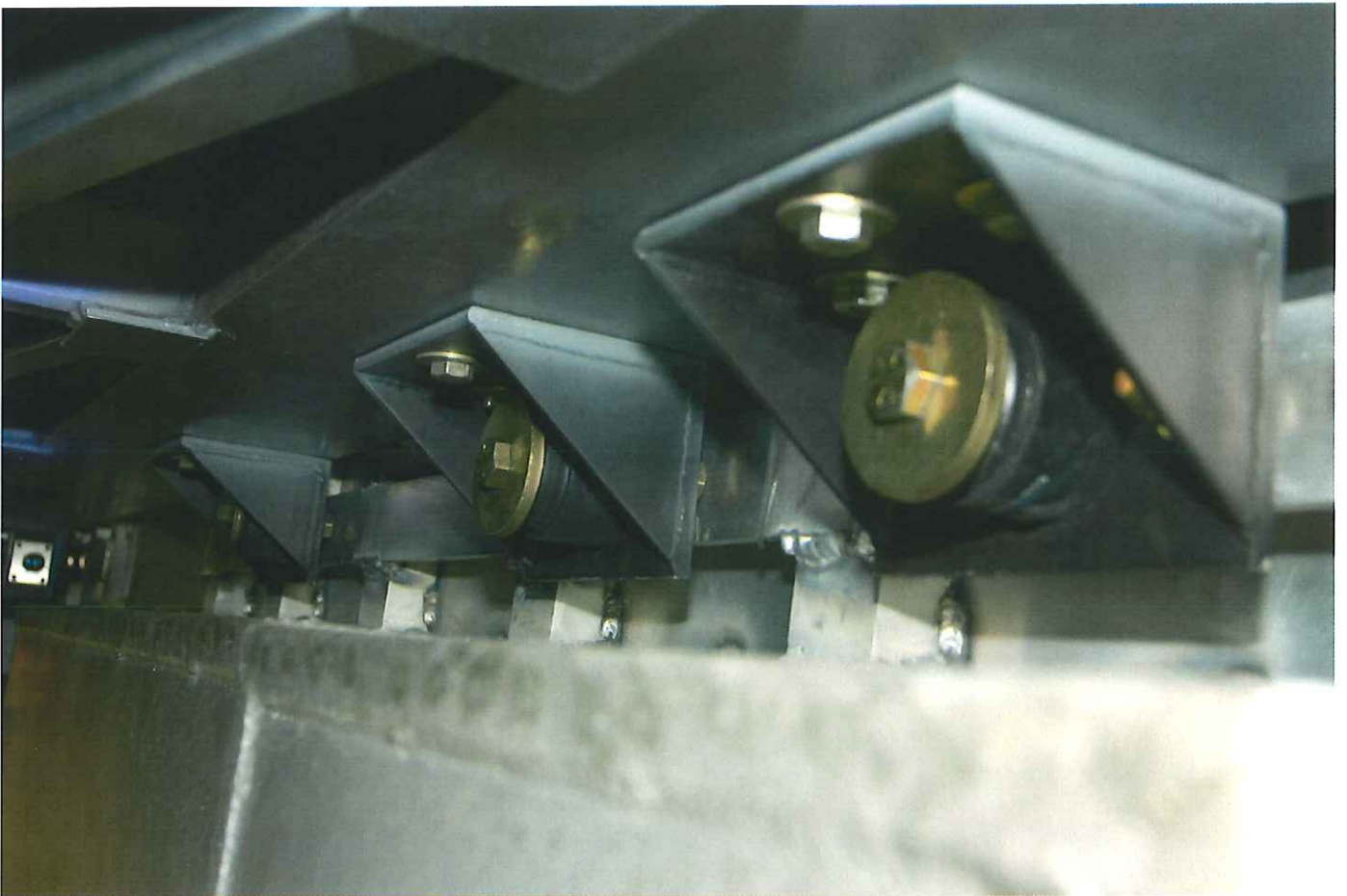


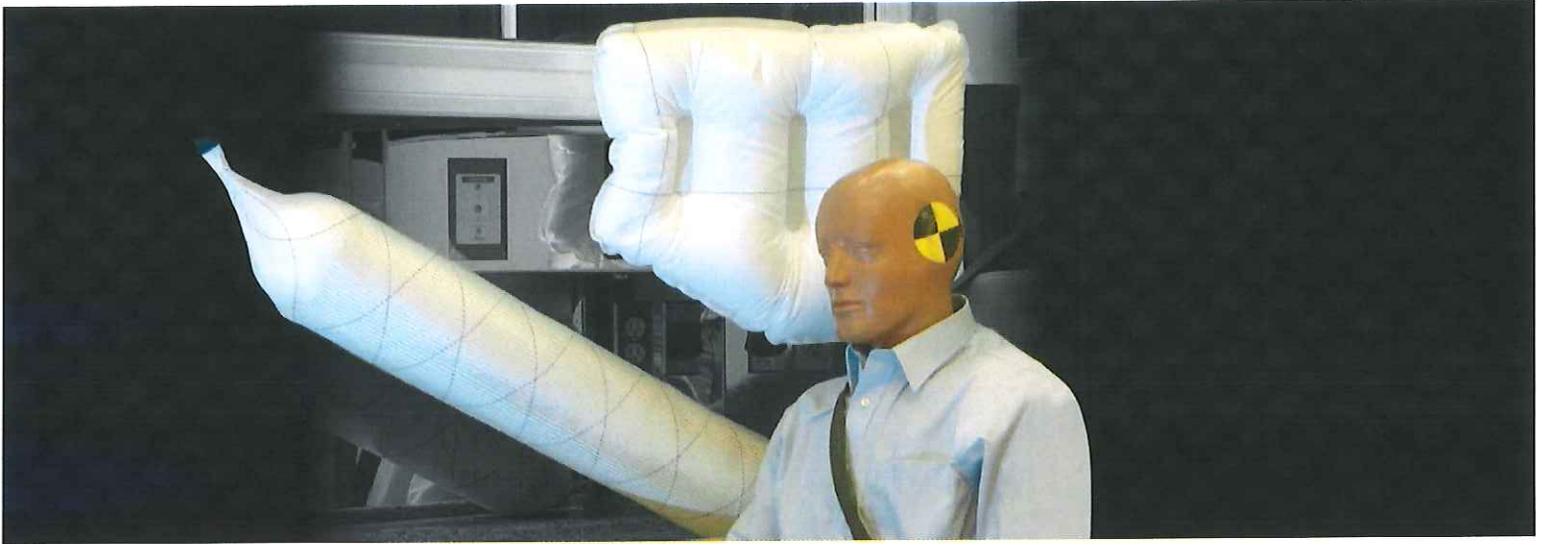
**Horton Emergency Vehicles
2006 623 Modular Ambulance
Flat Frontal Impact Simulation
VI Tech Mounting System
Test Numbers: S060420**











Dare to Compare

Seeking to improve safety, the ambulance industry is constantly introducing new technology. But without extensive testing, can companies really stand by their product? Many manufacturers lead end-users to a false sense of security by offering unverified solutions.

As the only ambulance manufacturer independently and repeatedly testing since 1974, Horton continues to lead the industry. Unsatisfied by simply "getting by" on testing, Horton exceeds all KKK, NFPA and AMD standards, **by as much as 400%**.

Take, for example, AMD standard 001 – the static load roof test. All manufacturers are required to administer the test and most exceed the standard. Horton not only exceeds the standard, Horton exceeds the competition with a verified performance of **up to 90,000 pounds of force**.

Taking the extra step for more accurate testing, we perform our static load test with a 10-point mounting system to the test body floor. Some competitors complete their static load test on steel beams for full-body support. Horton's test **more accurately represents authentic mounting** to a chassis frame, giving the consumer results that are based on real-life scenarios, not a modified testing environment.

Test	Horton
Static Load Test – Roof	90,000 lbs.*
Static Load Test – Side	44,000 lbs.
Side Body Impact	35G of force
Squad Bench Test	19,500 lbs.
Attendant Seat Test	9,250 lbs.
CPR Seat Test	7,000 lbs.
Cabinet Structure Test	145,874 lbs.**
Head of Bench Test	145,874 lbs.**
Liter Retention Test	2,500 lbs.

*On 10-point mounting system

**Based on side crash test with 35G of force

35G of destructive crash testing

We could have stopped there, but we didn't. Voluntarily, Horton subjects its ambulances to additional dynamic testing, again mirroring the real-life scenario of an actual crash (as opposed to a static load). These tests include:

HYGE sled

At impact, a vehicle stops rapidly and the occupant maintains velocity. HYGE testing stimulates this exact situation, but in reverse - driving the test vehicle out from under a stationary mannequin. As the acceleration and deceleration effects are interchangeable, HYGE sled testing provides the most accurate crash test results. High-speed cameras and instrumented test dummies capture the data for quantifiable results.



Destructive impact

The Society of Automotive Engineers (SAE) impact tests verify the integrity of patient area by striking the module front, side and roof edge with a computer-controlled sled, whose speed, weight and force of impact are all measurable and controlled.



Rollover crash

One of only two in the United States, this dynamic rollover test machine enables third-party engineers to simulate a rollover event while recording significant test data from inside the vehicle. Horton continues its tradition as the only ambulance manufacturer testing for rollover crashes, knowing it's our duty to try our systems in the most fatal of ambulance crashes. Rollover tests monitored effects on the module and successful HOPS deployment.



Hybrid III mannequins relay occupant impact data

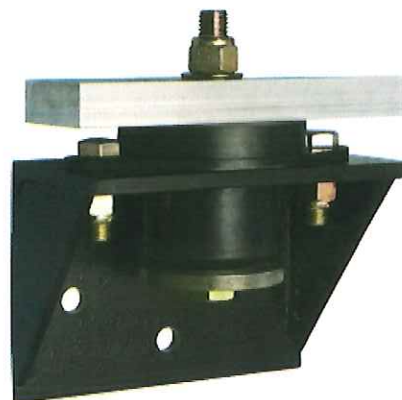
Testing with fully-instrumented Hybrid III mannequins allows us to measure the transference of energy through the module structure into the occupants themselves. During mannequin testing, Horton monitors the head, neck and chest cavity for potentially damaging forces. Just because a seatbelt holds up to several thousand pounds of pull doesn't mean that it will have an acceptable impact on the occupants themselves.

Results from mannequin testing have influenced future Horton innovations, such as progressive resistance foam. Mannequin testing showed significant head injuries due to standard foam "bottoming out" during a head strike. As a result, Horton now uses progressive resistance foam padding, which dissipates the head strike energy throughout a layered protective surface.



Strength reinforced by mounting

Horton's dynamic testing has shown the standard .33" center bolt in OEM mounting pucks to shear in accidental forces greater than 10Gs (roughly a 15 mph collision). In some cases, this can actually cause the module to separate from the frame rails. As a result, Horton developed a stronger and more reliable mounting system – VI-Tech. The VI-Tech mounting system uses three failsafe flanges, two elastomer isolators and a .75" center bolt, tested and proven to retain mounting in 35G impact crashes. As an added benefit, VI-Tech improves ride quality with reduced noise and a smoother ride.



Third-party validation

At Horton, we believe the best way to conduct testing is through third-party facilities. While some might find in-house testing to be more economically reasonable, there are several distinct advantages to using a third party for testing. These include:

- Unbiased assessment of quality and assembly
- Independent perspective in testing process
- Professionals that specialize in the area of testing
- More accurate reporting and results

Horton uses several third-party testing companies with differing specialties including CTL Engineering, Transportation Research Center Inc., IMMI CAPE® (the Center for Advanced Product Evaluation), Progressive Engineering Inc. and Bosch Testing Facility.

And testing isn't limited just to safety. All features introduced as Horton standards have been tested and proven to stand up in performance, durability and reliability.



In the comparison of ambulance features, testing results are the voice of reason. Horton continues to outperform all other products available on the market and we have the results to prove it.

We dare you to compare.





5/27/15

Ladies and Gentlemen:

Thank you for the opportunity to submit a bid for your emergency vehicle. Enclosed you will find Everest Emergency Vehicle's proposal.

2016 Ford F550 4x4 Road Rescue Type I Ultramedic Per specifications with exceptions noted.


		\$195,000
Option:	To meet customer's conceptual design per drawing and specifications, move side entry door, ALS compartment, HVAC.	
	Add to Bid Total:	\$2211
	Installation of Customer supplied items. (See detailed attachment in Tab 1)	
	Add to Bid Total:	\$4015
	Install Dealer Provided Firecom 5200D (Two radio capability) Intercom system. Per customer specifications.	
	Add to Bid Total:	\$4580

Everest Emergency Vehicles is family owned and operated business founded in 1999. Everest Emergency Vehicles is the authorized dealer for Road Rescue, Inc. in Minnesota, Wisconsin, and Iowa and Wheeled Coach Industries in the states of Minnesota and Wisconsin. Our dedicated staff, sales team and service department has over 140 years combined emergency vehicle experience, we promise to exceed your highest expectations.

Everest EV provides new and used ambulance sales along with module service and parts. We have mobile service as well as a loaner/rental fleet available. Our service team specializes in ambulance repair and maintenance. We are a factory authorized ambulance remounting center and Ford QVM Certified. Please call on us for all your emergency vehicle needs.

Please let me know if you have any questions, thank you.

Respectfully Submitted by:


Everest Emergency Vehicles, Inc.



1133 Rankin St., Suite D-1 • Saint Paul, Minnesota 55116
Toll Free 800-889-6143 • Direct 651-690-1867 • Fax 651-690-1871
info@everestev.com • www.everestev.com

Village of Pleasant Prairie
Respondent's Proposal

Village of Pleasant Prairie
Kenosha County, Wisconsin

We hereby propose to furnish one (1) Medium Duty Type I ambulance module & vehicle chassis for the following price:

Medium Duty Type I ambulance module & vehicle chassis:

Total cost \$ \$195,000

Exceptions: Please see exception and clarifications tab in proposal.

**Note: if more room is needed, please attach sheet.*


1. If awarded the contract, delivery will be in 180 days, or less. After receipt of chassis.
2. Enclosed is a proposal bid bond for 10%.

Respectfully submitted,

Robert Marhefke

Everest Emergency Vehicles, Inc.

By:


Signature

WI Sales Rep

Title

May 27, 2015
Date

MV-4302

State of Wisconsin Dealer
License Number

N/A

State of Wisconsin Salesperson
License Number

**Village of Pleasant Prairie
Respondent's Proposal
History and Financial Background**

1. Name of company Everest Emergency Vehicles, Inc

2. Date your company entered the business 1999

3. Names of principals Robert Haselman

Lori Haselman

4. Current number of fulltime equivalent employees

Overall company	
Sales and marketing	<u>4</u>
Technical support	<u>2</u>
Maintenance	<u>5</u>
Other	<u>1</u>
Total	<u>12</u>

5. Number of locations serving this village:

Sales	<u>2</u>
Service	<u>1</u>

6. Number of equipment in service 500+

7. Cities, villages, towns using your equipment * see references

8. Dunn and Bradstreet rating N/A

9. Major subsidiaries, siblings, parent Co. N/A

10. Description of organization* Please see dealer information tab.

**Note: if more room is needed, please attach sheet.*

**Village of Pleasant Prairie
Respondent's Proposal
Signature Page and Legal Status**

The undersigned certified that they are an official legally authorized to bind their firm and to enter into a contract should the Village of Pleasant Prairie, Fire & Rescue Department accept this proposal.

Proposal by: Everest Emergency Vehicles, Inc.
(Name of Firm)

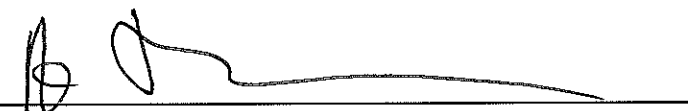
Legal status of respondent: *(please check the appropriate box)*

A. Corporation: _____ State of Incorporation: MN

B. Partnership: _____ List Names: _____

C. DBA: _____ explain: _____

D. Wisconsin Ambulance Dealers License # _____

Signature of respondent:  _____

Title : President

Address: 1133 Rankin Street Suite D-1

City: St Paul, MN Zip: 55116

Telephone no. (651) 236-8944

Signed this ___ day of _____ 20__.

Document A310™ – 2010

Conforms with The American Institute of Architects AIA Document 310

Bid Bond

CONTRACTOR:

(Name, legal status and address)

Everest Emergency Vehicles, Inc.
1133 Rankin Street Suite D-1
St. Paul, MN 55116

SURETY:

(Name, legal status and principal place of business)

RLI Insurance Company
9025 N. Lindbergh Drive
Peoria, IL 61615

This document has important legal consequences. Consultation with an attorney is encouraged with respect to its completion or modification.

OWNER:

(Name, legal status and address)

Village of Pleasant Prairie Fire & Rescue Department
Village Hall, 9915 39th Avenue
Pleasant Prairie, WI 53158-6504

Any singular reference to Contractor, Surety, Owner or other party shall be considered plural where applicable.

BOND AMOUNT: \$ 10%

Ten Percent of Amount Bid

PROJECT:

(Name, location or address, and Project number, if any)

Supply One (1) Type 1 Ambulance

The Contractor and Surety are bound to the Owner in the amount set forth above, for the payment of which the Contractor and Surety bind themselves, their heirs, executors, administrators, successors and assigns, jointly and severally, as provided herein. The conditions of this Bond are such that if the Owner accepts the bid of the Contractor within the time specified in the bid documents, or within such time period as may be agreed to by the Owner and Contractor, and the Contractor either (1) enters into a contract with the Owner in accordance with the terms of such bid, and gives such bond or bonds as may be specified in the bidding or Contract Documents, with a surety admitted in the jurisdiction of the Project and otherwise acceptable to the Owner, for the faithful performance of such Contract and for the prompt payment of labor and material furnished in the prosecution thereof; or (2) pays to the Owner the difference, not to exceed the amount of this Bond, between the amount specified in said bid and such larger amount for which the Owner may in good faith contract with another party to perform the work covered by said bid, then this obligation shall be null and void, otherwise to remain in full force and effect. The Surety hereby waives any notice of an agreement between the Owner and Contractor to extend the time in which the Owner may accept the bid. Waiver of notice by the Surety shall not apply to any extension exceeding sixty (60) days in the aggregate beyond the time for acceptance of bids specified in the bid documents, and the Owner and Contractor shall obtain the Surety's consent for an extension beyond sixty (60) days.

If this Bond is issued in connection with a subcontractor's bid to a Contractor, the term Contractor in this Bond shall be deemed to be Subcontractor and the term Owner shall be deemed to be Contractor.

When this Bond has been furnished to comply with a statutory or other legal requirement in the location of the Project, any provision in this Bond conflicting with said statutory or legal requirement shall be deemed deleted herefrom and provisions conforming to such statutory or other legal requirement shall be deemed incorporated herein. When so furnished, the intent is that this Bond shall be construed as a statutory bond and not as a common law bond.

Signed and sealed this 29th day of May, 2015

(Witness)

(Witness) Cindy Winek

Everest Emergency Vehicles, Inc.

(Principal)

(Seal)

By: 

(Title)

RLI Insurance Company

(Surety)

(Seal)

By: 

(Title) Teresa M. Sheppard Attorney-in-Fact



RLI Surety
 P.O. Box 3967 | Peoria, IL 61612-3967
 Phone: (800)645-2402 | Fax: (309)689-2036
 www.rlicorp.com

POWER OF ATTORNEY

RLI Insurance Company

Know All Men by These Presents:

That this Power of Attorney is not valid or in effect unless attached to the bond which it authorizes executed, but may be detached by the approving officer if desired.

That **RLI Insurance Company**, an Illinois corporation, does hereby make, constitute and appoint:
Teresa M. Sheppard, William J. Lammel, jointly or severally

in the City of Memphis, State of Tennessee its true and lawful Agent and Attorney in Fact, with full power and authority hereby conferred, to sign, execute, acknowledge and deliver for and on its behalf as Surety, the following described bond.

Any and all bonds provided the bond penalty does not exceed Twenty Five Million Dollars (\$25,000,000.00).

The acknowledgment and execution of such bond by the said Attorney in Fact shall be as binding upon this Company as if such bond had been executed and acknowledged by the regularly elected officers of this Company.

The **RLI Insurance Company** further certifies that the following is a true and exact copy of the Resolution adopted by the Board of Directors of **RLI Insurance Company**, and now in force to-wit:

"All bonds, policies, undertakings, Powers of Attorney or other obligations of the corporation shall be executed in the corporate name of the Company by the President, Secretary, any Assistant Secretary, Treasurer, or any Vice President, or by such other officers as the Board of Directors may authorize. The President, any Vice President, Secretary, any Assistant Secretary, or the Treasurer may appoint Attorneys in Fact or Agents who shall have authority to issue bonds, policies or undertakings in the name of the Company. The corporate seal is not necessary for the validity of any bonds, policies, undertakings, Powers of Attorney or other obligations of the corporation. The signature of any such officer and the corporate seal may be printed by facsimile."

IN WITNESS WHEREOF, the **RLI Insurance Company** has caused these presents to be executed by its Vice President with its corporate seal affixed this 24th day of February, 2012.



RLI Insurance Company

By: [Signature]
 Roy C. Die Vice President

State of Illinois }
 County of Peoria } SS

CERTIFICATE

On this 24th day of February, 2012, before me, a Notary Public, personally appeared Roy C. Die, who being by me duly sworn, acknowledged that he signed the above Power of Attorney as the aforesaid officer of the **RLI Insurance Company** and acknowledged said instrument to be the voluntary act and deed of said corporation.

I, the undersigned officer of **RLI Insurance Company**, a stock corporation of the State of Illinois, do hereby certify that the attached Power of Attorney is in full force and effect and is irrevocable; and furthermore, that the Resolution of the Company as set forth in the Power of Attorney, is now in force. In testimony whereof, I have hereunto set my hand and the seal of the **RLI Insurance Company** this 29th day of May, 2015.

By: [Signature]
 Jacqueline M. Bockler Notary Public

RLI Insurance Company

By: [Signature]
 Roy C. Die Vice President



Surety Bond Request Form

Prepared for
Allied Specialty Vehicles, Inc.

Date: 05-19-2015			
From:	Bob Haselman, Everest EV	ASV Entity:	Road Rescue
		Phone:	651-236-8944
Bond Type: (check one)	<input checked="" type="checkbox"/> Bid Bond	<input type="checkbox"/> Performance Bond	<input type="checkbox"/> Other Bond type (specify)
Guarantor/Principal Name and Address:		Obligee(s) Name and Address (company to whom the bond should be made payable)	
Everest Emergency Vehicles, Inc.		Village of Pleasant Prairie Fire & Rescue Department	
1133 Rankin Street Ste-D1		Village Hall, 9915-39th Avenue	
St Paul, MN 55116		Pleasant Prairie, WI 53158	
Description of Goods and Services to be provided			
2016 F550 4x4 Road Rescue Ultramedic			
Type 1 Ambulance			
For Bid Bonds , complete the following: <input checked="" type="checkbox"/> Generic Bond Form		<input type="checkbox"/> Specific Bond Form required by Obligee (form must be attached)	
Bid date	May 29th 2015	Penalty Clause: <input type="checkbox"/> Yes <input type="checkbox"/> No	LDs:
Bond amount	10%	Proposed delivery	
Estimated contract price	\$200,000	Warranty period	
Insurance amount	\$		
Performance bond required	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No		
For Performance Bonds , complete the following and attach a copy of contract or purchase order: <input type="checkbox"/> Generic Bond Form		<input type="checkbox"/> Specific Bond Form required by Obligee (form must be attached)	
Contract award date		Start date	
Total contract price	\$	Completion date	
Value of subcontractor portion	\$	Down payment	\$
Performance bond amount	\$	Trade-in	
Payment bond amount	\$		
For "Other" Bond type , complete the following: <input type="checkbox"/> Generic Bond Form		<input type="checkbox"/> Specific Bond Form required by Obligee (form must be attached)	
Type		Bond amount	\$
Effective date		Expiration date	

Deliver Completed Bond To:
Everest EV ATTN: Bob Haselman
1133 Rankin Street Suite D-1
St Paul, MN 55116
651-236-8944

NOTE: Always attach a copy of contract specifications and specimen of bond form required.





QUOTATION

Village of Pleasant Prairie Fire & Rescue Departme Craig Roepke Village Hall, 9915-39th Avenue Pleasant Prairie, WI 53158 (262) 694-8027	Everest Emergency Vehicles, Inc. Bob Marhefke 1133 Rankin Street Suite D-1 St Paul, MN 55116 651-236-8948
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Quote No: 138-0010
 05/27/2015 14:47:47

PART NO	S	DESCRIPTION	QTY
		== Road Rescue Boilerplate - 2014.018 03/26/15 ==	1
00-01-0999		Information - Scope, Purpose and Classification	1
		WARRANTY	1
00-02-8100		Warranty - Documentation	1
00-02-8200		Warranty - Module Structural	1
00-02-8300		Warranty - Electrical, Standard System	1
00-02-8400		Warranty - Paint	1
00-02-8500		Warranty - Conversion	1
		== Ford, F-Series, 189" WB - 168" Bodies - 2014.018 03/26/15 ==	1
		01- CHASSIS REQUIREMENTS	1
01-01-2604	>	2015 Ford F550 XLT 4X4, , 189"W/B, 18,000 GVWR (Special Request)	1
		CHASSIS OPTIONS	1
01-03-1001		Alternator - Standard OEM	1
01-07-0120		Front Suspension - Ford F-Series (4x4)	1
01-07-1000		Sway Bar, Front - OEM	1
01-07-8015	<	Rear Suspension - Liquid Spring- F550 4x4 R4G004	1
01-07-9020	<	Dump - Circuit, Dump W/ enable switch in cab. State Location: Cab console Enable switch to override auto dump with rear door open. Only to reset when switch is deactivated.	1
01-17-5100	< >	Block Heater, - Wired to Shoreline- includes switch Block heater wired to shoreline with ability to turn on/off	1
		CAB EXTERIOR OPTIONS	1
01-20-0100		Mirrors - OEM Standard	1
		CAB INTERIOR OPTIONS	1
01-21-3500		Cab Insulation - Thinsulate in Cab Ceiling (STANDARD)	1
01-22-1900		Additional Key for OEM Chassis	1
		TIRES / WHEELS	1
01-23-4000	<	Spare Tire Bracket - Installed. (STANDARD) Install Spare Tire Mounting Bracket. See drawings for location. (STANDARD)	1
01-24-2003		SS Wheel Simulators - Phoenix w/ Lug Nut covers, F-Series and TerraStar	1
		== Type 1 - UM 168" Module, F-Series - 2014.018 03/26/15 ==	1
02-01-7002	S < >	Ultramedic - 168"L x 96"W Type 1 Module 15-4 Ultramedic I - 168"L x 96"W Type I Aisle width to be 46". The interior headroom shall be 72"	1
02-01-9500		Interior Headroom - 72"	1
02-01-9622	>	Standard Aisle Width = 46"	1
02-02-1000		Materials, Type I & Type III	1
		03 - MODULE EXTERIOR	1

PART NO	S	DESCRIPTION	QTY
03-02-0200		Module Undercoating (STANDARD)	1
EXTERIOR MODULE CONSTRUCTION			1
03-03-1400		> Module Roof Radius, 3 Inch High (STANDARD)	1
03-03-1500		> Extreme Bonding Tape (STANDARD)	1
03-03-1600		> Sub-floor Gusset Supports (STANDARD)	1
03-03-1700		> One Piece Side Body Panels (STANDARD)	1
03-03-1800		> Reinforce Rear Header (STANDARD)	1
03-03-1900		> Single Sheet Module Roof Sheet (STANDARD)	1
03-03-8005		Fuel Fill - Standard Pocket -Square	1
03-03-8401		DEF Fill - Standard Pocket, For Ford F series	1
MODULE ENTRANCE			1
03-05-0100		> Module Access - Standard	1
03-06-0100	<	Entry Doors - Hidden Hinge, Seal on Door ,W/Fail Safe Latching,STD This includes the Fail Safe Latching mechanisms on all three entrance doors.	1
03-06-0700		Door Restraints - Rear, Polished Cast Aluminum, Pin & Slot Style	1
03-06-0850	<	Door Restraints - Side, Heavy Duty Gas Spring Hold Opens Curbside Door Restraint - Heavy Duty gas spring hold opens. - Attached by 1/4" x 20 SS Machine Screws with 1/4" x 20 Nut Inserts into Corner Gusset in Door Side entrance door to open to 90+ degrees. (STANDARD)	1
03-06-1011		Entry Door Handles - TriMark Chrome (includes interior handles)	1
03-06-1500		Door Panel, Inner - Stainless Steel (STANDARD)	1
03-06-2200		Magnetic Entry Door Switches (STANDARD)	1
03-06-4010	>	Lock Boxes, White Diamond Grade (STANDARD)	1
03-06-4200		Coil Cords for Electrical Circuits (STANDARD)	1
03-06-4310		Door Reflectors - Red, (1) Each Door at Bottom	1
03-06-4400		Door Panel Mounting Screws (STANDARD)	1
03-06-5057	S <	Door Flashers - (1) Whelen Red LED Strip Light, Specify Location (1) Whelen Red LED Strip Light w/ internal flashers. - Specify Location and Quantity of light. LED Strip to be placed on side patient access door on lower section. Activated when door is opened	1
03-06-6065	S <	Power Door Locks - Module Entry Doors,W/Hidden Switch Install power door locks in side and rear entrance doors. Includes (2) lock/unlock switches, one at side door and one at rear door. Also includes (1) hidden switch in rear license frame for unlock function only. System will be tied to OEM chassis power locks so that both systems operate as one. Does not include wireless remote. Note: Hidden Switch can be moved per dealer instructions.	1
SIDE DOOR STEP			1
03-08-0040	>	Step Well, Side Entry - 9" Deep, Grip Strut Step. (STANDARD)	1
03-08-3010		Stepwell Light - LED, Whelen OS Mini, Clear (STANDARD)	1
WINDOWS			1
03-09-0030		Module Window Requirements	1
03-09-1170		Window - Side Entry Door, 18X18, Fixed - Bronze Glass	1
03-09-1220		Window - Rear Entry Doors, 18X24, Fixed - Bronze Glass	1
03-09-2000	<	Window - Squad Bench, Framming Only for Future Use Add framework only over curbside squad bench. - Add framework in curbside wall over the squad bench to allow 14"H x 33.5"L window to be installed at a later date.	1
MODULE TO CHASSIS MOUNTING SYSTEM			1
03-10-5000		Module to chassis mounting system-Light Duty I (10)	1
03-11-6000		Connecting Bellows, Type I	1
REAR BUMPER AND REAR STEP CONSTRUCTION			1
03-12-1800		Rear Bumper - Recessed 9" Pocket, Flip-up	1

PART NO	S	DESCRIPTION	QTY
03-12-3020	< >	Bumper Pods, CPI Cast Aluminum w/RR Logo. (UM & PM) Cast Products Bumper Pods, High Polish Finish. Mounted on the outboard rear step bumper frame members. The CPI cast aluminum pod shall include a RR Logo embossed in the cast pattern on the step tread.	1
INSULATION			1
03-13-2220	< >	Whisper Quiet - Sound Dampening/Thermal Insulation Package (UM & PM) Whisper Quiet - Sound Dampening/Thermal Insulation Package. (ULTRAMEDIC AND PROMEDIC) Spray foam underbody insulation prior to undercoat application, full subfloor coverage where applicable. Includes curbside step well area.	1
ELECTROLYSIS PREVENTION			1
03-14-4000	<	Electrolysis Prevention, Fluid Film (STANDARD) Fluid Film is used at Every Point Where the Mounting Process has the Propensity to Break Paint (STANDARD)	1
COMBINATION RUB RAIL AND FENDER RING			1
03-15-2000	<	Crash Rail and Fender Ring (STANDARD) Extruded Black Rubber Crash Rail and Fender Ring - The lower body Rub Rail/Crash Rail and Fender Ring to be Black extruded rubber. The rubber rail will be snap-on mount with no fasteners. The extrusion that the rubber rail fastens to will be secured to the body with aluminum rivets for ease of replacement. The lower body crash rail and the transition to the Fender Ring will have Polished Aluminum end caps. The extrusion to accept multiple color inserts. (STANDARD)	1
03-15-5110		White Scotchlite	1
SPLASH GUARDS AND RUNNING BOARDS			1
03-16-1520		Running Boards - Diamond Plate, Type I (Standard)	1
03-16-1800	<	Mud Flaps - Rear, Black w/ RR Logo (Standard) Heavy Duty Rubber Mud Flaps w/ RR Logo - Heavy Duty Rubber Mud Flaps to be bolted to the wheel liner behind the rear duals with with 1/4" x 20 Stainless Steel bolts, washer and nylon locknut for ease of maintenance and repair (STANDARD)	1
03-17-1000	<	Drip Rails Polished Aluminum Drip Rails - Above All Doors, Entry and Compartment (Standard)	1
03-18-1000	<	Wheel well liners Aluminum Wheelwell Liners - Extending to Bottom of Skirt (Standard)	1
CAB TO MODULE ACCESS			1
03-19-1000		Walk Thru Opening (Full Height)- Type I, Ford/Chevy/GMC	1
03-19-2000	<	Cab to Module - Walk Through Door, (STANDARD) The cab to module walk/crawl through to include a clear fixed window and a self latching hinged door in the open or closed position with the actuator latch being on the cab side. Overall door height to accommodate a fluid dam. (STANDARD)	1
03-19-2100	<	Cab to Module Fluid Dam, (STANDARD) Cab to Module Fluid Dam - A formed fluid dam, 4" tall to be installed below the cab to module walk through door to prevent fluids from contaminating the cab. The floor and fluid dam to be fully sealed. (STANDARD)	1
LICENSE PLATE HOLDER			1

PART NO	S	DESCRIPTION	QTY
03-20-0510	>	Rear License Holder Location - C/S Above Rear Kickplate	1
		EXTERIOR COMPARTMENT CONSTRUCTION	1
04-01-0100		Exterior Compartment Construction, Light Duty	1
04-01-3000	<	Door Sill Protection, Stainless Steel. (STANDARD)	1
		Door Sill Protection - Install Stainless Steel sill protector on lower edge of all door frames to prevent paint damage. (STANDARD)	
		EXTERIOR COMPARTMENT DOORS	1
04-02-0100		Compartment Door - Hidden Hinge, Seal on Door (UM)	1
04-02-0710	<	Compartment Handles, Exterior - TriMark Chrome (UM)	1
		TriMark Two-Point Cast w/Polished Chrome Paddle Handle and Housing, Locking and Non-Locking, with floating cam - Mounted in CNC Cut Opening in Each Compartment Door Skin w/OEM Clamp Assembly. (STANDARD ULTRAMEDIC)	
		Note: Must pick handles from menu.	
04-02-0740	<	Compartment Handle, TriMark Chrome, Street side, locking	1
		Pick QTY for single doors or Leading doors on the streetside.	
04-02-0750	<	Compartment Handle, TriMark Chrome, Curbside, locking	1
		Pick QTY for single doors or Leading doors on the curbside.	
04-02-0800	<	Compartment Door Locks - TriMark	1
		Compartment Door Handle Manual Locks. - All doors shall incorporate double cut, non-directional tumbler assemblies that are keyed alike (STANDARD)	
04-02-1000		Compartment Door Nader Pin - (STANDARD)	1
04-02-1200		Compartment Door Lubrication - Door Handles and Latches (STANDARD)	1
04-02-1300		Compartment Door Reflectors - (STANDARD)	1
04-02-1500		Magnetic Compartment Door Switches - (STANDARD)	1
04-02-2010		Compartment Door Panel, Inner - Diamond Plate (STANDARD)	1
04-02-2300		Compartment Door, Gas Struts - (STANDARD)	1
04-02-2500		Compartment Door Panel Mounting Screws - (STANDARD)	1
04-02-5999		Power Door Locks - Exterior Compartment(s)	1
04-02-6010		Exterior Compartment #'s 1, 2, 4, 6 & 8, Only	1
04-03-0410	<	Compartment Light Locations - Ultramedic	1
		Compartment Light Locations - Standard for Ultramedic	
		(1) #1 Ceiling. 4" light	
		(1) #1 Under O2 shelf (Surface Mount) 6" light with switch	
		(2) #2 Wall above and below standard shelf. 4" light	
		(2) #4 Wall above and below standard shelf. 4" light	
		(1) #6 Ceiling.4" light	
		(3) #8/Right Stack - One in each shelf section. 4" light	
		NOTE: Must pick lights from menu.	
		Any lights other than the standard 4" rnd grommet light must be picked ILOS and any additional locations must be picked as IATS.	
04-03-1600	<	Door Open Indicators, (STANDARD Smart Display)	1
		Door Open Indicators, LCD/VGA - Magnetic Proximity Switches located at the top of the door between the jamb and the door frame will activate a door open indicator on the driver's control panel. The VGA screen will include a digital layout with door open indicators for each module door and activate the corresponding Interior compartment light. NOTE- OPEN CAB DOOR WILL NOT BE SHOWN ON VGA DISPLAY.	
		(STANDARD)	

PART NO	S	DESCRIPTION	QTY
04-03-2400		Compartment Lights - LED, Flexible Strip, KineQuip	1
		EXTERIOR COMPARTMENT INTERIOR	1
04-04-1000	<	Exterior Compartment - Interior Finish, Scorpion Rubberized Exterior Compartment Interior Scorpion Rubberized Liner Color Choice. Scorpion rubberized polyurethane coating applied to the surfaces of ALL exterior compartment walls and dividers. Does not include slide out battery tray or mounting angles. Battery Tray to be raw aluminum. NOTE: If there is not a slide out battery compartment in the order the #8.5 exterior compartment area will be Scorpion lined.	1
04-04-1001		Color = Dark Gray	1
04-05-0001	<	Exterior Compartment Shelving and Unistrut - Standard All exterior shelving Unistrut shall be welded to the walls prior to any compartment wall finish. Where specified, exterior adjustable shelves shall be box pan formed of a minimum .125 inch Aluminum Diamond Plate and corners shall be welded. Shelves shall be infinitely adjustable, and securely mounted to heavy gauge aluminum Unistrut track. (Standard)	1
04-05-1000	<	Compartment Shelving - Diamond Plate (STANDARD) Diamond Plate Exterior Compartment Shelving. (STANDARD)	1
		EXTERIOR - COMPARTMENT #1	1
04-06-0010	<	Electrical Storage - Compartment #1 #1 Compartment Electrical Storage - Recessed into the bulkhead side of compartment #1 shall be an enclosed area for the installation of miscellaneous electrical components. The aluminum cover for this area to be installed with 'J' molding The 'J' molding to be full length of panel on bottom and inboard side. Outboard side of panel to be secured with mechanical fasteners. (STANDARD)	1
04-06-0020		Compartment #1 - Full Height, Standard Configuration (UM)	1
04-06-1505		Shelf, Fixed- Comp. #1 standard	1
04-06-1650		Divider, Fixed - Compartment #1 -Standard	1
		EXTERIOR - COMPARTMENT #2	1
04-07-0600	<	Compartment #2 - Standard Configuration, (UM) #2 Compartment to be directly behind the #1 compartment and below the interior action area shelf on the street side of the module. Includes (1) adjustable shelf on Unistrut track. (STANDARD Ultramedic)	1
04-07-2615		Shelf Adjustable - Compartment #2, Delete	1
		EXTERIOR - COMPARTMENT #4	1
04-09-3000	S <	Compartment #4 - 3/4 height, Standard Configuration Full Height #4 Compartment streetside rear of module with Interior Upper left #4 Cabinet - #4 Compartment to be full height to bottom of upper left #4 cabinet.	1
04-09-4505		Shelf Adjustable , Compartment #4, Standard	1
		EXTERIOR - COMPARTMENT #6	1
04-11-0600	<	Compartment #6 - Standard Configuration (UM) Compartment must be able to accept and store a Stryker Stair-chair model 6252, a scoop stretcher and two standard 72 tall backboards. To be approved by customer for final spec.	1
04-11-2000		Divider, Adjustable - Compartment #6	1
04-11-8000		Exterior #6 Compartment - Floor Matting "Turtle Tile", Black	1

PART NO	S	DESCRIPTION	QTY
EXTERIOR - COMPARTMENT #8			1
04-14-1400		Compartment #8 - Full Height, "A" Style Configuration	1
EXTERIOR - COMPARTMENT #8.5			1
04-15-0400	<	Compartment #8.5 - Drawer Style Battery Box Configuration Battery access door mounted slide out battery tray. The interior of the battery compartment to be made of diamond plate aluminum. Battery Tray to be raw aluminum.	1
NOTE: All 8.5 compartments with a battery tray of any kind will get diamond plated, If the 8.5 does not get a battery tray then it will be painted the same color as the other exterior compartments.			
INTERIOR TRIM AND FEATURES			1
< New Interior Trim - All cabinet and wall panel aluminum trim to be Gray anodized. (No Black trim to be used). All protective corner trim will be Opaque and will include a matching domed end cap. (STANDARD)			
INTERIOR ADJUSTABLE SHELVES			1
INTERIOR TRIM			1
05-05-1000		Interior Trim - Standard	1
HEADLINER			1
05-06-1100		Ceiling Medical Device Rail (STANDARD)	1
05-06-1200		Headliner - Vinyl Ceiling, No Seams (STANDARD)	1
05-06-1802		White LED Strip Lights, Ceiling Medical Device Rail. (UM & PM)	1
05-06-1902		Red/Amber LED Turn/Brake Strip Lights, Ceiling Medical Device Rail. (UM & PM)	1
FLOORING			1
05-07-1000		Aluminum Floor/Wall Cove Moulding. (STANDARD)	1
05-07-1400		Stainless Steel Rear Threshold, 45 Degree Chamfered (STANDARD)	1
05-07-5000		Loncoin II Flecks Flooring Choice (STANDARD)	1
05-07-5003		Color - 157 Moonstone	1
05-07-6100		Curbside Stainless Steel Threshold, 4" wide, W/Safety Walk Anti-Skid Tape	1
HEAD BUMPERS			1
BACKRESTS			1
Rear Entry Door Grab Bars			1
05-10-1301	<	Rear Entry Door Grab Bars - "L" Bars, 16" Anti-Microbial "L" Bars - 16" Anti-Microbial. 1.25" Diameter stainless steel "L" Bars mounted to each rear door. Approximately 16" wide by 24" high. Clear Anti-Microbial finish.	1
05-10-1401	<	Side Entry Door Grab Bars - "L" Bar - 19" Anti-Microbial "L" Bar - 19" Anti-Microbial. 1.25" Diameter stainless steel "L" Bars mounted to side entry door. Approximately 19" wide by 24" high. Clear Anti-Microbial finish.	1
05-10-1903		Ceiling Grab Rail - Center 108" Anti-Microbial	1
05-10-6900		Additional 24" Grab Bar - Antimicrobial	1
05-10-6901		(1) 24" Bar (SPECIFY LOCATION)	1
PLASMA HANGER			1
05-11-4000		IV Hangers - CPI #IV2008 (STANDARD)	1
05-11-4002		Quantity (2)	1
INTERIOR STREETSIDE #1 - LEFT STACK and BULKHEAD			1
05-12-0200	<	Upper Bulkhead Cabinet - Double Doors (STANDARD) Upper Bulkhead Electrical Cabinet, Double Doors - Install double hinged Kydex Thermoplastic or multi-spec (picked in the proper section of work order) doors with locking latch on the right hand door and center mullion on the left hand door on the standard upper bulkhead	1

PART NO	S	DESCRIPTION	QTY
		electrical cabinet. (STANDARD)	
05-12-0250	<	Left Stack - Storage Area #1. (STANDARD Ultramedic) Left Stack Radio/Inverter cabinet to include (1) adjustable shelf in upper section. Both sections shall have double solid aluminum, Kydex Thermoplastic or multi-spec (picked in the proper section of work order) covered doors with locking Latch on the upper doors and locking latch on the lower doors. (standard Ultramedic)	1
05-12-0500	X <	Custom Lower Bulkhead Cabinet Custom Lower Bulkhead Cabinet to accommodate the Norcold Refrigerator in the lower section. Above the Norcold unit will be space to accommodate a Smithworks IV warmer with drop down door and locking latch. Existing left stack to have door(s) moved to facing the rear.	1
05-12-1639	S <	CLICK TO ADD - Lower Bulkhead Interior Cabinet Below (Pass-Though) Vertical column opposite patient access door – behind attendant seat a. Locate at the base of the vertical column (1) Norcold DE0740 refrigerator or approved alternate. Unit to power from either shoreline or on-board inverter. b. Locate above the refrigerator (1) Smithworks floor mount IV Warmer or approved alternate. Provide for a 12-14 gap/space from the top of the IV Warmer to the bottom of the next shelf/cabinet. Unit to power from either Shoreline or vehicle. c. Located above refrigerator/IV Warmer, an open lockable cabinet for storage. Cabinet to have one adjustable shelf. Cabinet to have both (1) 110v receptacle (duplex) and (1) 12v cigarette receptacle. These to be mounted on the rear wall at the top aft of the cabinet. d. Reminder of upper vertical space reserved for system electronics as necessary by manufacturer. To be approved by customer for final spec.	1
		INTERIOR STREETSIDE #2 - ACTION AREA	1
05-13-0700		Medical Device Rail - Action Area Wall	1
05-13-0900	X <	Rear Monitor Area Aft of CPR Seat work area needs to be 11" in length. Electrical connections above this area to include 110v receptacle powered by either shoreline or inverter. One (1) 12v cigarette style outlet located adjacent to 110v receptacle. 12v power from either shoreline or vehicles power system. To be approved by customer for final spec.	1
05-13-1000	X <	Recessed pocket Recessed pocket: See attached drawing regarding notes on recessed pocket for phone and radio storage. Install a recessed pocket for smart phone storage. Locate in the inhalation panel between the control panel and the radio head. Size: 7"w x 4.5"h x 8"d. (all i.d.) Include a 1" retaining lip on the outboard edge. Include (2) 12v battery hot 12v outlets in the back wall for charging jacks. To be approved by customer for final spec	1

PART NO	S	DESCRIPTION	QTY
05-13-1100	X <	Pull out writing surface A pull out writing table will be installed below the inhalation counter top. The writing surface is to be positioned for use at the attendant seat location. Include a pull handle and a large ballcatch latch to hold it in the close position. Install with locking slides to hold it in the open position. Writing tray size: Approximately 13.5"w x 16"d. (i.d.) Install a small trim lip on the rear facing edge to help hold the computer in place. NOTE: This is a flat writing surface with a small angle attached near the aisle side edge. I	1
05-13-1201	<	Cabinet - UL2 and UL2.5 Combined - Single Restock Cabinet (Max Width 43") Cabinet will have one solid vertical divider at the center. Each side of the cabinet will have one adjustable horizontal shelf. Overall dimensions in general to be: 42 (L) x 18 (H) x 18(D) Cabinets to be approved by customer for final spec.	1
05-13-2200	>	Countertop - Forward Action Area and Rear Monitor Area.	1
05-13-2209		Avonite Stormy Gray, F1-9010 (STANDARD)	1
		INTERIOR STREETSIDE #3 - CPR SEAT	1
05-14-0500	S <	Streetside - CPR Seat, Standard Configuration The CPR seat shall include a flip up seat equipped with a gas strut hold open device and positive latch for securing the lid when closed. The CPR seat shall include a 2" foam seat and backrest, the upper and lower sections will be padded on both sides for added protection. CPR seat to be 32" wide	1
05-14-0501	X <	Custom CPR Seat The CPR seat shall include a flip up seat equipped with a gas strut hold open device and positive latch for securing the lid when closed. The CPR seat shall include a 2" foam seat and backrest, the upper and lower sections will be padded on both sides for added protection. CPR seat to be 32" wide. This stretch will extend forward and includes reducing the width of the action area counter and the upper 2 & 2.5 cabinets.	1
05-14-0900	<	CPR Seat Lid Hold-Down CPR Seat Lid Hold-down - Install a Black Tri Mark recessed paddle latch, (1) under CPR seat lid.	1
05-14-2450		Seat Belts, CPR Seat - Standard 2-Point	1
		INTERIOR STREETSIDE #4 - REAR AREA	1
05-15-1000	S <	Cabinet - Upper Left U4, Standard Configuration Streetside #4 Cabinet dimensions to be 27.5 (L) x 12.5 (H) x 18" (D) One adjustable shelf added to cabinet. Cabinets to be approved by customer for final spec	1
		INTERIOR CABINETS- SQUAD BENCH AREA	1
05-16-0100	S <	Curbside Squad Bench - Seating Capacity (3),Curbside Squad Bench The ambulance shall be equipped with a squad bench no less than 73" long by 22" wide. The squad bench lid shall be one-piece and have two heavy-duty, bi-directional gas piston hold-open	1

PART NO	S	DESCRIPTION	QTY
		devices. The force value selected and ball stud locations shall provide lift assistance after twenty degrees of bench lid lift angle. The ball stud mounts shall be at least ten millimeter. The lid shall be attached to the bench with a continuous hinge. Engineering to determine which shock to use in this application. To be approved by customer for final spec.	
05-16-0300		Restraint Belts, Squad Bench - Standard	1
05-16-0500	S <	Medical Device Rail - Squad Bench (1) 110v duplex outlet (1) 12v cigarette outlet (constant hot) (1) Chemetron Oxygen quick-connect (1) Firecom headset connection plate To be approved by customer for final spec	1
05-16-0600		Latches, Lids - Squad Bench Tri Mark , Standard	1
05-16-1010		Seat Belts, Squad Bench - Upgrade 3-Point	1
05-16-1321		Storage, Recessed (2) O2 Brackets - Single Door, Head of Squad Bench	1
		INTERIOR - BIOHAZARD	1
05-17-0200		Squad Bench "A" Bar 16" - Anti-Microbial, Waste/Sharps, (STANDARD)	1
05-17-2000		> Glove Butler(s)	1
05-17-2002	S < >	> Glove Butler (2) Total, Surface Mounted (2) Glove Butler II glove boxes, surface mounted. Must specify location.	1
05-17-2110	<	Glove Butler (3) Total - Over Curbside Entry, Flip Up with (2) Gas Struts Mounted above curbside cabinet. See drawing for location	1
		INTERIOR CURBSIDE - RIGHT STACK STORAGE #8	1
05-18-0600		Right Stack - Upper Heater-A/C Unit, Standard Configuration (UM & PM)	1
05-18-0800		Right Stack - Hidden Air Intake (STANDARD)	1
05-18-1123	< >	> Right Stack-"N2" Style Config, 2 Lexan,2 Aluminum, 1 Drawer, 22" Deep Right Stack ALS, with Locking Middle Drug Drawer. 22" deep for applications with STANDARD or NO CAB RECESS. A 6 Inch high locking drug drawer with gas strut assist shall be installed in the middle portion of the right stack. The drawer shall have 22" slides. The remainder of the right stack shall be split into (2) sections. The lower section shall have framed Lexan double doors with interior and exterior access. The upper section shall have aluminum double doors with interior access only. Both sections shall have (1) shelf in each.	1
		ATTENDANT SEAT	1
05-19-5900		Attendant Seat - EVS Vac Form, 3-Point Belt W/ Child Seat, Swivel Base	1
05-19-5901		Color - Ash	1
		INTERIOR COLORS	1
05-20-5205		Thermoplastic, anti-microbial, Standard Interior Finish	1
05-20-5210	<	Thermoplastic, anti-microbial, Sterile Linen, cabinet finish Part Number #KD18800	1
05-20-5300	<	Upper Band Vinyl Color - Cabinets and Stitched Cushions Upper Band Vinyl Color Choice for cabinets and stitched cushions.	1
05-20-5326		Color - Stratford Smoke SF-74	1
05-20-5500		Lower Band Vinyl Color - Vacuum Formed Cushions	1

PART NO	S	DESCRIPTION	QTY
05-20-5502		Color - Ash Gray	1
05-20-9000		Acrylic Glass Color Choice (STANDARD)	1
05-20-9001		Color - Clear	1
MISC. INTERIOR OPTIONS			1
05-21-1300	S <	Refrigerator - Norcold Model NR751, 2.7 Cu. Ft., 12V/110V NOTE: Need to SR a location for this product. Locate in Left stack behind attendant seat (1) Norcold DE0740 refrigerator or approved alternate. Unit to power from either shoreline or on-board inverter.	1
05-21-5000		Squad Bench & Lower Left, 4 Inch Brushed Stainless Kickpanels (STANDARD)	1
05-21-8400		Cabinet Latch - Southco 2" Round Pull Ring Latch, Stainless Non-Locking	1
05-21-8400		Cabinet Latch - Southco 2" Round Pull Ring Latch, Stainless Non-Locking	2
05-21-8400		Cabinet Latch - Southco 2" Round Pull Ring Latch, Stainless Non-Locking	1
05-21-8400		Cabinet Latch - Southco 2" Round Pull Ring Latch, Stainless Non-Locking	1
06-01-0100		General Wiring - General Harness Standard	1
COMMUNICATION			1
06-06-0150		Antenna UHF/VHF - (2) Prewire, Standard	1
06-06-1000	<	Camera - Exterior Back-Up, Over Rear Doors for Multi-plex Install one (1) Audiovox, ASA Voyager VCCS-155 color camera. Exterior mounted and connected to vehicle's main video display.	1
06-06-1600		Two Way Radio Routing Path Cab to Module. (STANDARD)	1
06-06-1700		Two Way Radio Prewire, 12VDC Power and Ground (STANDARD)	1
06-06-5100		Antenna UHF/VHF - Prewire (1) Additional	1
RADIO AND CLOCKS			1
06-07-5700	<	Clock - Digital , Intellitec Time Manager To be installed above rear doors at center top of module	1
06-07-7400		Speakers - Mounted in Ceiling w/Volume Control in Action Wall	1
07 - ELECTRICAL 12 VOLT DC			1
07-01-0010	<	Crct Pwr Accs.,Ign/Shrline,1-20 amp 12VDC to 2 locs,W/O,PD9130 chrgr (1) 10 amp lead shall be coiled up behind the A/A panel for future use. (1) 10 amp lead shall be coiled up behind the drivers seat in the cab, for future use. Note: This code will be used when and additional battery charger has already been installed, the PD9130 will not be used with this option.	1
07-02-1000		Voltmeter - Standard	1
07-02-1100		Alarm,Low Voltage,With Buzzer and Indicator,in cab console	1
07-03-2000		Ammeter - for Smart Display System	1
BATTERY SYSTEM			1
07-04-5305		Ignition Battery Shut off Timer, 5 minute, (Standard)	1
07-05-0400	<	Batteries - Type I UM/PM, Ford or Dodge Ram (2) OEM (1) Additional,STD The two OEM batteries under the hood. One 735 CCA AC Delco additional battery shall be supplied by the manufacturer and located in the exterior compartment located below the right front ALS cabinet.	1
07-07-0500		Module Disconnect - Smart Display System	1
07-08-0100		Battery Ground	1
07-09-6000		Battery Charger - Prewire only, 12V. (STANDARD)	1
07-10-1000		Power Outlets 12V, (2) Power Point Style, On with Ignition (STANDARD)	1
07-10-7200	<	Outlet, Dual USB Port, 5VDC, 2.1Amp output, Standard Located on the passenger side of the center console, per Electrical Engineering	1
FRONT CONSOLE			1
08-01-1603		Driver's Switch Panel/Radio Console - Smart Display III, Standard	1

PART NO	S	DESCRIPTION	QTY
08-02-1101		Driver's Control Panel - Smart Display III Ultraview UV700, LCD/VGA Display	1
08-02-1200	X <	Fabricate metal slid-in tray/shelf for metal clip board. LOCATE: on inside wall of cab centered above rear of console.	1
08-02-9100		Map Light - Federal LED "Littlite" Gooseneck Light, 12", Fixed Mounted	1
08-02-9400	S <	Cab Dome Light - Whelen Red 4" Round LED in Cab Ceiling 1 light over each seated position in cab. Add switch at light. Whelen 2GR00FRR With Chrome Flangeng 2GFLANGC	2
ATTENDANT CONTROL PANEL			1
08-04-4204	< >	Power Distribution - Smart Display III, Ultraview (UM & PM) Power Distribution - Smart Display 3.0, with Super Node II, Video Inputs. Power Distribution - UV700, Video Inputs (UM & PM) Smart Display III, Multiplex System w/LCD VGA Screen on front switch panel. System to include sealed Micro-Touch Membrane Switches and electro-luminescent backlighting in the Front and Rear Switch Panels. The front panel to include an LCD/VGA screen and the rear panel to include a digital clock and thermostat. Includes an O2 Regulator/Transducer Mounted in #1 Compartment INCLUDES VIDEO INPUTS FOR CAMERA OR GPS	1
08-04-4600	X <	Radio cut out Radio cut-out panel: to be located between the recessed pocket area and the lighting/HVAC control panel. Radio cut-out to fit Motorola APX6500 remote head	1
EMERGENCY SYSTEMS - Ford F-Series			1
WARNING AUDIBLE - SIREN, SPEAKERS, AIR HORNS			1
09-03-1010		Siren Electronic - Whelen 295SLSA1	1
09-03-2020	S <	Speakers - (2) CPI "Though-The-Bumper" for F-Series Cast Products #SAD3806 & SAP3806 siren speakers shall be recess mounted into the ends of the chassis bumper. Locate siren amp on floor of electronics cabinet Configure siren unit to integrate with vehicle horn ring to change modes (yelp/wail)	1
09-03-3080	S < >	Air Horns - Buell, Dual 10", Thru Bumper, Tank/Compressor, F-Series NO Air horns thru center of Front bumper type 1 FORDS per ECN1820 -Install (2) under the front bumper recessed into the air dam valance. System to include compressor and remote air tank. Configure air horn operation so that devices can only be activated when the vehicle is in gear.	1
09-05-0200		Backup Alarm (STANDARD)	1
09-06-0200		Emergency Sequencer/Load Manager (STANDARD)	1
09-40-1010		Lightbar 90" - Whelen 4500 Series Halogen - Base Configuration	1
AUXILLARY EMERGENCY LIGHTS			1
09-70-5000	<	Traffic Pre-Empt - Pre-wire for a 3M Opticom in the Front Light Bar Pre-wire for customer supplied 43 Receptacles, Interior - 110V-15	1
09-80-2110	< >	Warning Light Flasher - Vanner 9860GCPE - (STANDARD)Smart Display/PC System Vanner 9860GCPE Halogen/LED electronic flasher to power specified lights. Four flash Patterns avail.(Alternate flash, Triple burst, Quad burst, double burst.) Standard will be dual Burst Flash unless otherwise noted.	1

PART NO	S	DESCRIPTION	QTY
09-80-3300		Emergency Flashers Set to - KKK-A-1822F Flash Pattern (STANDARD)	1
09-95-1501	<	Whelen M9 LED, w/Chrome Flange - RED (Internal Flasher) ILOS Red lights should be spaced evenly with other lights on top front of module. 2 on each side of the white center light.	4
09-95-1504	<	Whelen M9 LED, w/Chrome Flange LED - WHITE (Internal Flasher) IATS White LED should be at center top front of module spaced evenly with other lights	2
09-95-1505	<	Whelen M9 LED, w/Chrome Flange - RED / Clear Lens (Internal Flasher) ILOS (2) Mounted in rear riser in line with tail lights, (1) each side. Lights to be positioned on rear in these positions: (2) lights on each side of rear of module positioned so that they are at center window when the doors are opened. NOTE: Configure Rear LED warning light to activate w/Brake lighting when not in emergency activation. (2) lights at upper rear outboard corners	6
09-95-1507	<	Whelen M9 LED, w/Chrome Flange - AMBER / Clear Lens (Internal Flasher) IATS Locate Amber LED at top center of rear of module. (Be sure to leave space under LED for rear back up camera space	1
09-95-2401	<	Whelen M7 LED - RED (Internal Flasher) IATS Install (4) four Whelen M7 RED LEDs blank flange. LOCATE: on front grill of cab location. CONFIGURE: Flash to "X" pattern	2
09-95-2401	<	Whelen M7 LED - RED (Internal Flasher) ILOS Red LED should be mounted on both curbside and streetside top row to rear of rear wheel well on module	4
09-95-2404	<	Whelen M7 LED - WHITE (Internal Flasher) IATS White LED light should be mounted on both curbside and driver side top row and in front rear wheel wheel	2
09-95-2410	<	Whelen M7 LED - RED/WHITE Split Clear Lens (Internal Flasher) ILOS Install (2) two Whelen M7 Split Red/White LED black flange – white forward.LOCATE: One each at top of and forward at cab front wheel well	2

PART NO	S	DESCRIPTION	QTY
09-95-4001		Whelen 500 Halogen - RED	2
09-95-9230		Housings, Cast, 500 Series, Grille, Ford, F-series, Pair	1
09-95-9235		Housings, Cast, 15 degree angled, 7x3, Intersection, F-series, Pair	1
EXTERIOR AUTOMOTIVE LIGHTING			1
10-01-1010	<	Tail Lights, Brake/Turn - Whelen 600-Series, All Halogen, Must Pick Flng Must pick housings or flanges.	1
10-01-1210	<	Back-up Lights - Whelen 600-Series Halogen, Must Pick Flng Must pick housings or flanges.	1
10-01-2110		Marker/Clearance Lights, Side and Rear - Whelen OS Mini LED, Red/Amber	1
10-01-3000		Rear Tail/Turn/Brake Light Mounting - Must choose mounting flanges	1
10-01-5020		Outboard Rear Flashers, Wired to OEM Brake Lights (STANDARD)	1
FLOOD AND LOAD SYSTEMS			1
10-02-1000	<	Scene Lighting - (4) Whelen 900 Series Halogen, 8-32°, Standard Do not install the standard light. Only Pioneers	1
10-02-4030	<	Scene, Additional - (2) Whelen Single Panel Pioneer, Center Side Mod LOCATE: two each side of module at top (curbside/streetside) just inside adjacent to M9 emergency side lighting. LOCATE: at rear (patient loading) of module at top, at the outside corners of each patient door. Keep in-line to adjacent M9 light heads at outboard corner	3
Cab Entry - Lighting			1
INTERIOR LIGHTING - CEILING			1
11-01-1201	<	Dome Lights - Whelen 8" , White Flng, LED, (11) ILOS, No Visible Fasteners The interior lighting system shall consist of (11) Whelen 8" round dual intensity white diode super-LED lights with white flange in the following configuration: (3) Cot lights In the medical device rail over the primary cot (2) Bench lights over the squad bench (3) Dome lights Streetside (1) Dome light Curbside over head of squad bench (1) Dome light Curbside rear (1) Dome Light over the walkway The four outside corner lights and the (1) light over the CPR seat area and the (1) over the walkway shall be designated module dome lights and be activated when the side or rear module entrance doors are opened or by a three-way circuit allowing these lights to be turned on and off from cab or module. The lights over the primary cot and squad bench shall have switches in the module that will allow independent high/low/off control. The lights shall produce at least 35 foot candles of light over ninety percent of the cot surface area in the high setting. These lights will not be installed with mechanical fasteners, they will be fastened with a spring clip. NOTE: NO VISABLE FASTENERS OR HOLES.	1
11-01-9000		Timer -15 minute- Restocking (STANDARD)	1
SPOTLIGHTS / HANDHELD LIGHTS			1
11-02-1000		Handheld Spotlight - Sho-Me 200,000 CP (STANDARD)	1
ATTENDANT LIGHT			1
11-03-1001	X	Red/White LED dome light over action area	1

PART NO	S	DESCRIPTION	QTY
12-01-3000	<	Shore Inlet - 15 Amp Straight Blade. (STANDARD) 15 Amp straight blade shoreline inlet and includes a GFI circuit breaker on module wall forward of #1 compartment on streetside of module. (STANDARD)	1
12-01-5500		Additional Kussmaul Super Auto-Eject, 20Amp	1
12-01-6200		Kussmaul, Eject,Cover,15 or 20A,White	1
12-02-0200	S <	Receptacles, Interior - 110V-15A (1) Action Wall, (1) Right Stack	1
		The Streetside Aft compartment shall have two (2) 12 gauge sized 12v fused circuits with a 24 " wire pigtail each originating from the upper left rear interior. Pigtail circuits to be clearly marked. Fuse locations to be customer accessible.	
12-02-2100	>	Receptacles, Interior - (1) Additional 110V-15A, Custom Location (each)	1
		POWER SUPPLY - 110VAC	1
12-03-1000	< >	Inverter Charger - Prewire, Standard Wiring only for 1200 watt maximum 'UL' listed inverter with internal transfer relay and battery charger. Wiring setup is for installation in bottom section of Left Stack/Radio Cabinet. Includes remote on/off switch in the rear switch panel. (STANDARD)	1
		ENVIROMENTAL CLIMATE CONTROL SYSTEM	1
13-01-1200		HVAC - Central Air Flow Discharge (UM & PM)	1
13-01-2000		Climate Control - Standard (UM & PM)	1
13-02-0100		Exhaust Fan - 100 CFM Standard Location	1
13-02-5311		HVAC Base System - Hoseline, Ford F-series MULTIPLEX	1
13-03-1000		Return Air System	1
13-03-2000		Return Air Central Plenum	1
13-03-3000		Heat /AC Cabinet	1
		COT MOUNTING PROVISIONS	1
14-01-1010		Cot Mounting - Hardware	1
14-01-1110		Post & Wheel Cups - None (STANDARD)	1
14-01-1210		Safety Hook - Ship Loose	1
14-01-1230		Safety Hook - Stryker with bolts,Shipped Loose	1
14-01-1500		Cot Mount - Stryker 6370 Single Center Position	1
		OXYGEN AND AIR SYSTEMS	1
14-02-0100		Oxygen System - Smart System II	1
14-02-1000		O2 Cylinder Wrench - (STANDARD)	1
14-02-2000		O2 Cylinder Bracket - Zico #QR-MV (STANDARD)	1
14-02-3000		O2 Control - Electric with Manual Bypass on Action Wall. (STANDARD)	1
14-02-4020		O2 Outlets - NCG Chemtron, (2) Action Wall, (1) Squad Bench	1
14-02-4212		O2 Outlet, Additional - NCG Chemtron in Ceiling	1
14-02-7500		Flowmeter - Ball Type O2 with Chemtron (NCG) Adapter	2
14-02-7501		(1) Flow meter.	2
14-02-9040	<	SCBA, (2) Brackets Installed - Inside Vehicle - NFPA Compliant,Zico #ULLH Installed at head of squad bench underneath.	1
		VACUUM SYSTEM	1
14-03-1010		Suction System - SSCOR On-Board (STANDARD)	1
		MISC MEDICAL	1
14-04-1000		Fire Extinguisher - 5# (ABC) w/ Mounting Bracket. (STANDARD)	1
14-04-2030		IV Warmer - Smithworks 12VDC w/Switch	1
14-04-5030	<	Swivel Tray - For Zoll E Series. H9000 General location to be located Aft of the counter area on the main action area. The specific location shall be determined. NOTE: Fabricate Avonite spacer sized to fit beneath a Zoll E-Series heart	1

PART NO	S	DESCRIPTION	QTY	
		monitor. Spacer to allow bracket to swivel over counter top lip.		
		to be approved by customer for final spec		
		PAINT AND FINISH		
15-00-0100		Road Rescue Paint Process - Standard	1	
15-01-2020	< >	Module and Cab - Lower Half, Different Color, No Jamb Note: Must state color with paint code and request a paint spray-out for approval.	1	
15-01-9810	<	Upper Paint Color Note: Must state color with paint code and request a paint spray-out for approval.	1	
15-01-9820	<	Lower Paint Color Note: Must state color with paint code and request a paint spray-out for approval.	1	
15-01-9900		Color - White Match to OEM Chassis	1	
15-01-9902	<	Color - Red FLNA30573 Note: Must request a paint spray-out for approval.	1	
		GRAPHICS AND LETTERING		
15-03-3900		RR Badge Logos (STANDARD)	1	
15-03-4000		RR Logos and Model Name (STANDARD)	1	
15-03-4001		Ultramedic - Black Logos	1	
15-04-1000		Nomenclature Plaques. DIESEL. (STANDARD)	1	
		16 - REFERENCES AND STANDARDS		
16-01-0200		Owner's Manual (STANDARD)	1	
		== Dealer Provided - EMS Equipment - 2014.018 03/26/15 ==	1	
		Subtotal		191,265.00
		Delivery		1,800.00
		Ford GPC Discount		-4,565.00
		Graphix Per Bid		3,000.00
		Factory Inspection Trips 2 personal		3,000.00
		Prep and final delivery to Pleasant Prairie		500.00
		Total		195,000.00



1133 RANKIN STREET SUITE D-1
ST. PAUL, MN 55116
6/9/2015

Pleasant Prairie Side Entry Door Option Estimate

Option: To meet customer's conceptual design per drawing and specifications, move side entry door forward on module, move ALS compartment, and move HVAC.

Side Entry Door, Move Forward. The curbside entry door to be moved forward to the bulk head wall. 750

Reduce the height of the #1 compartment to accommodate the upper left cabinet required for the relocation of the HVAC unit. \$31

Upper left cabinet to house the HVAC. There shall be a cabinet to house the HVAC unit. This cabinet is to have (2) stainless steel access panels. These panels will be punched to provide venting for the HVAC return air. There shall be an interior access panel to the power distribution area that will be hidden by this cabinet. There shall be a heat A/C unit located above the compartment. \$440

The HVAC unit to be relocated to the street side of the module. The conditioned air to be ducted directly to a plenum that runs the entire length of the module and will include (6) adjustable vents that will evenly distribute the air flow. The interior of the plenum to be completely insulated with a reflective barrier. This option includes modifying the ceiling structure to accommodate air flow down the street side of the module. \$990



1133 RANKIN STREET SUITE D-1
 ST. PAUL, MN 55116
 6/9/2015

EXCEPTIONS AND CLARIFICATIONS TO BID FOR PLEASANT PRAIRIE, WI

<u>Page:</u>	<u>Item Number:</u>	<u>Description:</u>
2	General Requirements (conversion)	Module height_72" headroom Standard
2	Section #1	Module length is 168"x 96"
5	Section G	Diamond Plate Splash shields
6	Section N	Heater/AC unit and cool air return in Left Stack
7	Interior Wall & Cabinets	Moving side patient access door allows squad bench dimensions requested to accept a 72" backboard. While keeping the other cabinet items intact. (see item listed under "Exterior Compartments")
8	Interior Wall & Cabinets	Left stack forward of side entry patient entry door
8	Interior Wall & Cabinets	Knox Med-vault moved to the Left Stack
8	Interior Wall & Cabinets	Move the interior drawer to Left Stack
8	Interior Wall & Cabinets	Move glove butler to a position above the side entry door.
9	Street Side Forward Top	Inverter and suction vacuum motor in left stack. Accessible from compartment. Not as listed above O2 tank.
9	Street side Compartment	Compartment size: 23.25" x 81.5" x 22"
9	Street side Intermediate	Compartment size: 43.88" x 35.5" x 21.25
9	Street side aft compartment	Compartment size: 29.50" x 59.06" x 21.25"
10	Exterior compartments	Patient entry door aft of ALS compartment
	Standard for Road Rescue	

Minnesota: 1133 Rankin St., Suite D-1, Saint Paul, MN 55116
Wisconsin: 801 Fullview Drive Unit #4, Appleton, WI 54913
Toll Free 800-889-6143 | Fax 651-690-1871 | info@everestev.com | www.everestev.com



Everest Emergency Vehicles

***Reaching New Heights in
Emergency Vehicle Sales and Service.***

EVEREST EMERGENCY VEHICLES



About Us

Family owned and operated since 1999, Everest Emergency Vehicles is the authorized dealer for Road Rescue in Minnesota, Wisconsin and Iowa and Wheeled Coach Industries in the states of Minnesota and Wisconsin. Our dedicated staff, sales team and service department has over 75 years combined emergency vehicle experience, we promise to exceed your highest expectations.

Everest EV provides new and used ambulance sales along with module service and parts. Our service team specializes in ambulance repair and maintenance. We are a factory authorized ambulance Re-Mount center with Ford QVM certification. Please call on us for all your emergency vehicle needs.



Remounts

Everest Emergency Vehicles is a authorized Road Rescue & Wheeled Coach remount facility. We specialize in the remounting and refurbishing of all manufacturers makes and model ambulances to a "like new" condition on a current model year chassis of your choice. We operate within strict compliance to KKK, FMVSS, and DOT standards on all operations performed on your remount.

All operations are completed by skilled craftsmen with many years of ambulance assembly and remount experience. Backed up by a strong warranty. We service what we sell, including field warranty repair when needed. We have excellent service staff to make warranty and after sale services friendly and easy.



HealthEast Medical Transportation

- Ambulance converted to a Critical Care Unit
- Cab to module access redesigned adding counter space and storage
- Cabinets, drawers, and refrigerator added in patient compartment

Everest Emergency Vehicles is a Ford QVM certified remount facility.



Service

Service

Here at Everest Emergency Vehicles, we not only sell new and previously owned emergency vehicles but also have the facility and experienced personnel to service any maker's emergency vehicle after the sale.

Our service department has extensive industry experience. We can trouble shoot and repair all styles, makes and models of emergency vehicles, including maintenance of the electrical, oxygen, and Heat/AC systems and all modular conversion workings. We also work with our experienced partners to provide collision repair, including full paint services and graphic design for all your custom emergency vehicle decals and lettering needs.

Our Service Department has developed a preventative maintenance program to ensure that your ambulance stays in top operational condition. Let us help you establish a preventive maintenance program for your vehicles. Should your vehicle need to be serviced over a period of time, loaner and rental vehicles are available.

Whether it's routine maintenance, warranty repairs, or any other type of service needed, we're ready to get you back on the road. Everest EV is a certified Road Rescue and Wheeled Coach service center.



Parts

We offer a full inventory of emergency vehicle parts and a staff of knowledgeable service technicians waiting to serve you. We carry a wide range of parts and supplies in our inventory that are commonly used in this field. This allows for immediate delivery to our customers. We also have a working relationship with most major suppliers so that we can obtain any additional needed parts.

We are a Stryker EMS Dealer and also an On-Spot Tire Chain installation center.





Allina Medical Transportation

- Cabinet redesign and seating relocation
- Front lightbar replaced with surface mounted L.E.D. lighting
- Custom designed console installed to accommodate computer mounting

North Kitsap Fire & Rescue

- Module mounted on International medium duty chassis
- Rhino Lining added to all compartments walls & shelves
- Interior lighting converted to Whelen halogen lights



The **Road Rescue** product line is well-known throughout the industry for quality and innovation and has been in the business of constructing emergency medical vehicles for over 30 years. Road Rescue engineers ambulances well beyond the standards of today and meet or exceed all Federal KKK-1822-F specifications. With a state-of-the-art electrical system, a lifetime module structural warranty, and more standard features than anyone else, Road Rescue sets the standards for quality, safety, innovation and industry expertise. When you choose Road Rescue, you're getting the safest, most innovative ambulances in the industry.

Road Rescue ambulances are built not just for you, but according to your most challenging needs. Which may be why Road Rescue has earned the trust and loyalty of those who expect rugged durability, exciting customization and unmatched safety on the roads of life.



Since the beginning, **Wheeled Coach** has kept its eye on the future; always working with customers to improve upon today's ambulances and rescue vehicles. The goal has been to make emergency vehicles safer for both patient and crew, and at the same time, make them more efficient.

Wheeled Coach Industries routinely conducts external audits and testing of their Quality Management System, materials, construction methods and workmanship to provide customers the highest levels of quality and safety possible. Quality Management System Audits include: International Organization for Standardization (ISO), Ford Motor Company Qualified Vehicle Modifier Program (QVM), National Truck Equipment Association MVP Membership, Test Certifications to the current KKK-A-1822 and AMD requirements, Hygee Sled Test for Frontal Impact, Modular Body Drop Test, Internal and External Weld Inspections, and Ambulance Side Impact Crash Test. Please visit www.everestev.com for a complete presentation on these safety tests.

Wheeled Coach constructs complete and versatile Type II and modular style Type I and III ambulances with long-term reliability, unique capabilities and insightful, innovative solutions. Their modular ambulances come in a wide variety of design layouts to fit your specific requirements. As the world's largest ambulance manufacturers, Wheeled Coach provides a customizable stock ambulance program with over 50 stocked vehicles ready and capable for short delivery timelines. Please visit our website, www.everestev.com for an available listing and more Wheeled Coach information.





Factory Authorized Service Center Information

Road Rescue and Wheeled Coach factory approved service center for preventative maintenance, service, repair, warranty & remounts.

Customer Service Representatives

Eric Olson- Direct- 651-236-8943

Bob Harris- Direct- 651-236-8949



-Mobile Service Available

-Loaner/Rental Ambulance Fleet Available

- Ford QVM Certified



-Service of All makes and models

Visit our web page for all of your ambulance needs.

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Statewide Services, Inc.

Claim Division

1241 John Q. Hammons Dr.
P.O. Box 5555
Madison, WI 53705-0555
877-204-9712

VIA E-MAIL ONLY

June 3, 2015

Village of Pleasant Prairie
Attn: Ms. Jane Romanowski, Clerk
9915 39th Ave.
Pleasant Prairie, WI 53158

RE: Our Claim #: WM000301740125
Date of Loss: 04/09/2015
Claimant: Todd Kaitis
4500 83rd St,
Pleasant Prairie, WI 53158

Loss location: 4500 83rd St,
Pleasant Prairie, WI 53158

Dear Ms. Romanowski:

As you know, Statewide Services, Inc. administers the claims for the League of Wisconsin Municipalities Mutual Insurance, which provides the insurance coverage for the Village of Pleasant Prairie. We are in receipt of the above-captioned claim in which the claimant is asserting damage on account of a backup of sewer water into his home.

Our investigation has revealed that the Village was not negligent for this incident. Therefore, we recommend that the Village disallow this claim pursuant to the Wisconsin Statute for disallowance of claim 893.80(1g). The disallowance of the claim in this manner will shorten the statute of limitations period to six months.

This loss occurred due to a diagnosed clog within the sewer main; however, Village operations did not cause the clog. Furthermore, the Village had no prior notice of the clog so as to take a measure to avoid damage. Regarding the notice issue, the Village would not be liable for a clog in the sewer main unless they knew, or should have known, about it and had adequate time to resolve the problem. Finally, Village staff immediately reacted once advised of the problem in order to diagnose the clog and remove the clog/clear the line, thus, there is no liability for any inappropriate response time by Village staff.

I have enclosed a sample Notice of Disallowance for your use, should you choose to use it, or you may use your own. Please send your disallowance, on your letterhead, directly to the claimant at the above listed address. This should be sent certified

or registered (restricted) mail and must be received by the claimant within 120 days after you received the claim. Please send me copies of the letters for our file.

Thank you, Jane, for your attention to the above, and please do not hesitate to contact me with any questions.

Best regards,

Doug Detlie

Douglass A. Detlie
Casualty Claims Specialist
Statewide Services, Inc
PO Box 5555
Madison, WI 53705-0555
Office: 608-828-5503
Fax: 800-720-3512
E-mail: ddetlie@statewidesvcs.com

[Cc: Rick, Kalscheuer, Agent](#)



Office of the Village Clerk
Jane M. Romanowski

MEMORANDUM

TO: Village Board of Trustees

FROM: Jane M. Romanowski
Village Clerk

RE: 2015-2016 Towing Licenses

DATE: June 11, 2015

The following companies have submitted applications to renew their respective towing licenses in the Village:

Atlas Service Center Inc., 5150 60th Street, Kenosha, WI
Firehouse Performance, 4502 22nd Avenue, Kenosha, WI
J & M Towing, 6010 26th Avenue, Kenosha, WI
Jantz Towing, 3405 Washington Road, Kenosha, WI
Jensen & Jensen, 5410 – 50th Street, Kenosha, WI
Pro Towing, 6018 26th Avenue, Kenosha, WI

All applications comply with Section 332 of the Municipal Code and the license fees have been paid. Chief Smetana and I recommend approval of the six applications for towing licenses effective July 1, 2015 - June 30, 2016 subject to the provisions of Chapter 332 of the Municipal Code.

* * * * *



**CERTIFICATE FOR REDUCTION
TO THE LETTER OF CREDIT NO. 37351600002**

DATE: June 15, 2015

TO: Talmer Bank as Successor Interest to
First Banking Center
8700 75th Street
Kenosha, WI 53142

RE: Reduction Certification No. 5 to Letter of Credit No. 37351600002
Applicant: Burco Holdings LLC

Jane M. Romanowski, being first duly sworn on oath, deposes and certifies as follows:

1. I am the Village Clerk of the Village of Pleasant Prairie, Wisconsin (the "Village").
2. The Village Board of Trustees, at a meeting duly held on the 15th day of June, 2015, duly approved an irrevocable reduction to the Maximum Amount of this Letter of Credit by the amount of \$4,573.00.

Village of Pleasant Prairie

Jane M. Romanowski, Clerk

Subscribed and sworn to before me
this 15th day of June, 2015.

Notary Public, State of Wisconsin
My Commission expires: _____